

Borough, Bankside and Walworth Community Council

Monday 29 September 2014

7.00 pm

Amigo Hall, St George's Cathedral, Lambeth Road (junction with St.
George's Road), London SE1 6HR

Supplemental Agenda No.1

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Date: 19 September 2014

TP(Permit)
Agenda Item 15
SOUTHWARK COUNCIL



TOWN AND COUNTRY PLANNING ACT 1990 (as amended)

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PLANNING PERMISSION WITH LEGAL AGREEMENT

Applicant Guys & St Thomas' NHS Foundation Trust
Date of Issue of this decision 31/01/2013

LBS Registered Number 12/AP/2062

Planning Permission was GRANTED for the following development:

Demolition of existing buildings on the corner of Great Maze Pond and Snowfields and erection of a 14 storey building for a Cancer Treatment Centre (with an additional 2 storeys of roof plant) 71 metres in height and 29,000sqm floor area, with preservation in situ of a Scheduled Ancient Monument (Roman Boat), public realm works, disabled parking, cycle parking facilities and basement link to hospital campus.

At: GUYS HOSPITAL, GREAT MAZE POND, LONDON SE1 9RT

In accordance with application received on 26/06/2012 Your Ref. No.:
and revisions/amendments received on 15/10/2012
10/09/2012

and Applicant's Drawing Nos. RSHP-00-0001-GA-00 Rev P04, RSHP-00-0010-GA-00 Rev P04, RSHP-00-0015-GA-00 Rev P04, RSHP-00-0100-GA-00 P03, RSHP-XX-0101-GA-01 P03, RSHP-XX-0102-GA-02 P03, RSHP-XX-0103-GA-RF P03, RSHP-00-0149-GA-B1 Rev P04, RSHP-01-00150-GA-00 Rev P04, RSHP-01-0151-GA-01 Rev P04, RSHP-02-0152-GA-02 Rev P04, RSHP-02-0153-GA-03 Rev P04, RSHP-02-0154-GA-04 Rev P04, RSHP-03-0155-GA-05 Rev P04, RSHP-03-0156-GA-06 Rev P04, RSHP-04-0157-GA-07 Rev P04, RSHP-04-0158-GA-08 Rev P04, RSHP-04-0159-GA-09 Rev P04, RSHP-05-0160-GA-10 Rev P04, RSHP-05-0161-GA-11 Rev P04, RSHP-05-0162-GA-12 Rev P04, RSHP-05-0163-GA-13 Rev P04, RSHP-06-0164-GA-RF Rev P04, RSHP-06-0165-GA-PD P03, RSHP-XX-0020-EL-S P03, RSHP-XX-0021-EL-W P03, RSHP-XX-0050-EL-S Rev P04, RSHP-XX-0051-EL-W Rev P04, RSHP-XX-0603-SE-DD P03, RSHP-XX-0605-SE-FF P03, RSHP-XX-0613-SE-DD Rev P04, RSHP-XX-0615-SE-FF Rev P04, RSHP-XX-0800-EL-S P03, RSHP-XX-0801-EL-W P03, RSHP-XX-0802-EL-N P03, RSHP-XX-0810-EL-S Rev P04, RSHP-XX-0811-EL-W Rev P04, RSHP-XX-0812-EL-N P03, RSHP-XX-0813-EL-W Rev P02, RSHP-XX-0814-EL-E Rev P02, RSHP-XX-2010-DT-W P03, RSHP-XX-2011-DT-W P03, RSHP-XX-2012-DT-W P03, RSHP-XX-2013-DT-2 P03, RSHP-XX-2014-DT-N P03, RSHP-XX-2015-DT-N P03, RSHP-XX-2016-DT-W Rev P01, RSHP-XX-0814-EL-E P01, RSHP-XX-0813-EL-W

PLANNING SUPPORTING STATEMENT, OUTLINE CONSTRUCTION METHOD STATEMENT, LVMF KENWOOD HOUSE ASSESSMENT, TOWNSCAPE AND VISUAL IMPACT ASSESSMENT, LOW/ZERO CARBON TECHNOLOGIES FEASIBILITY STUDY, ENERGY STRATEGY, CONSULTATION STATEMENT, SUSTAINABILITY AND ENERGY ASSESSMENT, TRANSPORT ASSESSMENT, TRAVEL PLAN, WSI FOR ARCHAEOLOGICAL EXCAVATION, TECHNICAL NOTE - CONVERSION OF SNOWFIELDS TO ONE WAY, AIR QUALITY IMPACT TECHNICAL REPORT, NOISE IMPACT TECHNICAL REPORT, ROMAN BOAT MONITORING PROPOSAL, SUSTAINABILITY ASSESSMENT CHECKLIST, ECOLOGY ASSESSMENT, WIND ASSESSMENT (INCLUDING ADDENDUM 06/09/2012), FLOOD RISK ASSESSMENT, ARBORICULTURAL IMPLICATIONS ASSESSMENT, DELIVERY AND SERVICING PLAN, WATCHING BRIEF REPORT, ARCHAEOLOGICAL EVALUATION REPORT, ODYSSEY CONSULTING ENGINEERS TECHNICAL NOTE 10/09/2012, TVIA & LVMF SUPPLEMENTARY STATEMENT (SEPTEMBER 2012), DAYLIGHT AND SUNLIGHT REPORT (INCLUDING TECHNICAL NOTE 07/09/2012), WIND ASSESSMENT, LANDSCAPE STATEMENT (INCLUDING ADDENDUM SEPTEMBER 2012), & DESIGN & ACCESS STATEMENT (INCLUDING ADDENDUM SEPTEMBER 2012)

Reasons for granting permission.

This planning application was considered with regard to various policies including, but not exclusively:

a) Core Strategy 2011

Strategic Policy 1 Sustainable Development which requires developments to improve the places we live in and work in and enable a better quality of life for Southwark's diverse population.

Strategic Policy 2 - Sustainable transport: We will encourage walking, cycling and the use of public transport rather than travel by car. This will help create safe, attractive, vibrant and healthy places for people to live and work by reducing congestion, traffic and pollution.

Strategic Policy 3 Shopping, Leisure and Entertainment which defines a hierarchy of town and local centres which reflect their sizes and roles.

Strategic Policy 4 Places to Learn and Enjoy seeks to ensure that there will be a wide range of well used community facilities that provide spaces for many different communities and activities in accessible areas.

Strategic Policy 10 - Jobs and businesses: We will increase the number of jobs in Southwark and create an environment in which businesses can thrive. We will also try to ensure that local people and businesses benefit from opportunities which are generated from development.

Strategic Policy 11 Open Spaces and Wildlife protects important open spaces, trees and woodland from inappropriate development.

Strategic Policy 12 - Design and conservation: Development will achieve the highest possible standards of design for buildings and public spaces to help create attractive and distinctive places which are safe, easy to get around and a pleasure to be in.

Strategic Policy 13 - High environmental standards: Development will help us live and work in a way that respects the limit's of the planet's natural resources, reduces pollution and damage to the environment and helps us to adapt to climate change.

b) Saved Policies of the Southwark Plan 2007

Policy 2.2 (Provision of new community facilities): seeks to ensure planning permission will be granted for new community facilities.

Policy 2.5 (Planning obligations): seeks to ensure that any adverse effects arising from a development is taken into account and mitigated and contributions towards infrastructure and the environment to support the development are secured, where relevant in accordance with Circular 05/2005 and other relevant guidance.

Policy 3.1 (Environmental effects): seeks to ensure there will be no material adverse effect on the environment and quality of life resulting from new development.

Policy 3.2 (Protection of amenity): advises that permission will not be granted where it would cause a loss of amenity.

Policy 3.4 (Energy efficiency): advises that development should be designed to maximise energy efficiency.

Policy 3.6 (Air quality): advises that permission will not be granted for development that would lead to a reduction in air quality.

Policy 3.12 (Quality in design): requires new development to achieve a high quality of architectural and urban design.

Policy 3.13 (Urban design) seeks to ensure that principles of good urban design are taken into account in all developments.

Policy 3.14 (Designing Out Crime) seeks to ensure that development is designed to improve community safety and crime prevention.

Policy 3.20 (Tall Buildings) advises that permission may be granted for buildings that are significantly taller than their surroundings or have a significant impact on the skyline, where they have excellent public transport accessibility and are located within the Central Activities Zone),

Policy 3.22 (Important Local Views) advises that the Local Planning Authority will seek to protect and enhance identified views, panoramas, prospects and their settings. Developments that would impact negatively on important local views will not be granted.

Policy 3.28 (Biodiversity) requires biodiversity to be taken into account in the determination of planning applications and the inclusion in developments of features which enhance biodiversity will be encouraged.

Policy 3.31 (Flood Defences) advises that permission will not be granted for development sited adjacent to the River Thames unless it is set back at a suitable distance from the river wall to allow for the replacement/repair of flood defences and for any future raising to be undertaken in a suitable and cost effective manner.

Policy 5.1 (Locating Developments) states that location of development must be appropriate to the size and trip generating characteristics of the development, stating that schemes generating a significant number of trips must be located within easy access of public transport nodes.

Policy 5.2 (Transport Impacts) states that permission will not be granted for development which has an adverse impact on transport networks through significant increases in traffic or pollution and consideration has been given to impacts on the Transport for London road network as well as adequate provision for servicing, circulation and access to and from the site.

Policy 5.3 (Walking and cycling) seeks to ensure that there is adequate provision for cyclists and pedestrians within developments, and where practicable the surrounding area

Policy 5.6 (Car Parking) states that all developments requiring car parking should minimise the number of spaces provided.

Policy 5.7 (Parking Standards for Disabled People) requires development (subject to site constraints) to provide adequate car parking for disabled people and the mobility impaired.

c) London Plan 2011

Policy 2.10 Central Activities Zone – strategic priorities, Policy 2.11 Central Activities Zone – strategic functions, Policy 2.13 Opportunity Areas and Intensification Areas, Policy 2.15 Town Centres, Policy 4.1 Developing London's economy, Policy 4.7 Retail and town centre development, Policy 5.1 Climate change mitigation, Policy 5.2 Minimising carbon dioxide emissions, Policy 5.5 Decentralised energy networks, Policy 5.6 Decentralised energy in development proposals, Policy 5.7 Renewable energy, Policy 6.1 Strategic approach (Transport), Policy 6.13 Parking, Policy 7.4 Local character, Policy 7.5 Public realm, Policy 7.6 Architecture, Policy 7.7 Location and design of tall and large buildings, Policy 7.11 London View Management Framework, Policy 7.12 Implementing the London View Management Framework and Policy 8.2 Planning obligations.

d) The National Planning Policy Framework

- Section 1: Building a strong, competitive economy
- Section 2: Ensuring the vitality of town centres
- Section 4: Promoting sustainable development
- Section 7: Requiring good design
- Section 8: Promoting healthy communities
- Section 10: Meeting the challenge of climate change, flooding and coastal change
- Section 11: Conserving and enhancing the natural environment
- Section 12: Conserving and enhancing the historic environment

Particular regard was had to the principle of the proposed uses which were considered to be acceptable, and which will provide an acceptable mix to support the regeneration of the London Bridge Borough and Bankside Opportunity Area. The redevelopment of the site would generate significant economic benefits for the local and wider area and will increase the level of hospital care in Southwark.

The proposal satisfies the policy requirements for tall buildings, and for high quality design, including the NPPF expectation that new development has the highest standard of design. The design of the building is considered to be high quality that will result in a striking form, creating a new destination point and is considered to make a positive contribution to the skyline of London creating a local landmark building within Southwark.

Particular regard was had to the impact of the building on views with specific reference to views from Kenwood. Taking into account the revised LVMF SPG, the impact on these views is considered acceptable and any perceived harm to the wider historic environment is outweighed by the regenerative improvements delivered by the scheme that will achieve substantial public benefits.

The proposal would provide an extensive improvement of the streetscape together with new active frontages which would improve the experience for pedestrians, and provide for natural surveillance. The new paved areas allow for a range of uses to spill out into the space, which would add vitality to the space. New green roofs should enhance biodiversity and create habitat.

The impacts of the scheme in relation to daylight and sunlight have been assessed and are considered acceptable. The scheme is not expected to cause any unacceptable impact to local highway or transport conditions

Planning obligations are secured to offset the impact of the development in accordance with the Supplementary Planning Document on Planning Obligations.

Other policies have been considered, but in this instance were not considered to have such weight as to justify a refusal of permission. It was considered that the regenerative benefit to the wider community would outweigh any adverse impacts of the proposed development. It was therefore considered appropriate to grant planning permission having regard to the policies considered and any other material planning considerations.

Subject to the following conditions:

1 Standard Time Limit

The development hereby permitted shall be begun before the end of three years from the date of this permission.

Reason

As required by Section 91 of the Town and Country Planning Act 1990 as amended.

2 Approved plans

The development hereby permitted shall not be carried out otherwise than in accordance with the following approved plans:

RSHP-00-0001-GA-00 Rev P04, RSHP-00-0010-GA-00 Rev P04, RSHP-00-0015-GA-00 Rev P04,
 RSHP-00-0100-GA-00 P03, RSHP-XX-0101-GA-01 P03, RSHP-XX-0102-GA-02 P03,
 RSHP-XX-0103-GA-RF P03, RSHP-00-0149-GA-B1 Rev P04, RSHP-01-00150-GA-00 Rev P04,
 RSHP-01-0151-GA-01 Rev P04, RSHP-02-0152-GA-02 Rev P04, RSHP-02-0153-GA-03 Rev P04,
 RSHP-02-0154-GA-04 Rev P04, RSHP-03-0155-GA-05 Rev P04, RSHP-03-0156-GA-06 Rev P04,
 RSHP-04-0157-GA-07 Rev P04, RSHP-04-0158-GA-08 Rev P04, RSHP-04-0159-GA-09 Rev P04,
 RSHP-05-0160-GA-10 Rev P04, RSHP-05-0161-GA-11 Rev P04, RSHP-05-0162-GA-12 Rev P04,
 RSHP-05-0163-GA-13 Rev P04, RSHP-06-0164-GA-RF Rev P04, RSHP-06-0165-GA-PD P03,
 RSHP-XX-0020-EL-S P03, RSHP-XX-0021-EL-W P03, RSHP-XX-0050-EL-S Rev P04,
 RSHP-XX-0051-EL-W Rev P04, RSHP-XX-0603-SE-DD P03, RSHP-XX-0605-SE-FF P03,
 RSHP-XX-0613-SE-DD Rev P04, RSHP-XX-0615-SE-FF Rev P04, RSHP-XX-0800-EL-S P03,
 RSHP-XX-0801-EL-W P03, RSHP-XX-0802-EL-N P03, RSHP-XX-0810-EL-S Rev P04,
 RSHP-XX-0811-EL-W Rev P04, RSHP-XX-0812-EL-N P03, RSHP-XX-0813-EL-W Rev P02,
 RSHP-XX-0814-EL-E Rev P02, RSHP-XX-2010-DT-W P03, RSHP-XX-2011-DT-W P03,
 RSHP-XX-2012-DT-W P03, RSHP-XX-2013-DT-2 P03, RSHP-XX-2014-DT-N P03, RSHP-XX-2015-DT-N
 P03, RSHP-XX-2016-DT-W Rev P01, RSHP-XX-0814-EL-E P01, RSHP-XX-0813-EL-W

Reason:

For the avoidance of doubt and in the interests of proper planning.

3 Submission of further details - external facing materials

Samples of all external facing materials to be used in the carrying out of this development shall be presented on site and approved in writing by the Local Planning Authority prior to works commencing above grade; the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the Local Planning Authority may be satisfied as to the design and details in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

4 Submission of further details - mock-up

Full-scale mock-ups of the cladding to the building to be used in the carrying out of this development shall be provided and made available for approval in writing by the Local Planning Authority prior to works commencing above grade; the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the Local Planning Authority may be satisfied as to the design and details in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

Continued overleaf...

TP(Permit)

SOUTHWARK COUNCIL

TOWN AND COUNTRY PLANNING ACT 1990 (as amended)



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PLANNING PERMISSION WITH LEGAL AGREEMENT**LBS Reg. No. 12/AP/2062****Date of Issue of this decision 31/01/2013****5 Submission of further details - detailed drawings**

1:5/10 section detail-drawings through all main elements of the elevations and roof elements to be used in the carrying out of this permission shall be submitted to and approved in writing by the Local Planning Authority prior to works commencing above grade; the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the Local Planning Authority may be satisfied as to the design and details in the interest of the special architectural or historic qualities of the listed building in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

6 Telecommunications

Notwithstanding the provisions of Parts 24 and 25 of the Town & Country Planning (General Permitted Development) Order 1995 (or any order revoking and re-enacting that Order with or without modification) no external telecommunications equipment or structures shall be placed on any part of the buildings hereby permitted, without the prior written consent of the Local Planning Authority.

Reason:

In order to ensure that no telecommunications plant or equipment which might be detrimental to the design and appearance of the building and visual amenity of the area is installed on the roof of the building in accordance with Strategic Policy 12 – Design and Conservation of the Core Strategy and saved policies 3.2 Protection of amenity and 3.13 Urban Design of the Southwark Plan 2007.

7 Roof plant

No roof plant, equipment or other structures, other than as shown on the plans hereby approved or approved pursuant to a condition of this permission, shall be placed on the roof or be permitted to project above the roofline of any part of the building[s] as shown on elevational drawings or shall be permitted to extend outside of the roof plant enclosure[s] of any building[s] hereby permitted.

Reason:

In order to ensure that no additional plant or equipment which might be detrimental to the design and appearance of the building and visual amenity of the area is installed on the roof of the building in accordance with Strategic Policy 12 – Design and Conservation of the Core Strategy and saved policies 3.2 Protection of amenity and 3.13 Urban Design of the Southwark Plan 2007.

8 Construction and Environmental Management Plan

The development shall not commence until details of an Environmental Management Plan for Construction has been submitted to, and approved in writing by the Local Planning Authority for that part of the development. The Environmental Management Plan shall oblige the applicant, or developer and its contractor to use all best endeavours to minimise disturbances including but not limited to noise, vibration, dust, smoke and plant emissions emanating from the site during demolition and construction and will include the following information for agreement:

- A detailed specification of demolition and construction works at each phase of development including consideration of environmental impacts and the required remedial measures.

- The specification shall include details of the method of piling.
- Engineering measures, acoustic screening and the provision of sound insulation required mitigating or eliminating specific environmental impacts.
- Consideration of the sensitive nature of the listed buildings at 25 and 27 Crosby Row.
- Arrangements for publicity and promotion of the scheme during construction.
- A commitment to adopt and implement of the ICE Demolition Protocol and Considerate Contractor Scheme registration.

All demolition and construction work shall be undertaken in strict accordance with the approved management scheme and code of practice, unless otherwise agreed in writing by the Local Planning Authority.

Reason:

To ensure that residents and occupiers of neighbouring buildings do not suffer a loss of amenity by reason of pollution and nuisance in accordance with Strategic Policy 13 of the Core Strategy 2011 and saved policies 3.1, 3.2, 3.6 and 3.10 of the Southwark Plan 2007.

9

Cycle Storage

Prior to the occupation, details of the cycle storage facilities within the site (including shower and change facilities) and within the vicinity of the site shall be submitted to and approved in writing by the local planning authority. Thereafter the cycle parking facilities will be retained and used for no other purposes.

Reason:

In order to ensure satisfactory safe and secure cycle parking facilities are provided and retained in order to encourage the use of cycling as an alternative means of transport to the development and reduce reliance on the use of the private car in accordance with strategic policy 2 of the Core Strategy 2011 and saved policy 3.5 Walking and Cycling of the Southwark Plan 2007.

10

Car Parking – Vehicle Charging Points

Prior to occupation, details of the installation (including the location and type) of the electric vehicle charger points within the car park area shall be submitted to and approved in writing by the Local Planning Authority. The electric vehicle charger points shall be installed and retained thereafter and the development shall not be carried out other than in accordance with any such approval.

Reason:

To encourage more sustainable travel in accordance with saved policies 3.1 Environmental Effects and 5.2 Transport Impacts of the Southwark Plan 2007 and Strategic Policy 2 Sustainable Transport of the Core Strategy 2011.

11

Service and Delivery Management

All service and delivery activity will be managed in accordance with the approved Service and Delivery Management Plan (Appendix D of the Transport Assessment May 2012).

Reason:

To ensure compliance with Strategic Policy 2 – Sustainable Transport of the Core Strategy 2011 and saved policy 5.2 Transport Impacts of the Southwark Plan 2007.

12

BREEAM

Before occupation, a certified Post Construction Review (or other verification process agreed with the local planning authority) as relevant to that element of the building, shall be submitted to and approved in writing by the Local Planning Authority confirming that BREEAM Excellent has been achieved.

Reason:

To ensure the proposals comply with saved policies 3.3 sustainability and 3.4 Energy Efficiency of the Southwark Plan 2007 and Strategic Policy 13 – High Environmental Standards of the Core Strategy 2011.

13

Green/brown roof/ living walls/ vertical gardens and planters

Before any above grade work hereby authorised begins, details (including a specification and maintenance plan) of the green/brown roof/ living walls/ vertical gardens and planters to be used in the carrying out of this permission shall be submitted to and approved in writing by the Local Planning Authority. The development shall not be carried out otherwise than in accordance with any such approval given, and the green/brown roof/ living walls/ vertical gardens and planters are to be retained for the duration of the use. Where trees and large shrubs are proposed to be provided within planters the soil volume shall be a minimum of 4 cubic metres per tree and 1 cubic metre per shrub or climbing plant. All planters are to provide a minimum internal soil height of 1m height. Where these are at ground level planters shall have their bottoms open to native soil beneath so that roots may naturally colonise and exploit such soil. Details of irrigation shall be provided such that water is available for the maintenance of all planters by mains, grey water or other sustainable drainage specification such as attenuation tanks.

Reason

To ensure the proposed development will preserve and enhance the visual amenities of the locality and is designed for the maximum benefit of screening, local biodiversity and adaptation to climate change, in accordance with NPPF Parts 7, 8, 11 & 12 and policies of The Core Strategy 2011: SP11 Open spaces and wildlife; SP12 Design and conservation; SP13 High environmental standards. and Saved Policies of The Southwark Plan 2007: Policy 3.13 Urban Design; Policy 3.2 Protection of amenity; Policy 3.28 Biodiversity.

14 Lighting

Details of the external lighting scheme for the relevant building shall be submitted and approved by the Local Planning Authority prior to occupation and maintained thereafter.

Reason:

To ensure an adequate level of amenity of surrounding local residents by reason of pollution in accordance with saved policy 3.2 Protection of Amenity of the Southwark Plan 2007.

15 Signage

A signage strategy will be submitted to and approved by the Local Planning Authority prior to occupation.

Reason:

In order that the LPA may be satisfied with the design and details in the interest of the special architectural qualities of the proposed buildings and the public spaces around it in accordance with saved policies 3.12 Quality in Design; 3.13 Urban Design of the Southwark Plan 2007 and Strategic Policy 12 Design and Conservation of the Core Strategy (2011).

16 Archaeological Evaluation

Notwithstanding references to archaeological work contained within the Lang O Rourke Outline Construction Method Statement the archaeological evaluation shall be undertaken as detailed in the Written Scheme of Investigation for an archaeological Evaluation Museum of London Archaeology dated 12 June 2012.

Reason:

In order that the applicants supply the necessary archaeological information to ensure suitable mitigation measures and/or foundation design proposals be presented in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

17 Archaeological Mitigation

Notwithstanding references to archaeological work contained within the Lang O Rourke Outline Construction Method Statement before any work hereby authorised begins, excepting demolition, the applicant shall submit a written scheme of investigation for a programme of archaeological recording, which shall be approved in writing by the Local Planning Authority and implemented and shall not be carried out other than in accordance with any such approval given.

Reason:

In order that the details of the programme of archaeological excavation and recording works are suitable with regard to the impacts of the proposed development and the nature and extent of archaeological remains on site in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

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TP(Permit)

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TOWN AND COUNTRY PLANNING ACT 1990 (as amended)



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PLANNING PERMISSION WITH LEGAL AGREEMENT**LBS Reg. No. 12/AP/2062****Date of Issue of this decision 31/01/2013****18 Archaeological Foundation Design**

Before any work hereby authorised begins, a detailed scheme showing the complete scope and arrangement of the foundation design and all ground works shall be submitted to and approved in writing by the Local Planning Authority and the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that details of the foundations, ground works and all below ground impacts of the proposed development are detailed and accord with the programme of archaeological mitigation works to ensure the preservation of archaeological remains by record and in situ in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

19 Archaeological Reporting

Within six months of the completion of archaeological site works, an interim archaeological report shall be submitted covering works both within and without the scheduled monument. A full assessment report detailing the proposals for post-excavation works, publication of the site and preparation of the archive shall be submitted to and approved in writing by the Local Planning Authority within one year of the completion of archaeological sites works, and that the works detailed in this assessment report shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the archaeological interests of the site are secured with regard to the details of the post-excavation works, publication and archiving to ensure the preservation of archaeological remains by record in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

20 Landscaping plan

Before any above grade work hereby authorised begins, detailed drawings scale 1:50 of a hard and soft landscaping scheme showing the treatment of all parts of the site not covered by buildings (including surfacing materials of any parking, access, or pathways layouts, materials and edge details and material samples of hard landscaping), shall be submitted to and approved in writing by the Local Planning Authority. The landscaping shall not be carried out otherwise than in accordance with any such approval given and shall be retained for the duration of the use. The planting, seeding and/or turfing shall be carried out in the first planting season following completion of building works and any trees or shrubs that is found to be dead, dying, severely damaged or diseased within five years of the completion of the building works OR five years of the carrying out of the landscaping scheme (whichever is later), shall be replaced in the next planting season by specimens of similar size and species in the first suitable planting season. Planting shall comply to BS: 4428 Code of practice for general landscaping operations, BS: 3996 Nursery stock specification, BS: 5837 Trees in relation to construction and BS: 7370 Recommendations for establishing and managing grounds maintenance organisations and for design considerations related to maintenance.

Reason

To ensure the proposed development will preserve and enhance the visual amenities of the locality and is designed for the maximum benefit of screening, local biodiversity, adaptation to climate change, in addition to the attenuation of surface water runoff, in accordance with NPPF Parts 7, 8, 11 & 12, London Plan 2011 Policy 2.18 Green infrastructure; Policy 5.1 Climate change mitigation; Policy 5.10 Urban greening, Policy 5.11 Green roofs and development site environs; Policy 5.13 Sustainable drainage; Policy 6.1 Strategic approach; Policy 7.4 Local character; Policy 7.21 Trees and woodlands; The London Climate Change Adaption Strategy Action 19 and policies of The Core Strategy 2011: SP11 Open spaces and wildlife; SP12 Design and conservation; SP13 High environmental standards and Saved Policies of the Southwark Plan 2007: Policy 3.13 Urban Design; Policy 3.2 Protection of amenity; Policy 3.28 Biodiversity.

21 Tree planting

No works or development shall take place above grade until full details of all proposed tree planting or transplanting, and the proposed times of planting, have been approved in writing by the local planning authority. This will include planting and maintenance specifications, use of guards or other protective measures and confirmation of location, species and sizes, nursery stock type, supplier and defect period. All tree planting shall be carried out in accordance with those details and at those times. Planting shall comply with BS: 4428 Code of practice for general landscaping operations.

If within a period of five years from the date of the planting of any tree that tree, or any tree planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree of the same species and size as that originally planted shall be planted at the same place, unless the local planning authority gives its written consent to any variation.

Reason

In order that the Local Planning Authority may be satisfied as to the design and details in the interest of the special architectural qualities of the existing building and the public spaces around it in accordance with Policies 3.12 Quality in Design, 3.13 Urban Design and 3.28 Biodiversity of The Southwark Plan 2007.

22 Service and Delivery Management

All service and delivery activity will be managed in accordance with the approved Service and Delivery Management Plan (June 2012).

Reason:

To ensure compliance with Strategic Policy 2 – Sustainable Transport of the Core Strategy 2011 and saved policy 5.2 Transport Impacts of the Southwark Plan 2007.

23 Noise creep from noise and/or vibration generation – commercial plant.

The noise level from any plant (e.g. refrigeration, air conditioning), together with any associated ducting, shall be 10(A) dB or more below the lowest measured external ambient L_{Aeq, T^*} at the site boundary. The equipment shall be installed and constructed in accordance with any approved scheme and be permanently maintained thereafter.

Within one month of the installation of the plant and equipment, you are required to submit a further noise report confirming previous details and subsequent measurement data of the installed plant to demonstrate compliance with the above requirements. The supplementary acoustic report must include:

- i) A schedule of all plant and equipment installed;
- ii) Locations of the plant and machinery and associated: ducting; attenuation and damping equipment;
- iii) Manufacturer specifications of sound emissions in octave or third octave detail;
- iv) Distances between plant & equipment and receptor location/s and any mitigating features that may attenuate the sound level received at the most affected receptor location;
- i) The lowest existing $L_{Aeq, T}$ measurement as already established.
- ii) New noise monitoring data, measurement evidence and any calculations demonstrating that plant complies with the planning condition.

* $L_{Aeq, T}$. $T = 1$ hr between 07:00 and 23:00 and 5min between 23:00 and 07:00.

Reason

To ensure that users of the surrounding area not suffer a loss of amenity by reason of noise nuisance and other excess noise from plant and that the operation of plant does not add by cumulative effect to the existing sound environment in accordance with saved policies 3.1 'Environmental Effects' and 3.2 'Protection of Amenity' of the Southwark Plan 2007.

24 Ventilation

Prior to occupation, details relating to ventilation of the building as a whole and any specific ventilation for laboratories or other 'polluting activities', including positioning of fresh air intake and relationship to extract from the building and adjacent building within the complex shall be submitted and approved by the local planning authority.

Reason

To ensure that users of the surrounding area not suffer a loss of amenity by reason of odour and emissions in

accordance with saved policies 3.1 'Environmental Effects' and 3.2 'Protection of Amenity' of the Southwark Plan 2007.

25 Environment Agency - flooding

The development permitted by this planning permission shall only be carried out in accordance with the approved Flood Risk Assessment (FRA) by Arup (dated 11 June 2012 with reference ARUP-XXX-RP-0006 P03) and the following mitigation measures detailed within the approved FRA:

- with regards to the basement level, the proposed electrical plant should benefit from appropriate flood resistant and resilient measures;
- only 'less vulnerable' uses should be accommodated on the ground floor;
- safe refuge on the first floor and above should be made available at all times, with escape routes within the building provided from every floor;
- a drainage strategy should be finalized and agreed with the London Borough of Southwark before planning permission is granted, incorporating the use of Sustainable Drainage Systems (SuDS) wherever possible.

Reasons

To reduce the impact of flooding to the proposed development, future occupants and to attenuate surface water flows and prevent flooding by ensuring satisfactory and sustainable storage or disposal of surface water from the site, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

26 Environment Agency - contamination

If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, a remediation strategy detailing how this unsuspected contamination shall be dealt with. The remediation strategy shall be implemented as approved and reported to the satisfaction of the Local Planning Authority.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

27 Environment Agency - contamination

Prior to occupation of the development, a verification report demonstrating completion of the works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to and approved, in writing, by the Local Planning Authority. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met. It shall also include a plan (a 'long-term monitoring and maintenance plan') for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action, as identified in the verification plan, if appropriate, and for the reporting of this to the Local Planning Authority. Any long-term monitoring and maintenance plan shall be implemented as approved.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

28 Environment Agency - contamination

Piling or any other foundation designs using penetrative methods shall not be permitted other than with the express written consent of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

Continued overleaf...

TP(Permit)

SOUTHWARK COUNCIL

TOWN AND COUNTRY PLANNING ACT 1990 (as amended)



www.southwark.gov.uk

PLANNING PERMISSION WITH LEGAL AGREEMENT**LBS Reg. No. 12/AP/2062****Date of Issue of this decision 31/01/2013****29 Environment Agency - SUDS**

Whilst the principles and installation of Sustainable Drainage Schemes (SuDS) are to be encouraged, no infiltration of surface water drainage into the ground is permitted other than with the express written consent of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to Controlled Waters. The development shall be carried out in accordance with the approved details.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

30 Submission of further details - detailed drawings

Notwithstanding the drawings hereby approved, detailed drawings of the rooftop plant enclosure (including lift over-runs) shall be submitted and approved prior to works commencing above grade.

Reason:

In order that the Local Planning Authority may be satisfied that the design of this prominent element is of the highest standard in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; 3.20 Tall Buildings of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

31 Submission of further details - detailed drawings

Notwithstanding the drawings hereby approved, detailed drawings (including typical storey-bays in elevation at 1:20 and 1:5) of the vertical solid panel elements in Snowfields and Great Maze Pond shall be submitted and approved prior to works commencing above grade.

Reason:

In order that the Local Planning Authority may be satisfied that the design of these prominent elements is of the highest standard in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

Statement of positive and proactive action in dealing with the application

To assist applicants the Local Planning Authority has produced policies and written guidance, all of which is available on the Council's website and offers a pre planning application advice service. The application was determined in a timely manner.

Continued overleaf...

TP(Permit)

SOUTHWARK COUNCIL

TOWN AND COUNTRY PLANNING ACT 1990 (as amended)

www.southwark.gov.uk**PLANNING PERMISSION WITH LEGAL AGREEMENT****LBS Reg. No. 12/AP/2062****Date of Issue of this decision 31/01/2013**Signed *Gary Rice*

Head of Development Management

Your attention is drawn to the notes accompanying this document

Any enquiries regarding this document should quote the LBS Registered Number and be sent to the Head of Development Management, Southwark Council, Chief executive's department, Planning division, Development management, PO Box 64529, London SE1 5LX, or by email to planning.applications@southwark.gov.uk

UPRN: 10000816532

checked by _____ TP/151-A

PLANNING PERMISSION WITH LEGAL AGREEMENT

LBS Registered Number: 12/AP/2062

Date of issue of this decision: 31/01/2013

www.southwark.gov.uk

INFORMATIVE NOTES TO APPLICANT RELATING TO THE PROPOSED DEVELOPMENT

- 1 The planning permission granted includes alterations and amendments to areas of the public highway, which will need to be funded by the developer. Although these works are approved in principle by the Highway Authority, no permission is hereby granted to carry out these works until all necessary and appropriate design details have been submitted and agreed. You are advised to contact the Principal Engineer, Infrastructure Group (020 7525 5509), at least 4 months prior to any works commencing on the public highway.
- 2 At least 6 months before the occupation of the new commercial units hereby permitted you are advised that you must obtain the Council's approval for the numbering and naming of buildings and the naming of any new streets created by the development.
- 3 The details and/or samples required by the Condition(s) above must be accompanied by a letter stating:
 1. the LBS Reference Number which appears at the top of this decision notice;
 2. the full address of the application site;
 3. which condition(s) you seek to discharge; and
 4. a list of all drawing numbers/ sample name and manufacturer, together with the condition(s) they relate to.All samples submitted must be clearly labelled with the LBS Reference Number of the original application and the address of the application site.

Please note that the approval of details are subject to the same eight week timeframe as a full planning permission.
- 4 The Archaeology Officer can, on request, provide an archaeological brief detailing the methodology of the archaeological programme and can also provide information concerning archaeological organisations who work frequently within the Borough and who may be able to carry out the works.

PLANNING PERMISSION WITH LEGAL AGREEMENT

LBS Registered Number: 12/AP/2062

Date of issue of this decision: 31/01/2013

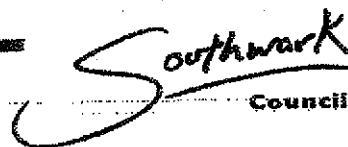


www.southwark.gov.uk

IMPORTANT NOTES RELATING TO THE COUNCIL'S DECISION

- [1] **APPEAL TO THE SECRETARY OF STATE.** If you are aggrieved by this decision of the council as the local planning authority to grant permission subject to conditions you can appeal to the Secretary of State under Section 78 of the Town and Country Planning Act 1990. If you appeal you must do so within six months of the date of this notice. The Secretary of State can allow a longer period for giving notice of an appeal but will not normally use this power unless there are special circumstances which excuse the delay in giving notice of appeal. The Secretary of State need not consider an appeal if it seems that the local planning authority could not have granted it without the conditions imposed, having regard to the statutory requirements, to the provisions of any development order and to any directions given under a development order. If you do decide to appeal you can do so using The Planning Inspectorate's online appeals service. You can find the service through the appeals area of the Planning Portal at www.planningportal.gov.uk/pcs. You can also appeal by completing the appropriate form which you can get from The Planning Inspectorate, Customer Support Unit, Temple Quay House, 2 The Square, Temple Quay, Bristol BS1 6PN [tel. 0117-3726372]. The form can also be downloaded from the Inspectorate's website at www.planning-inspectorate.gov.uk. The Planning Inspectorate will publish details of your appeal on the internet on the appeals area of the Planning Portal. This may include a copy of the original planning application form and relevant supporting documents supplied to the council by you or your agent, together with the completed appeal form and information you submit to The Planning Inspectorate. Please ensure that you only provide information, including personal information belonging to you, that you are happy will be made available to others in this way. If you supply information belonging to someone else please ensure you have their permission to do so. More detailed information about data protection and privacy matters is available on the Planning Portal.
- [2] **PURCHASE NOTICE.** If either the local planning authority or the Secretary of State grants permission subject to conditions, the owner may claim that the land can neither be put to a reasonably beneficial use in its existing state nor made capable of reasonably beneficial use by the carrying out of any development which has been or would be permitted. In these circumstances the owner may serve a purchase notice on the Council requiring the Council to purchase the owner's interest in the land in accordance with Part VI of the Town and Country Planning Act 1990.
- [3] **PROVISIONS FOR THE BENEFIT OF THE DISABLED.** Applicants are reminded that account needs to be taken of the statutory requirements of the Disability Discrimination Act 1995 to provide access and facilities for disabled people where planning permission is granted for any development which provides:
- (i) Buildings or premises to which the public are to be admitted whether on payment or otherwise. [Part III of the Act].
 - (ii) Premises in which people are employed to work as covered by the Health and Safety etc At Work Act 1974 and the Management of Health and Safety at Work Regulations as amended 1999. [Part II of the Act].
 - (iii) Premises to be used as a university, university college or college, school or hall of a university, or intended as an institution under the terms of the Further and Higher Education Act 1992. [Part IV of the Act].
- Attention is also drawn to British Standard 8300:2001 Disability Access, Access for disabled people to schools buildings – a management and design guide. Building Bulletin 91 (DfEE 99) and Approved Document M (Access to and use of buildings) of the Building Regulations 2000 or any such prescribed replacement.
- [4] **OTHER APPROVALS REQUIRED PRIOR TO THE IMPLEMENTATION OF PLANNING PERMISSION.** The granting of planning permission does not relieve the developer of the necessity for complying with any Local Acts, regulations, building by-laws and general statutory provisions in force in the area, or allow them to modify or affect any personal or restrictive covenants, easements, etc., applying to or affecting either the land to which the permission relates or any other land or the rights of any persons or authorities [including the London Borough of Southwark] entitled to the benefits thereof or holding an interest in the property concerned in the development permitted or in any adjoining property.
- [5] **WORKS AFFECTING THE PUBLIC HIGHWAY.** You are advised to consult the council's Highway Maintenance section [tel. 020-7525-2000] about any proposed works to, above or under any road, footway or forecourt.
- [6] **THE DULWICH ESTATE SCHEME OF MANAGEMENT.** Development of sites within the area covered by the Scheme of Management may also require the permission of the Dulwich Estate. If your property is in the Dulwich area with a post code of SE19, 21, 22, 24 or 26 you are advised to consult the Estates Governors', The Old College, Gallery Road SE21 7AE [tel: 020-8299-1000].
- [7] **BUILDING REGULATIONS.** You are advised to consult Southwark Building Control at the earliest possible moment to ascertain whether your proposal will require consent under the Building Act 1984 [as amended], Building Regulations 2000 [as amended], the London Building Acts or other statutes. A Building Control officer will advise as to the submission of any necessary applications, [tel. call centre number 0845 600 1285].
- [8] **THE PARTY WALL Etc. ACT 1996.** You are advised that you must notify all affected neighbours of work to an existing wall or floor/ceiling shared with another property, a new building on a boundary with neighbouring property or excavation near a

151-A



DATED

31st January

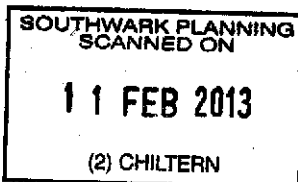
2013

THE MAYOR AND BURGESSES OF
THE LONDON BOROUGH OF SOUTHWARK

- AND -

GUY'S AND ST THOMAS'S NHS FOUNDATION TRUST

PLANNING OBLIGATIONS BY AGREEMENT
PURSUANT TO SECTION 106 OF
THE TOWN AND COUNTRY PLANNING ACT 1990 AND OTHER POWERS
IN RESPECT OF LAND KNOWN AS
GUY'S CANCER TREATMENT CENTRE,
GUYS HOSPITAL, GREAT MAZE POND, LONDON SE1 9RT



Doreen Forrester-Brown
Director of Legal Services
London Borough of Southwark
PO Box 64529
London SE1 5LX

Ref: LEG/RP/PL/S106/00063412(SY)/12-AP-2062

THIS DEED is made the
thousand and thirteen

31st day of *January* *Two*

BETWEEN

- (1) **THE MAYOR AND BURGESSES OF THE LONDON BOROUGH OF SOUTHWARK** of the Town Hall Peckham Road London SE5 8UB ("the Council"); AND
- (2) **GUY'S AND ST THOMAS'S NHS FOUNDATION TRUST** of Guy's Hospital, St Thomas Street, London, SE1 9RT ("The Developer")

WHEREAS:

- (A) The Developer is the freehold owner of the Site.
- (B) The Council is the local planning authority by whom the obligations contained in this Deed are enforceable.
- (C) The Developer wishes to construct the Development in accordance with the Planning Permission.
- (D) The obligations contained in this Deed are planning obligations for the purposes of Section 106 of the Town and Country Planning Act 1990 which may be enforced against the Site and the Developer by the Council.
- (E) The Developer acknowledges that the site specific transport and public realm contributions would have been contributions payable to the Council in accordance with the Council's adopted Section 106 Obligations Supplementary Planning Document, July 2008 towards mitigating the need for site related transport and public realm improvements by the Council. In lieu of such contributions the parties agree that the Developer shall carry out the Highways Works in order to mitigate the adverse impacts of the Development upon transport and public realm in the vicinity of the Site.
- (F) Initial archaeological investigations discovered the presence of a Roman boat on the Site classified as a Scheduled Ancient Monument. On 3rd September 2012, English Heritage granted Scheduled Ancient Monument Consent subject to various conditions one of which requires an agreement under Section 17 of the Ancient Monuments and Archaeological Areas Act 1979 to be completed prior to Demolition.
- (G) Having regard to the provisions of the development plan and the planning considerations affecting the Site, the Council considers that in the interests of the proper planning of its area the Development of the Site ought only be permitted in accordance with the Planning Permissions and subject to the

terms hereof and for that purpose the Developer is willing to enter into this Deed so as to bind the Site and any successors in title and assigns from time to time.

NOW THIS DEED WITNESSETH:

1. Definitions and Interpretation
 - 1.1 The following words and phrases shall have unless the context otherwise requires bear the following meanings:

"1990 Act"	The Town and Country Planning Act 1990 and any statutory amendment, variation, substitution or re-enactment thereof together with all other powers enabling;
"Acts"	Section 278 of the Highways Act 1980, Section 27 of the Greater London Council (General Powers) Act 1969, Section 16 of the Greater London Council (General Power) Act 1974, Section 2 of the Local Government Act 2000 and Sections 111 120 and 123 of the Local Government Act 1972 and in each case any statutory amendment, variation, substitution or re-enactment thereof together with all other powers enabling;
"Administration Contribution"	The sum of £13,340.80 Index Linked to be paid by the Developer to the Council for the reasonable costs incurred by the Council in administering this Deed including maintenance of financial records, monitoring the progress of the Development including receipt of payments made and expended and applied, and monitoring compliance with the terms of this Deed;
"Application"	The application for planning permission submitted by the Developer to the Council and registered by the Council on 13 July 2012 to carry out the Development upon the Site with LBS registered number 12-AP-2062;
"1st Archaeology Contribution"	The sum of £5,261 Index Linked to be paid by the Developer to the Council in accordance with Paragraph 1 of Schedule 3;
"2nd Archaeology Contribution"	The sum of £50,000 Index Linked to be paid (if applicable) by the Developer to the Council in accordance with paragraph 3.4.3 of Schedule 2;
"Borough"	The London Borough of Southwark;

"Contributions"

Public Open Space Contribution, Strategic Transport Contribution, WPC Contribution (if relevant), Off Site Carbon Reduction Contribution and WPC Management Contribution;

"Council"

The London Borough of Southwark the part of the first part hereto in its statutory capacity as the local planning authority for the Borough and any statutory successor body;

"Demolition"

The taking down of the existing buildings on the Site or any substantial part thereof but excludes inter alia the removal of doors, flooring, fixtures, services and temporary structures including room partitions and 'Demolish' shall be construed accordingly;

"Developer"

The party of the second part hereto including any successors and assigns from time to time in existence;

"Development"

Demolition of existing buildings on the corner of Great Maze Pond and Snowfields and erection of a 14 storey building for a Cancer Treatment Centre (with an additional 2 storeys of roof plant) 71 metres in height and 29,000sqm floor area, with preservation in situ of a Scheduled Ancient Monument (Roman Boat), public realm works, disabled parking, cycle parking facilities and basement link to hospital campus pursuant to and in accordance with the Planning Permission;

"Director of Planning"

The Council's Director of Regeneration and Neighbourhoods or any other officer or person properly exercising the authority of the Director of Regeneration and Neighbourhoods for the time being;

"Dispose"

The occurrence of any of the following after the date of Demolition;

- (i) the grant and commencement of any lease or any other lease or tenancy in respect of the Site or any floorspace comprised in the Site; and / or
- (ii) the completion of an assignment of a lease or tenancy of the Site or in respect of any floorspace comprised in the Site, and / or

the terms "Disposal" shall be construed accordingly and the date upon which any of the above events occur shall be deemed the Disposal Date;

"Highways Agreement(s)"

Any agreement(s) between the Developer and the Highways Authority pursuant to section 278 or Section 38 (in respect of the Adoption Works) of the Highways Act 1980 for securing and authorising the Developer to carry out the Highway Works and (unless otherwise agreed between Developer and the Council) the Highways Agreement(s) will include (without limitation) provisions for: -

- (a) the relevant Highways Works to be secured in the sum of the Highways Bond(s);
- (b) the Developer to carry out the Highways Works in accordance with the approved specification(s) (or any relevant part thereof) at its own cost and at no cost to the Council; and
- (c) the security or Highways Bond(s) relating to the amount of the relevant Highway Works to be delivered prior to the commencement of such works and no later than the date of the Highways Agreement(s); and
- (d) the works shall be designed by the Developer to the value of £839,070.00 Index Linked

FOR THE AVOIDANCE OF DOUBT nothing in this Agreement shall preclude the Developer from entering into one or more Highways Agreement subject to ensuring the design compatibility of any Highways Agreements (if more than one);

"Highways Bond(s)"

The deposit, bond, guarantee, surety or similar security relating to the Highway Works: -

- (i) in a sum equivalent to the cost(s) of the Highway Works plus ten percent Index Linked to be agreed with the Council pursuant to paragraph 4 of Schedule 2 to this Deed, and
- (ii) to be procured by the Developer from a reputable financial institution pursuant to and at the date of completion of the Highways Agreement(s) is completed;

"Highways Works Specification"

A specification in respect of the Highway Works including (but without limitation) detailed and scaled plans, drawings, full specification of the works, costs, specification and samples of proposed materials, the phasing and timing for the delivery of the Highway Works and any other details as may be reasonably required by the Council;

"Highway Works"

The highway works to be provided in lieu of site specific transport and public realm contributions which shall be designed to the value of £839,070.00 and which shall include (but without limitation) the following works which are shown for indicative purposes on Plan 1: -

- (i) Pedestrian crossing on and all works, consents and processes necessary to convert Snowfields to a one way system including appropriate markings and signage;
- (ii) Repaving of the kerb edge of Snowfields (being a public highway maintainable at public expense) along its boundary with the Site within the area shown hatched blue on Plan 1;
- (iii) Carriageway resurfacing works to that area of the carriageway being Snowfield and comprised within the red line application site boundary shown on Plan 1;
- (iv) Replacement street furniture including waste disposal bins and street lighting at locations to be approved;
- (v) The creation of parking spaces by line markings on and such other works as shall be necessary to introduce the line marked parking spaces or obtaining all necessary consents which facilitate parking, waiting and loading restrictions along Crosby Row;
- (vi) Repaving, landscaping and tree planting on the footway along the south and north side of Snowfields between Kipling Street and Crosby Row more particularly shown

hatched blue on Plan 1;

- (vii) Carrying out all the above works to adoptable standards (as specified by the Council as highway authority) and in particular the dedication of so much of the area shown for illustrative purposes only hatched blue (or otherwise agreed between the Parties) and shown on Plan 2 as shall be necessary as highway maintainable at public expense ("the Adoption Works");

full details of which shall be submitted by the Developer for the approval of the Council (as local planning authority and highway authority) before entering into the Highway Agreement(s);

"Implementation Date"

The date upon which a material operation as defined in section 56(4) of the 1990 Act shall be first carried out save that a material operation shall not include operations in connection with any archaeological investigations works of excavation demolition site clearance diversion of services installation of services for construction purposes only site or soil investigations remedial action in respect of any contamination landscaping works provision of construction access landscaping works noise attenuation works or the erection of hoardings and fences in respect of the Development upon the Site and references to "Implementation" and "Implement" shall be construed accordingly;

"Index"

The Index means: -

- (i) in relation to the Contributions, the RPIX all items excluding mortgage interest (RPIX) published by the Office for National Statistics or the BCIS General Building Cost Index - published monthly by the Building Cost Information Service (as the case may be) to be applied in accordance with Clause 15, and
- (ii) in relation to the Highways Works, the Road Construction Tender Price Index issued by the Department for Transport to be applied in accordance

with Clause 15;

- "Index-Linked"** The increase in value of any of the Contribution(s) based on the Index to be calculated and payable in accordance with Clause 15 of this Deed and for the avoidance of doubt each of the Contributions payable in accordance with this Deed will be Index-Linked accordingly;
- "Local Employment and Skills Agencies"** Local and employment skills agencies such as the Bosco Centre and Construction Related Skills;
- "London Plan"** The London Plan Spatial Development Strategy for Greater London Consolidated with Alterations since 2004;
- "Material Operation"** A material operation as defined in section 56 of the Town and Country Planning Act 1990 carried out in respect of the Development pursuant to the Planning Permission details of which including any certified documentary evidence shall be provided by the Developer to the Council in writing and the first date upon which a material operation occurs shall be construed as the "Material Operation Date";
- "Occupation Date"** The first date upon which any part of the Site is occupied for any purpose pursuant to or in connection with the Planning Permission but does not include occupation by personnel engaged in construction, fitting out or decoration or occupation for marketing or display or operations in relation to security operations and for the avoidance of doubt in the case of the sale of the freehold or leasehold interest to a third party or any other Disposal occupation commences on the Disposal Date and the phrases "Occupy" "Occupied" shall be construed accordingly;
- "Off Site Carbon Reduction Contribution"** The sum of £38,640 Index Linked towards the carrying out of off-site carbon reduction projects in the Borough to be paid by the Developer to the Council in accordance with paragraph 1.4 of Schedule 3;
- "Parking Bay"** A parking place designated by the Council by an order under the Road Traffic Regulation Act 1984 or other relevant legislation for use in the locality in which the Development is situated;
- "Plan"** The plan of the Site annexed hereto;

"Plan 1"	The Indicative plan showing the area within which the Highways Works are to be carried out drawing number RSHP-XX-SK-0223 annexed hereto;
"Plan 2 "	The plan showing the Adoption Works drawing number RSHP-XX-SK-0226 annexed hereto;
"Plan 3"	The plan showing the Protected Area drawing number RSHP - XX-SK-0225 annexed hereto;
"Planning Permission"	The planning permission for the Development to be Issued by the Council pursuant to the Application substantially in the form annexed to Schedule 1 of this Deed;
"Practical Completion Date"	The date of issue of a certificate of practical completion by the Developer's architect or engineer or surveyor or in the event that the Development is constructed by a party other than the Developer the issue of a certificate of practical completion by that other party's architect or engineer or surveyor;
"Protected Area"	The area of land shown on Plan 3 hatched blue to be protected in order to facilitate the excavation of the SAM (if necessary) and annexed hereto;
"Public Open Space Contribution"	The sum of £254,974 Index Linked towards the creation of new Council maintained public space or the improvement of existing Council maintained public space in the vicinity of the Site to be paid by the Developer to the Council in accordance with paragraph 1.4 of Schedule 3;
"Service Improvement Development Manager"	the role of the Service Improvement Development Manager within the Trust is to lead on providing a high quality workforce working with the local community to provide job opportunities, training and apprenticeships or any successor to those functions
"Section 17 Agreement"	An agreement under section 17 of the Ancient Monuments and Archaeological Areas Act 1979 to be entered into by the Historic Building and Monuments Commission for England and the Developer;
"Significant Under-Performance"	The failure of the Developer to deliver 50% of the outputs specified in paragraph 1.2.2 to 1.2.3 of Schedule 2;

"Site"	The land known as Guy's Hospital, Great Maze Pond, London SE1 9RT which is shown for the purpose of identification only shown edged red on the Plan and which forms part of the registered title with the Land Registry under title numbers SGL167805 and TGL312982 and against which the obligations in this Deed may be enforced by the Council;
"Site and Development Facilities"	Facilities to be provided through the use of the Public Open Space Contribution, Strategic Transport Contribution and WPC Management Contribution;
"Southwark Plan"	The Core Strategy April 2011, the Canada Water AAP January 2011 and Saved Policies of the Southwark Plan 2007;
"Strategic Transport Contribution"	The sum of £406,805 Index Linked to be paid by the Developer towards strategic transport improvements to be used for either strategic transport improvements as set out in the Council's Transport Plan (or other successor or related documents making provision for strategic transport infrastructure improvements) or such other appropriate strategic transport related projects as may be agreed between the parties;
"Travel Plan"	A travel plan to be submitted by the Developer substantially in the form of the draft appended hereto at Appendix 1 to be approved by the Council in writing and which shall take effect upon Occupation and such variations thereof as may be agreed between the parties from time to time following reviews in accordance with paragraph 2 of Schedule 2;
"Working Days"	Any day of the week excluding Saturdays, Sundays and Bank Holidays;
"Workplace Co-ordinator (WPC)"	An employee provided by the Developer to perform a brokering role between the construction contractors on-site at the Development and unemployed local jobseekers seeking training, apprenticeships and employment during the construction phase of the Development as set out in paragraph 1.2.2 and 1.2.3 of Schedule 2;
"WPC Contribution"	The payment of £428,923 (thirty three thousand seven hundred and eighty seven pounds) Index Linked for the provision of a Workplace Co-ordinator during the

construction of the Development payable in accordance with paragraph 1.3 of Schedule 2 of this Deed only in the circumstances set out in paragraph 1.4 of Schedule 2;

"WPC Management Contribution" The sum of £34,778 Index Linked towards the programme management and monitoring costs of the workplace coordinator scheme;

In this Deed:

- 1.2 Where in this Deed reference is made to a clause, paragraph, schedule or plan it is to a clause, paragraph, schedule or plan in this Deed.
- 1.3 Headings used in this Deed are an aid to interpretation only and do not form part of this Deed.
- 1.4 A reference to any statute or statutory provision shall be construed as a reference to the same as it may from time to time be amended, modified or re-enacted.

2. Statutory Provisions

2.1 This Deed is made pursuant to section 106 of the 1990 Act and contains planning obligations which bind the Site and to the extent that they fall within the terms of section 106 of the 1990 Act, the obligations contained in this Deed are planning obligations for the purposes of section 106 of the 1990 Act and are enforceable by the Council, the restrictive covenants and Deeds herein on the part of the Developer are entered into with the intent that subject to Clause 6 the same shall be enforceable without limit of time not only against the Developer but also against its successors in title and assigns and any person corporate or otherwise claiming through or under the Developer an interest or estate created hereafter in the Site or any part or parts thereof as if that person had also been an original covenanting party in respect of such of the covenants and Deeds which relate to the interest or estate for the time being held by that person.

2.2 To the extent only that any of the obligations contained in this Deed are not planning obligations within the meaning of the 1990 Act, they are entered into pursuant to the powers contained in the Acts.

3. Legal Effect

3.1 This Deed is conditional upon the grant of the Planning Permission.

3.2 This Deed shall come into effect on the date hereof save where otherwise provided for the purposes of specific clauses or schedules of this Deed.

3.3 Section 73 Applications

In the event that any new planning permission(s) are granted by the Council pursuant to Section 73 of the 1990 Act (as amended) and unless otherwise agreed between the parties: -

- 3.3.1 the obligations in this Deed shall relate to and bind any subsequent planning permission(s) in respect of the Site granted pursuant to Section 73 of the 1990 Act and the Site itself, and
- 3.3.2 the definitions of Application(s), Development and Planning Permission(s) in this Deed shall be construed (as the context requires) to include reference to any application under Section 73 of the 1990 Act, the planning permission(s) granted thereunder and the development permitted by such subsequent planning permission(s), and
- 3.3.3 this Deed shall be endorsed with the following words in respect of any future Section 73 application: -

"The obligations in this Agreement relate to and bind the Site in respect of which a new planning permission referenced has been granted pursuant to Section 73 of the Town and Country Planning Act 1990 (as amended)"

PROVIDED THAT nothing in this clause shall fetter the Council's discretion in determining any application(s) under Section 73 of the 1990 Act or the appropriate nature and / or quantum of Section 106 obligations in so far as they are materially different to those contained in this Deed and required pursuant to a determination under Section 73 of the 1990 Act whether by way of a new deed or supplemental deed pursuant to S106 of the 1990 Act.

4. Obligations of the Developer

- 4.1 The Developer undertakes to observe and perform or cause to be observed and performed the obligations contained in Schedules 2 and 3 at the times and in the manner provided therein.
- 4.2 Without prejudice to any other remedy available to the Council, the Developer covenants that no part of the Development shall be used or Occupied unless and until the obligations contained within Schedule 2 and 3 of this Deed which require compliance on or prior to Occupation of any part of the Development have been complied with SAVE THAT any obligations which require ongoing

compliance by the Developer shall not be construed so as to preclude Occupation.

- 4.3 The Developer shall pay the Contributions as specified in Schedules 2 and 3 by way of BACS transfer into National Westminster Bank plc Account Number 27540006 Sort Code 51-50-03 at London Bridge Branch PO Box 35, 10 Southwark Street, London SE1 1TT or such other account as the Council shall nominate.

5. **Developer to Notify Council**

- 5.1 The Developer undertakes to the Council to notify the Council in writing:

- 5.1.1 of its application to H.M. Land Registry under clause 9.1 within 14 days of this Deed;
- 5.1.2 of the anticipated Implementation Date by giving not less than fourteen days notice in advance or
- 5.1.3 in default in respect of 5.1.2 above, of the occurrence of the Implementation Date immediately;
- 5.1.4 of the date of Occupation;
- 5.1.5 of the date of payment of the Contributions in accordance with Schedule 3 of this Deed by notice in writing specifying the amount of each Contribution and method of payment, the agreement and property to which it relates.

7. **Council's Covenants**

The Council covenants with the Developer to observe and perform or cause to be observed and performed the obligations contained in Schedule 4 of this Deed.

8. **Enforceability of Obligations**

- 8.1 No person shall be liable for any breach of the restrictive covenants or positive obligations contained in this Deed occurring after it has parted with its entire interest or part thereof in the Site (but without prejudice to the liability of such person for any breach occurring prior to its parting with such interest).

9. **Registration**

- 9.1 Within 28 days after the execution of this Deed, the Developer shall make an application to the Land Registry for entries relating to this Deed to be made in

the charges register(s) of the relevant title numbers so as to bind the Site as provided for in the before-mentioned statutory provisions. The Council shall within the said 28 days give the Developer such reasonable assistance as it reasonably requires to ensure the application is effectively processed by the Land Registry.

9.2 If the Developer fails to make application as referred to in clause 8.1 above the Council shall (without prejudice to any other right) be entitled to register the Deed and recover the expenses incurred in doing so from the Developer and the Developer hereby covenants with the Council to do or concur in doing all things necessary or advantageous to enable the said entries to be made.

9.3 The Deeds on behalf of the Developer to be observed and performed under this Deed shall be treated as Local Land Charges and registered in the Register of Local Land Charges for the purposes of the Local Land Charges Act 1975.

10. **Site Not To Be Encumbered**

11. The Developer hereby undertakes to the Council that it will not encumber nor deal with the Site in any manner whereby any party hereto or successor in title may be prevented from carrying out its obligations contained herein.

12. **Right of Access**

12.1 Without prejudice to the Council's statutory rights of entry the Developer shall so far as is absolutely necessary and to the extent necessary permit the Council and its authorised employees and agents upon reasonable written notice to enter the Site at all reasonable times for the purpose of verifying whether or not any obligation arising hereunder has been performed or observed.

13. **Waiver**

13.1 No waiver (whether express or implied) by the Council of any breach or default by the Developer in performing or observing any of the covenants Deeds obligations or restrictions contained in this Deed shall constitute a continuing waiver and no such waiver shall prevent the Council from enforcing any of the said covenants Deeds obligations or restrictions from acting upon any subsequent breach or default in respect thereof by the Developer.

14. **Interest on Late Payment**

14.1 Without prejudice to any other right remedy or power herein contained or otherwise available to the Council if any payment of any sum referred to herein shall have become due but shall remain unpaid for a period exceeding seven

days the Developer shall pay on demand to the Council interest thereon at the interest rate of three per centum per annum above the base lending rate of the National Westminster Bank plc from the date when the same became due until payment thereof.

15. Indexation

15.1 Any sums referred to in this Deed as or to be applied by any party other than the Council under this Deed shall be paid or applied TOGETHER WITH if such payment or application is being made after the date of this Deed a further sum ("A") being equal to the original sum ("B") multiplied by a figure being a fraction of which the Index figure last published by the Office for National Statistics or, where any sums relate to construction costs the BCIS General Building Cost Index is the denominator ("X") and the last Index figure published before the date such payment or application is made ("Y") less the last published Index figure at the date hereof ("X") is the numerator so that

$$A = \frac{B \times (Y - X)}{X}$$

15.2 For the avoidance of doubt the following contributions will in the absence of anything to the contrary be treated as representing construction related costs for the purposes of applying the BCIS index pursuant to this clause those contributions related to Strategic Transport and Site Specific Transport.

16. Enforcement Costs

16.1 Without prejudice to the terms of any other provision herein the Developer shall pay all costs charges and expenses (including without prejudice to the generality thereof legal costs and surveyor's fees) reasonably incurred by the Council (but not for the avoidance of doubt any other third party seeking to enforce the terms of this Deed) for the purpose of or incidental to the enforcement of this Deed.

17. Council's Legal Fees

17.1 The Developer shall pay on the date of this Deed to the Council, by way of a banker's draft or solicitor's client account cheque or BACS made payable to "the London Borough of Southwark", the Council's reasonable costs in the preparation and negotiation of this Deed.

18. VAT

18.1 All consideration given in accordance with the terms of this Deed shall be exclusive of any VAT properly payable in respect thereof.

- 18.2 The Developer hereby acknowledges and agrees that if at any time VAT is required to be paid in respect of any Site and Development Contributions then to the extent that VAT had not been previously charged in respect of that contribution the Council shall have the right to issue a VAT invoice to the Developer and the VAT shall be paid accordingly.

19. **Notices**

- 19.1 Any notice or other communication to be given under or in connection with this Deed shall be in writing which for this purpose shall not include e-mail and should be addressed as provided in clause 19.3.

- 19.2 Any such notice or other communication, if so addressed, shall be deemed to have been received as follows:

19.2.1 if delivered by hand, upon delivery at the relevant address;

19.2.2 if sent by first class post, at 9.00 a.m. on the second Working Day after the date of posting; and

19.2.3 if sent by facsimile, when successfully transmitted

except that where any such notice or other communication is or would otherwise be deemed to be received after 5.30 p.m., such notice or other communication shall be deemed to be received at 9.00 a.m. on the next following Working Day.

- 19.3 Subject to clause 19.4, the address, facsimile number, relevant addressee and reference for each party referred to in this Deed are as follows:

For the COUNCIL:

Address: PO Box 64529, London SE1 5LX

Facsimile number: 02075255432

Relevant addressee: The Director of Regeneration and
Neighbourhoods

Reference: S106/ 135136/00063412(SY)/12-AP-2062

For the Developer:

Address: Essentia Asset Management, Counting House, Guys Hospital, SE1
9RT

Facsimile number: 0207 188 5348

Relevant addressee: Mr Michael Haynes

Reference: MH/Cancer Centre/ S106

19.4 Any party referred to in this Deed may give notice of a change to its name, address, facsimile number or relevant addressee for the purposes of this clause provided that such notification shall only be effective on:

19.4.1 the date specified in the notification as the date on which the change is to take place; or

19.4.2 if no date is specified or the date specified is less than five clear Working Days after the date on which notice is received or deemed to be received, the fifth Working Day after notice of any such change is given.

20. **Contracts (Rights of Third Parties) Act 1999**

20.1 A person who is not named in this Deed does not have any right to enforce any of its terms under the Contract (Rights of Third Parties) Act 1999 unless such a party is successor in title to the Developer, successor to the Mortgagee or a successor in statutory function to the Council.

21. **Miscellaneous**

21.1 The construction validity and performance of this Deed shall be governed by English law.

21.2 Each clause, sub-clause or schedule shall be separate distinct and severable from each other to the extent only that if any clause, sub-clause or schedule becomes or is invalid because of a change of circumstances or any other unforeseen reasons or if any one or more of such clause, sub-clause or schedule shall be held by the Courts to be void for any reason whatsoever but would be valid if severed or any wording was deleted or any time period reduced or scope of activities or area covered diminished then any modifications necessary to ensure such clause sub-clause schedule or paragraph be valid shall apply without prejudice to any other clause, sub-clause or schedule contained herein.

21.3 In the event of the planning obligations contained in this Deed being modified a note or memorandum thereof shall be endorsed upon this Deed.

21.4 Nothing in this Deed shall prejudice or affect the rights powers duties and obligations of the Council under private or public statutes bye-laws orders and

regulations and the same may be as fully effectively exercised as if it were not a party to this Deed.

- 21.5 If the Planning Permission shall expire before the Development has begun within the meaning of Sections 91, 92 or 93 of the 1990 Act or is revoked or is otherwise withdrawn without the consent of the Developer or its successors in title but without prejudice to the Council's ability to enforce in respect of any breach occurring prior to such revocation or withdrawal this Deed shall have no further effect thereupon.
- 21.6 Nothing in this Deed shall be construed as prohibiting or limiting any right to develop the Site or any part of it in accordance with a planning permission (other than the Planning Permission) granted by the Council or by the relevant Secretary of State on appeal or by reference to her after this date.
- 21.7 Where the Council's approval or consent is required by the Developer under this Deed the Council shall not unreasonably withhold or delay the approval or consent.

22. DETERMINATION OF DISPUTES

- 22.1 Subject to clause 22.7, if any dispute arises relating to or arising out of the terms of this Agreement, either party may give to the other written notice requiring the dispute to be determined under this clause 22. The notice is to propose an appropriate Specialist and specify the nature and substance of the dispute and the relief sought in relation to the dispute.
- 22.2 For the purposes of this clause 22 a "Specialist" is a person qualified to act as an expert in relation to the dispute having not less than ten years' professional experience in relation to developments in the nature of the Development and property in the same locality as the Site.
- 22.3 Any dispute over the type of Specialist appropriate to resolve the dispute may be referred at the request of either party to the President or next most senior available officer of the Law Society who will have the power, with the right to take such further advice as he may require, to determine the appropriate type of Specialist and to arrange his nomination under clause 22.4.
- 22.4 Any dispute over the identity of the Specialist is to be referred at the request of either party to the President or other most senior available officer of the organisation generally recognised as being responsible for the relevant type of Specialist who will have the power, with the right to take such further advice as he may require, to determine and nominate the appropriate Specialist or to arrange his nomination. If no such organisation exists, or the parties cannot agree the identity of the organisation, then the Specialist is to be nominated by the President or next most senior available officer of the Law Society.

- 22.5 The Specialist is to act as an independent expert and: -
- 22.5.1 each party may make written representations within ten working days of his appointment and will copy the written representations to the other party;
 - 22.5.2 each party is to have a further ten working days to make written comments on the other's representations and will copy the written comments to the other party;
 - 22.5.3 the Specialist is to be at liberty to call for such written evidence from the parties and to seek such legal or other expert assistance as he or she may reasonably require;
 - 22.5.4 the Specialist is not to take oral representations from the parties without giving both parties the opportunity to be present and to give evidence and to cross-examine each other;
 - 22.5.5 the Specialist is to have regard to all representations and evidence before him when making his decision, which is to be in writing, and is to give reasons for his decision; and
 - 22.5.6 the Specialist is to use all reasonable endeavours to publish his decision within 30 working days of his appointment.
- 22.6 Responsibility for the costs of referring a dispute to a Specialist under this clause 22, including costs connected with the appointment of the Specialist and the Specialist's own costs, but not the legal and other professional costs of any party in relation to a dispute, will be decided by the Specialist.
- 22.7 This clause 22 does not apply to disputes in relation to matters of law or the construction or interpretation of this Agreement which will be subject to the jurisdiction of the courts.

SCHEDULE 2

PART I

1. **EMPLOYMENT AND TRAINING - Construction Jobs - Workplace Co-ordination**
- 1.1 Prior to the Implementation Date, the Developer will appoint and thereafter retain until completion of construction of the Development a Construction Workplace Co-ordinator who will provide training and support to facilitate access to construction jobs during the construction phase of the Development.
- 1.2 The Developer will use reasonable endeavours to: -
 - 1.2.1 retain the Construction Workplace Co-ordinator role throughout the construction of the Development;
 - 1.2.2 create and ensure the role of the Construction Workplace Co-ordinator includes the following duties and endeavours to comply with the contractor related targets below during the construction phase of the Development for apprentices or suitable opportunities for jobless residents within the Borough:
 - 1.2.2.1 identify twenty three suitable employment vacancies during construction of the Development;
 - 1.2.2.2 encourage seventeen applications from suitable candidates resident in the Borough
 - 1.2.2.3 provide eighteen basic skills and site safety training to suitable candidates
 - 1.2.2.4 place a minimum of ten Borough residents into sustainable construction jobs;
 - 1.2.2.5 train a minimum of nine workless Borough residents per annum using short courses;
 - 1.2.2.6 provide a minimum of thirteen Construction Skills Certification Scheme training opportunities;
 - 1.2.2.7 provide a minimum of eight apprenticeships during the construction of the Development

TP(Permit)

SOUTHWARK COUNCIL

Council

TOWN AND COUNTRY PLANNING ACT 1990 (as amended)

www.southwark.gov.uk

PLANNING PERMISSION WITH LEGAL AGREEMENT

Applicant Guys & St Thomas' NHS Foundation Trust
Date of Issue of this decision

LBS Registered Number 12/AP/2062

Planning Permission was **GRANTED** for the following development:

Demolition of existing buildings on the corner of Great Maze Pond and Snowfields and erection of a 14 storey building for a Cancer Treatment Centre (with an additional 2 storeys of roof plant) 71 metres in height and 29,000sqm floor area, with preservation in situ of a Scheduled Ancient Monument (Roman Boat), public realm works, disabled parking, cycle parking facilities and basement link to hospital campus.

At: GUYS HOSPITAL, GREAT MAZE POND, LONDON SE1 9RT

In accordance with application received on 26/06/2012 Your Ref. No.:
and revisions/amendments received on 15/10/2012
10/09/2012

and Applicant's Drawing Nos. RSHP-00-0001-GA-00 Rev P04, RSHP-00-0010-GA-00 Rev P04, RSHP-00-0015-GA-00 Rev P04, RSHP-00-0100-GA-00 P03, RSHP-XX-0101-GA-01 P03, RSHP-XX-0102-GA-02 P03, RSHP-XX-0103-GA-RF P03, RSHP-00-0149-GA-B1 Rev P04, RSHP-01-00150-GA-00 Rev P04, RSHP-01-0151-GA-01 Rev P04, RSHP-02-0152-GA-02 Rev P04, RSHP-02-0153-GA-03 Rev P04, RSHP-02-0154-GA-04 Rev P04, RSHP-03-0155-GA-05 Rev P04, RSHP-03-0156-GA-06 Rev P04, RSHP-04-0157-GA-07 Rev P04, RSHP-04-0158-GA-08 Rev P04, RSHP-04-0159-GA-09 Rev P04, RSHP-05-0160-GA-10 Rev P04, RSHP-05-0161-GA-11 Rev P04, RSHP-05-0162-GA-12 Rev P04, RSHP-05-0163-GA-13 Rev P04, RSHP-06-0164-GA-RF Rev P04, RSHP-06-0165-GA-PD P03, RSHP-XX-0020-EL-S P03, RSHP-XX-0021-EL-W P03, RSHP-XX-0050-EL-S Rev P04, RSHP-XX-0051-EL-W Rev P04, RSHP-XX-0603-SE-DD P03, RSHP-XX-0605-SE-FF P03, RSHP-XX-0613-SE-DD Rev P04, RSHP-XX-0815-SE-FF Rev P04, RSHP-XX-0800-EL-S P03, RSHP-XX-0801-EL-W P03, RSHP-XX-0802-EL-N P03, RSHP-XX-0810-EL-S Rev P04, RSHP-XX-0811-EL-W Rev P04, RSHP-XX-0812-EL-N P03, RSHP-XX-0813-EL-W Rev P02, RSHP-XX-0814-EL-E Rev P02, RSHP-XX-2010-DT-W P03, RSHP-XX-2011-DT-W P03, RSHP-XX-2012-DT-W P03, RSHP-XX-2013-DT-2 P03, RSHP-XX-2014-DT-N P03, RSHP-XX-2015-DT-N P03, RSHP-XX-2016-DT-W Rev P01, RSHP-XX-0814-EL-E P01, RSHP-XX-0813-EL-W

PLANNING SUPPORTING STATEMENT, OUTLINE CONSTRUCTION METHOD STATEMENT, LVMF KENWOOD HOUSE ASSESSMENT, TOWNSCAPE AND VISUAL IMPACT ASSESSMENT, LOW/ZERO CARBON TECHNOLOGIES FEASIBILITY STUDY, ENERGY STRATEGY, CONSULTATION STATEMENT, SUSTAINABILITY AND ENERGY ASSESSMENT, TRANSPORT ASSESSMENT, TRAVEL PLAN, WSI FOR ARCHAEOLOGICAL EXCAVATION, TECHNICAL NOTE - CONVERSION OF SNOWFIELDS TO ONE WAY, AIR QUALITY IMPACT TECHNICAL REPORT, NOISE IMPACT/TECHNICAL REPORT, ROMAN BOAT MONITORING PROPOSAL, SUSTAINABILITY ASSESSMENT CHECKLIST, ECOLOGY ASSESSMENT, WIND ASSESSMENT (INCLUDING ADDENDUM 06/09/2012), FLOOD RISK ASSESSMENT, ARBORICULTURAL IMPLICATIONS ASSESSMENT, DELIVERY AND SERVICING PLAN, WATCHING BRIEF REPORT, ARCHAEOLOGICAL EVALUATION REPORT, ODYSSEY CONSULTING ENGINEERS TECHNICAL NOTE 10/09/2012, TVIA & LVMF SUPPLEMENTARY STATEMENT (SEPTEMBER 2012), DAYLIGHT AND SUNLIGHT REPORT (INCLUDING TECHNICAL NOTE 07/09/2012), WIND ASSESSMENT, LANDSCAPE STATEMENT (INCLUDING ADDENDUM SEPTEMBER 2012), & DESIGN & ACCESS STATEMENT (INCLUDING ADDENDUM SEPTEMBER 2012)

Reasons for granting permission.

This planning application was considered with regard to various policies including, but not exclusively:

a) Core Strategy 2011

Strategic Policy 1 Sustainable Development which requires developments to improve the places we live in and work in and enable a better quality of life for Southwark's diverse population.

Strategic Policy 2 - Sustainable transport: We will encourage walking, cycling and the use of public transport rather than travel by car. This will help create safe, attractive, vibrant and healthy places for people to live and work by reducing congestion, traffic and pollution.

Strategic Policy 3 Shopping, Leisure and Entertainment which defines a hierarchy of town and local centres which reflect their sizes and roles.

Strategic Policy 4 Places to Learn and Enjoy seeks to ensure that there will be a wide range of well used community facilities that provide spaces for many different communities and activities in accessible areas.

Strategic Policy 10 - Jobs and businesses: We will increase the number of jobs in Southwark and create an environment in which businesses can thrive. We will also try to ensure that local people and businesses benefit from opportunities which are generated from development.

Strategic Policy 11 Open Spaces and Wildlife protects important open spaces, trees and woodland from inappropriate development.

Strategic Policy 12 - Design and conservation: Development will achieve the highest possible standards of design for buildings and public spaces to help create attractive and distinctive places which are safe, easy to get around and a pleasure to be in.

Strategic Policy 13 - High environmental standards: Development will help us live and work in a way that respects the limits of the planet's natural resources, reduces pollution and damage to the environment and helps us to adapt to climate change.

b) Saved Policies of the Southwark Plan 2007

Policy 2.2 (Provision of new community facilities): seeks to ensure planning permission will be granted for new community facilities.

Policy 2.5 (Planning obligations): seeks to ensure that any adverse effects arising from a development is taken into account and mitigated and contributions towards infrastructure and the environment to support the development are secured, where relevant in accordance with Circular 05/2005 and other relevant guidance.

Policy 3.1 (Environmental effects): seeks to ensure there will be no material adverse effect on the environment and quality of life resulting from new development.

Policy 3.2 (Protection of amenity): advises that permission will not be granted where it would cause a loss of amenity.

Policy 3.4 (Energy efficiency): advises that development should be designed to maximise energy efficiency.

Policy 3.6 (Air quality): advises that permission will not be granted for development that would lead to a reduction in air quality.

Policy 3.12 (Quality in design): requires new development to achieve a high quality of architectural and urban design.

Policy 3.13 (Urban design) seeks to ensure that principles of good urban design are taken into account in all developments.

Policy 3.14 (Designing Out Crime) seeks to ensure that development is designed to improve community safety and crime prevention.

Policy 3.20 (Tall Buildings) advises that permission may be granted for buildings that are significantly taller than their surroundings or have a significant impact on the skyline, where they have excellent public transport accessibility and are located within the Central Activities Zone).

Policy 3.22 (Important Local Views) advises that the Local Planning Authority will seek to protect and enhance identified views, panoramas, prospects and their settings. Developments that would impact negatively on important local views will not be granted.

Policy 3.28 (Biodiversity) requires biodiversity to be taken into account in the determination of planning applications and the inclusion in developments of features which enhance biodiversity will be encouraged.

Policy 3.31 (Flood Defences) advises that permission will not be granted for development sited adjacent to the River Thames unless it is set back at a suitable distance from the river wall to allow for the replacement/repair of flood defences and for any future raising to be undertaken in a suitable and cost effective manner.

Policy 5.1 (Locating Developments) states that location of development must be appropriate to the size and trip generating characteristics of the development, stating that schemes generating a significant number of trips must be located within easy access of public transport nodes.

Policy 5.2 (Transport Impacts) states that permission will not be granted for development which has an adverse impact on transport networks through significant increases in traffic or pollution and consideration has been given to impacts on the Transport for London road network as well as adequate provision for servicing, circulation and access to and from the site.

Policy 5.3 (Walking and cycling) seeks to ensure that there is adequate provision for cyclists and pedestrians within developments, and where practicable the surrounding area

Policy 5.6 (Car Parking) states that all developments requiring car parking should minimise the number of spaces provided.

Policy 5.7 (Parking Standards for Disabled People) requires development (subject to site constraints) to provide adequate car parking for disabled people and the mobility impaired.

c) London Plan 2011

Policy 2.10 Central Activities Zone – strategic priorities, Policy 2.11 Central Activities Zone – strategic functions, Policy 2.13 Opportunity Areas and Intensification Areas, Policy 2.15 Town Centres, Policy 4.1 Developing London's economy, Policy 4.7 Retail and town centre development, Policy 5.1 Climate change mitigation, Policy 5.2 Minimising carbon dioxide emissions, Policy 5.5 Decentralised energy networks, Policy 5.6 Decentralised energy in development proposals, Policy 5.7 Renewable energy, Policy 6.1 Strategic approach (Transport), Policy 6.13 Parking, Policy 7.4 Local character, Policy 7.5 Public realm, Policy 7.6 Architecture, Policy 7.7 Location and design of tall and large buildings, Policy 7.11 London View Management Framework, Policy 7.12 Implementing the London View Management Framework and Policy 8.2 Planning obligations.

d) The National Planning Policy Framework

- Section 1: Building a strong competitive economy
- Section 2: Ensuring the vitality of town centres
- Section 4: Promoting sustainable development
- Section 7: Requiring good design
- Section 8: Promoting healthy communities
- Section 10: Meeting the challenge of climate change, flooding and coastal change
- Section 11: Conserving and enhancing the natural environment
- Section 12: Conserving and enhancing the historic environment

Particular regard was had to the principle of the proposed uses which were considered to be acceptable, and which will provide an acceptable mix to support the regeneration of the London Bridge Borough and Bankside Opportunity Area. The redevelopment of the site would generate significant economic benefits for the local and wider area and will increase the level of hospital care in Southwark.

The proposal satisfies the policy requirements for tall buildings, and for high quality design, including the NPPF expectation that new development has the highest standard of design. The design of the building is considered to be high quality that will result in a striking form, creating a new destination point and is considered to make a positive contribution to the skyline of London creating a local landmark building within Southwark.

Particular regard was had to the impact of the building on views with specific reference to views from Kenwood. Taking into account the revised LVMF SPG, the impact on these views is considered acceptable and any perceived harm to the wider historic environment is outweighed by the regenerative improvements delivered by the scheme that will achieve substantial public benefits.

The proposal would provide an extensive improvement of the streetscape together with new active frontages which would improve the experience for pedestrians, and provide for natural surveillance. The new paved areas allow for a range of uses to spill out into the space, which would add vitality to the space. New green roofs should enhance biodiversity and create habitat.

The impacts of the scheme in relation to daylight and sunlight have been assessed and are considered acceptable. The scheme is not expected to cause any unacceptable impact to local highway or transport conditions

Planning obligations are secured to offset the impact of the development in accordance with the Supplementary Planning Document on Planning Obligations.

Other policies have been considered, but in this instance were not considered to have such weight as to justify a refusal of permission. It was considered that the regenerative benefit to the wider community would outweigh any adverse impacts of the proposed development. It was therefore considered appropriate to grant planning permission having regard to the policies considered and any other material planning considerations.

Subject to the following conditions:

1 **Standard Time Limit**

The development hereby permitted shall be begun before the end of three years from the date of this permission.

Reason

As required by Section 91 of the Town and Country Planning Act 1990 as amended.

2 **Approved plans**

The development hereby permitted shall not be carried out otherwise than in accordance with the following approved plans:

RSHP-00-0001-GA-00 Rev P04, RSHP-00-0010-GA-00 Rev P04, RSHP-00-0015-GA-00 Rev P04,
 RSHP-00-0100-GA-00 P03, RSHP-XX-0101-GA-01 P03, RSHP-XX-0102-GA-02 P03,
 RSHP-XX-0103-GA-RF P03, RSHP-00-0149-GA-B1 Rev P04, RSHP-01-00150-GA-00 Rev P04,
 RSHP-01-0151-GA-01 Rev P04, RSHP-02-0152-GA-02 Rev P04, RSHP-02-0153-GA-03 Rev P04,
 RSHP-02-0154-GA-04 Rev P04, RSHP-03-0155-GA-05 Rev P04, RSHP-03-0156-GA-06 Rev P04,
 RSHP-04-0157-GA-07 Rev P04, RSHP-04-0158-GA-08 Rev P04, RSHP-04-0159-GA-09 Rev P04,
 RSHP-05-0160-GA-10 Rev P04, RSHP-05-0161-GA-11 Rev P04, RSHP-05-0162-GA-12 Rev P04,
 RSHP-05-0163-GA-13 Rev P04, RSHP-06-0164-GA-RF Rev P04, RSHP-06-0165-GA-PD P03,
 RSHP-XX-0020-EL-S P03, RSHP-XX-0021-EL-W P03, RSHP-XX-0050-EL-S Rev P04,
 RSHP-XX-0051-EL-W Rev P04, RSHP-XX-0603-SE-DD P03, RSHP-XX-0605-SE-FF P03,
 RSHP-XX-0813-SE-DD Rev P04, RSHP-XX-0615-SE-FF Rev P04, RSHP-XX-0800-EL-S P03,
 RSHP-XX-0801-EL-W P03, RSHP-XX-0802-EL-N P03, RSHP-XX-0810-EL-S Rev P04,
 RSHP-XX-0811-EL-W Rev P04, RSHP-XX-0812-EL-N P03, RSHP-XX-0813-EL-W Rev P02,
 RSHP-XX-0814-EL-E Rev P02, RSHP-XX-2010-DT-W P03, RSHP-XX-2011-DT-W P03,
 RSHP-XX-2012-DT-W P03, RSHP-XX-2013-DT-2 P03, RSHP-XX-2014-DT-N P03, RSHP-XX-2015-DT-N
 P03, RSHP-XX-2016-DT-W Rev P01, RSHP-XX-0814-EL-E P01, RSHP-XX-0813-EL-W

Reason:

For the avoidance of doubt and in the interests of proper planning.

3 **Submission of further details - external facing materials**

Samples of all external facing materials to be used in the carrying out of this development shall be presented on site and approved in writing by the Local Planning Authority prior to works commencing above grade; the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the Local Planning Authority may be satisfied as to the design and details in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

4 **Submission of further details - mock-up**

Full-scale mock-ups of the cladding to the building to be used in the carrying out of this development shall be provided and made available for approval in writing by the Local Planning Authority prior to works commencing above grade; the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the Local Planning Authority may be satisfied as to the design and details in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

Continued overleaf...

TP(Permit)

SOUTHWARK COUNCIL

TOWN AND COUNTRY PLANNING ACT 1990 (as amended)



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PLANNING PERMISSION WITH LEGAL AGREEMENT

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5 Submission of further details - detailed drawings

1:5/10 section detail-drawings through all main elements of the elevations and roof elements to be used in the carrying out of this permission shall be submitted to and approved in writing by the Local Planning Authority prior to works commencing above grade; the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the Local Planning Authority may be satisfied as to the design and details in the interest of the special architectural or historic qualities of the listed building in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

6 Telecommunications

Notwithstanding the provisions of Parts 24 and 25 of the Town & Country Planning (General Permitted Development) Order 1995 (or any order revoking and re-enacting that Order with or without modification) no external telecommunications equipment or structures shall be placed on any part of the buildings hereby permitted, without the prior written consent of the Local Planning Authority.

Reason:

In order to ensure that no telecommunications plant or equipment which might be detrimental to the design and appearance of the building and visual amenity of the area is installed on the roof of the building in accordance with Strategic Policy 12 – Design and Conservation of the Core Strategy and saved policies 3.2 Protection of amenity and 3.13 Urban Design of the Southwark Plan 2007.

7 Roof plant

No roof plant, equipment or other structures, other than as shown on the plans hereby approved or approved pursuant to a condition of this permission, shall be placed on the roof or be permitted to project above the roofline of any part of the building[s] as shown on elevational drawings or shall be permitted to extend outside of the roof plant enclosure[s] of any building[s] hereby permitted.

Reason:

In order to ensure that no additional plant or equipment which might be detrimental to the design and appearance of the building and visual amenity of the area is installed on the roof of the building in accordance with Strategic Policy 12 – Design and Conservation of the Core Strategy and saved policies 3.2 Protection of amenity and 3.13 Urban Design of the Southwark Plan 2007.

8 Construction and Environmental Management Plan

The development shall not commence until details of an Environmental Management Plan for Construction has been submitted to, and approved in writing by the Local Planning Authority for that part of the development. The Environmental Management Plan shall oblige the applicant, or developer and its contractor to use all best endeavours to minimise disturbances including but not limited to noise, vibration, dust, smoke and plant emissions emanating from the site during demolition and construction and will include the following information for agreement:

- A detailed specification of demolition and construction works at each phase of development including consideration of environmental impacts and the required remedial measures.

- The specification shall include details of the method of piling.
- Engineering measures, acoustic screening and the provision of sound insulation required mitigating or eliminating specific environmental impacts.
- Arrangements for publicity and promotion of the scheme during construction.
- A commitment to adopt and implement of the ICE Demolition Protocol and Considerate Contractor Scheme registration.

All demolition and construction work shall be undertaken in strict accordance with the approved management scheme and code of practice, unless otherwise agreed in writing by the Local Planning Authority.

Reason:

To ensure that residents and occupiers of neighbouring buildings do not suffer a loss of amenity by reason of pollution and nuisance in accordance with Strategic Policy 13 of the Core Strategy 2011 and saved policies 3.1, 3.2, 3.6 and 3.10 of the Southwark Plan 2007.

9

Cycle Storage

Prior to the occupation, details of the cycle storage facilities within the site (including shower and change facilities) and within the vicinity of the site shall be submitted to and approved in writing by the local planning authority. Thereafter the cycle parking facilities will be retained and used for no other purposes.

Reason:

In order to ensure satisfactory safe and secure cycle parking facilities are provided and retained in order to encourage the use of cycling as an alternative means of transport to the development and reduce reliance on the use of the private car in accordance with strategic policy 2 of the Core Strategy 2011 and saved policy 3.5 Walking and Cycling of the Southwark Plan-2007.

10

Car Parking – Vehicle Charging Points

Prior to occupation, details of the installation (including the location and type) of the electric vehicle charger points within the car park area shall be submitted to and approved in writing by the Local Planning Authority. The electric vehicle charger points shall be installed and retained thereafter and the development shall not be carried out other than in accordance with any such approval.

Reason:

To encourage more sustainable travel in accordance with saved policies 3.1 Environmental Effects and 5.2 Transport Impacts of the Southwark Plan 2007 and Strategic Policy 2 Sustainable Transport of the Core Strategy 2011.

11

Service and Delivery Management

All service and delivery activity will be managed in accordance with the approved Service and Delivery Management Plan (Appendix D of the Transport Assessment May 2012).

Reason:

To ensure compliance with Strategic Policy 2 – Sustainable Transport of the Core Strategy 2011 and saved policy 5.2 Transport Impacts of the Southwark Plan 2007.

12

BREEAM

Before occupation, a certified Post Construction Review (or other verification process agreed with the local planning authority) as relevant to that element of the building, shall be submitted to and approved in writing by the Local Planning Authority confirming that BREEAM Excellent has been achieved.

Reason:

To ensure the proposals comply with saved policies 3.3 sustainability and 3.4 Energy Efficiency of the Southwark Plan 2007 and Strategic Policy 13 – High Environmental Standards of the Core Strategy 2011.

13

Green/brown roof/ living walls/ vertical gardens and planters

Before any above grade work hereby authorised begins, details (including a specification and maintenance

plan) of the green/brown roof/ living walls/ vertical gardens and planters to be used in the carrying out of this permission shall be submitted to and approved in writing by the Local Planning Authority. The development shall not be carried out otherwise than in accordance with any such approval given, and the green/brown roof/ living walls/ vertical gardens and planters are to be retained for the duration of the use. Where trees and large shrubs are proposed to be provided within planters the soil volume shall be a minimum of 4 cubic metres per tree and 1 cubic metre per shrub or climbing plant. All planters are to provide a minimum internal soil height of 1m height. Where these are at ground level planters shall have their bottoms open to native soil beneath so that roots may naturally colonise and exploit such soil. Details of irrigation shall be provided such that water is available for the maintenance of all planters by mains, grey water or other sustainable drainage specification such as attenuation tanks.

Reason

To ensure the proposed development will preserve and enhance the visual amenities of the locality and is designed for the maximum benefit of screening, local biodiversity and adaptation to climate change, in accordance with NPPF Parts 7, 8, 11 & 12 and policies of The Core Strategy 2011: SP11 Open spaces and wildlife; SP12 Design and conservation; SP13 High environmental standards, and Saved Policies of The Southwark Plan 2007: Policy 3.13 Urban Design; Policy 3.2 Protection of amenity; Policy 3.28 Biodiversity.

14

Lighting

Details of the external lighting scheme for the relevant building shall be submitted and approved by the Local Planning Authority prior to occupation and maintained thereafter.

Reason:

To ensure an adequate level of amenity of surrounding local residents by reason of pollution in accordance with saved policy 3.2 Protection of Amenity of the Southwark Plan 2007.

15

Signage

A signage strategy will be submitted to and approved by the Local Planning Authority prior to occupation.

Reason:

In order that the LPA may be satisfied with the design and details in the interest of the special architectural qualities of the proposed buildings and the public spaces around it in accordance with saved policies 3.12 Quality in Design; 3.13 Urban Design of the Southwark Plan 2007 and Strategic Policy 12 Design and Conservation of the Core Strategy (2011).

16

Archaeological Evaluation

Notwithstanding references to archaeological work contained within the Lang O Rourke Outline Construction Method Statement the archaeological evaluation shall be undertaken as detailed in the Written Scheme of Investigation for an archaeological Evaluation Museum of London Archaeology dated 12 June 2012.

Reason:

In order that the applicants supply the necessary archaeological information to ensure suitable mitigation measures and/or foundation design proposals be presented in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

17

Archaeological Mitigation

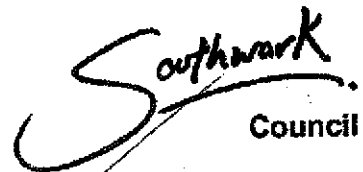
Notwithstanding references to archaeological work contained within the Lang O Rourke Outline Construction Method Statement before any work hereby authorised begins, excepting demolition, the applicant shall submit a written scheme of investigation for a programme of archaeological recording, which shall be approved in writing by the Local Planning Authority and implemented and shall not be carried out other than in accordance with any such approval given.

Reason:

In order that the details of the programme of archaeological excavation and recording works are suitable with regard to the impacts of the proposed development and the nature and extent of archaeological remains on site in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

Continued overleaf...

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18 Archaeological Foundation Design

Before any work hereby authorised begins, a detailed scheme showing the complete scope and arrangement of the foundation design and all ground works shall be submitted to and approved in writing by the Local Planning Authority and the development shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that details of the foundations, ground works and all below ground impacts of the proposed development are detailed and accord with the programme of archaeological mitigation works to ensure the preservation of archaeological remains by record and in situ in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

19 Archaeological Reporting

Within six months of the completion of archaeological site works, an interim archaeological report shall be submitted covering works both within and without the scheduled monument. A full assessment report detailing the proposals for post-excavation works, publication of the site and preparation of the archive shall be submitted to and approved in writing by the Local Planning Authority within one year of the completion of archaeological sites works, and that the works detailed in this assessment report shall not be carried out otherwise than in accordance with any such approval given.

Reason:

In order that the archaeological interests of the site are secured with regard to the details of the post-excavation works, publication and archiving to ensure the preservation of archaeological remains by record in accordance with Chapter 12, paragraph 141 of the National Planning Policy Framework, policy 12 of the Core Strategy 2011 and saved policy 3.19 of the Southwark Plan 2007.

20 Landscaping plan

Before any above grade work hereby authorised begins, detailed drawings scale 1:50 of a hard and soft landscaping scheme showing the treatment of all parts of the site not covered by buildings (including surfacing materials of any parking, access, or pathways layouts, materials and edge details and material samples of hard landscaping), shall be submitted to and approved in writing by the Local Planning Authority. The landscaping shall not be carried out otherwise than in accordance with any such approval given and shall be retained for the duration of the use. The planting, seeding and/or turfing shall be carried out in the first planting season following completion of building works and any trees or shrubs that is found to be dead, dying, severely damaged or diseased within five years of the completion of the building works OR five years of the carrying out of the landscaping scheme (whichever is later), shall be replaced in the next planting season by specimens of similar size and species in the first suitable planting season. Planting shall comply to BS: 4428 Code of practice for general landscaping operations, BS: 3996 Nursery stock specification, BS: 5837 Trees in relation to construction and BS: 7370 Recommendations for establishing and managing grounds maintenance organisations and for design considerations related to maintenance.

Reason

To ensure the proposed development will preserve and enhance the visual amenities of the locality and is designed for the maximum benefit of screening, local biodiversity, adaptation to climate change, in addition to the attenuation of surface water runoff, in accordance with NPPF Parts 7, 8, 11 & 12, London Plan 2011 Policy 2.18 Green infrastructure; Policy 5.1 Climate change mitigation; Policy 5.10 Urban greening, Policy 5.11 Green roofs and development site environs; Policy 5.13 Sustainable drainage; Policy 6.1 Strategic approach; Policy 7.4 Local character; Policy 7.21 Trees and woodlands; The London Climate Change Adaption Strategy Action 19 and policies of The Core Strategy 2011: SP11 Open spaces and wildlife; SP12 Design and conservation; SP13 High environmental standards and Saved Policies of the Southwark Plan 2007: Policy 3.13 Urban Design; Policy 3.2 Protection of amenity; Policy 3.28 Biodiversity.

21 Tree planting

No works or development shall take place above grade until full details of all proposed tree planting or transplanting, and the proposed times of planting, have been approved in writing by the local planning authority. This will include planting and maintenance specifications, use of guards or other protective measures and confirmation of location, species and sizes, nursery stock type, supplier and defect period. All tree planting shall be carried out in accordance with those details and at those times. Planting shall comply with BS: 4428 Code of practice for general landscaping operations.

If within a period of five years from the date of the planting of any tree that tree, or any tree planted in replacement for it, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the local planning authority, seriously damaged or defective, another tree of the same species and size as that originally planted shall be planted at the same place, unless the local planning authority gives its written consent to any variation.

Reason

In order that the Local Planning Authority may be satisfied as to the design and details in the interest of the special architectural qualities of the existing building and the public spaces around it in accordance with Policies 3.12 Quality in Design, 3.13 Urban Design and 3.28 Biodiversity of The Southwark Plan 2007.

22 Service and Delivery Management

All service and delivery activity will be managed in accordance with the approved Service and Delivery Management Plan (June 2012).

Reason:

To ensure compliance with Strategic Policy 2 – Sustainable Transport of the Core Strategy 2011 and saved policy 5.2 Transport Impacts of the Southwark Plan 2007.

23 Noise creep from noise and/or vibration generation – commercial plant.

The noise level from any plant (e.g. refrigeration, air conditioning), together with any associated ducting, shall be 10(A) dB or more below the lowest measured external ambient $L_{Aeq, T}$ at the site boundary. The equipment shall be installed and constructed in accordance with any approved scheme and be permanently maintained thereafter.

Within one month of the installation of the plant and equipment, you are required to submit a further noise report confirming previous details and subsequent measurement data of the installed plant to demonstrate compliance with the above requirements. The supplementary acoustic report must include:

- i) A schedule of all plant and equipment installed;
- ii) Locations of the plant and machinery and associated: ducting; attenuation and damping equipment;
- iii) Manufacturer specifications of sound emissions in octave or third octave detail;
- iv) Distances between plant & equipment and receptor location/s and any mitigating features that may attenuate the sound level received at the most affected receptor location;
- i) The lowest existing $L_{Aeq, T}$ measurement as already established.
- ii) New noise monitoring data, measurement evidence and any calculations demonstrating that plant complies with the planning condition.

* $L_{Aeq, T}$ = 1 hr between 07:00 and 23:00 and 5min between 23:00 and 07:00.

Reason

To ensure that users of the surrounding area not suffer a loss of amenity by reason of noise nuisance and other excess noise from plant and that the operation of plant does not add by cumulative effect to the existing sound environment in accordance with saved policies 3.1 'Environmental Effects' and 3.2 'Protection of Amenity' of the Southwark Plan 2007.

24 Ventilation

Prior to occupation, details relating to ventilation of the building as a whole and any specific ventilation for laboratories or other 'polluting activities', including positioning of fresh air intake and relationship to extract from the building and adjacent building within the complex shall be submitted and approved by the local planning authority.

Reason

To ensure that users of the surrounding area not suffer a loss of amenity by reason of odour and emissions in

accordance with saved policies 3.1 'Environmental Effects' and 3.2 'Protection of Amenity' of the Southwark Plan 2007.

25 Environment Agency - flooding

The development permitted by this planning permission shall only be carried out in accordance with the approved Flood Risk Assessment (FRA) by Arup (dated 11 June 2012 with reference ARUP-XXX-RP-0006 P03) and the following mitigation measures detailed within the approved FRA:

- with regards to the basement level, the proposed electrical plant should benefit from appropriate flood resistant and resilient measures;
- only 'less vulnerable' uses should be accommodated on the ground floor;
- safe refuge on the first floor and above should be made available at all times, with escape routes within the building provided from every floor;
- a drainage strategy should be finalized and agreed with the London Borough of Southwark before planning permission is granted, incorporating the use of Sustainable Drainage Systems (SuDS) wherever possible.

Reasons

To reduce the impact of flooding to the proposed development, future occupants and to attenuate surface water flows and prevent flooding by ensuring satisfactory and sustainable storage or disposal of surface water from the site, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

26 Environment Agency - contamination

If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, a remediation strategy detailing how this unsuspected contamination shall be dealt with. The remediation strategy shall be implemented as approved and reported to the satisfaction of the Local Planning Authority.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

27 Environment Agency - contamination

Prior to occupation of the development, a verification report demonstrating completion of the works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to and approved, in writing, by the Local Planning Authority. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met. It shall also include a plan (a 'long-term monitoring and maintenance plan') for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action, as identified in the verification plan, if appropriate, and for the reporting of this to the Local Planning Authority. Any long-term monitoring and maintenance plan shall be implemented as approved.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

28 Environment Agency - contamination

Piling or any other foundation designs using penetrative methods shall not be permitted other than with the express written consent of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason

To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

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29 Environment Agency - SUDS

Whilst the principles and installation of Sustainable Drainage Schemes (SuDS) are to be encouraged, no infiltration of surface water drainage into the ground is permitted other than with the express written consent of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to Controlled Waters. The development shall be carried out in accordance with the approved details.

Reason:
To reduce the potential for unexpected contamination to be identified during groundworks, particularly basement construction, in line with the London Plan 2011 and saved policy 3.31 of the Southwark Plan 2007.

30 Submission of further details - detailed drawings

Notwithstanding the drawings hereby approved, detailed drawings of the rooftop plant enclosure (including lift over-runs) shall be submitted and approved prior to works commencing above grade.

Reason:
In order that the Local Planning Authority may be satisfied that the design of this prominent element is of the highest standard in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; 3.20 Tall Buildings of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

31 Submission of further details - detailed drawings

Notwithstanding the drawings hereby approved, detailed drawings (including typical storey-bays in elevation at 1:20 and 1:5) of the vertical solid panel elements in Snowfields and Great Maze Pond shall be submitted and approved prior to works commencing above grade.

Reason:
In order that the Local Planning Authority may be satisfied that the design of these prominent elements is of the highest standard in accordance with saved policies: 3.12 Quality in Design; 3.13 Urban Design; of the Southwark Plan July 2007 and SP12 Design & conservation of the Core strategy (2011).

Signed **Gary Rice**

Head of Development Management

Your attention is drawn to the notes accompanying this document

Any enquiries regarding this document should quote the LBS Registered Number and be sent to the Head of Development Management, Southwark Council, Chief executive's department, Planning division, Development management, PO Box 64529, London SE1 5LX, or by email to planning.applications@southwark.gov.uk

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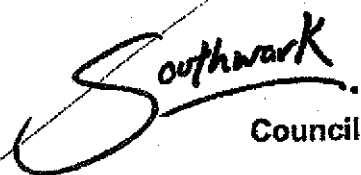
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PLANNING PERMISSION WITH LEGAL AGREEMENT

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INFORMATIVE NOTES TO APPLICANT RELATING TO THE PROPOSED DEVELOPMENT

- 1 The planning permission granted includes alterations and amendments to areas of the public highway, which will need to be funded by the developer. Although these works are approved in principle by the Highway Authority, no permission is hereby granted to carry out these works until all necessary and appropriate design details have been submitted and agreed. You are advised to contact the Principal Engineer, Infrastructure Group (020 7525 5509), at least 4 months prior to any works commencing on the public highway.
- 2 At least 6 months before the occupation of the new commercial units hereby permitted you are advised that you must obtain the Council's approval for the numbering and naming of buildings and the naming of any new streets created by the development.
- 3 The details and/or samples required by the Condition(s) above must be accompanied by a letter stating:
 1. the LBS Reference Number which appears at the top of this decision notice;
 2. the full address of the application site;
 3. which condition(s) you seek to discharge; and
 4. a list of all drawing numbers/ sample name and manufacturer, together with the condition(s) they relate to.

All samples submitted must be clearly labelled with the LBS Reference Number of the original application and the address of the application site.

Please note that the approval of details are subject to the same eight week timeframe as a full planning permission.
- 4 The Archaeology Officer can, on request, provide an archaeological brief detailing the methodology of the archaeological programme and can also provide information concerning archaeological organisations who work frequently within the Borough and who may be able to carry out the works.

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IMPORTANT NOTES RELATING TO THE COUNCIL'S DECISION

- (1) **APPEAL TO THE SECRETARY OF STATE.** If you are aggrieved by this decision of the council as the local planning authority to grant permission subject to conditions you can appeal to the Secretary of State under Section 78 of the Town and Country Planning Act 1990. If you appeal you must do so within six months of the date of this notice. The Secretary of State can allow a longer period for giving notice of an appeal but will not normally use this power unless there are special circumstances which excuse the delay in giving notice of appeal. The Secretary of State need not consider an appeal if it seems that the local planning authority could not have granted it without the conditions imposed, having regard to the statutory requirements, to the provisions of any development order and to any directions given under a development order. If you do decide to appeal you can do so using The Planning Inspectorate's online appeals service. You can find the service through the appeals area of the Planning Portal at www.planningportal.gov.uk/pcs. You can also appeal by completing the appropriate form which you can get from The Planning Inspectorate, Customer Support Unit, Temple Quay House, 2 The Square, Temple Quay, Bristol BS1 6PN [tel. 0117-3726372]. The form can also be downloaded from the Inspectorate's website at www.planning-inspectorate.gov.uk. The Planning Inspectorate will publish details of your appeal on the internet on the appeals area of the Planning Portal. This may include a copy of the original planning application form and relevant supporting documents supplied to the council by you or your agent, together with the completed appeal form and information you submit to The Planning Inspectorate. Please ensure that you only provide information, including personal information belonging to you, that you are happy will be made available to others in this way. If you supply information belonging to someone else please ensure you have their permission to do so. More detailed information about data protection and privacy matters is available on the Planning Portal.
- (2) **PURCHASE NOTICE.** If either the local planning authority or the Secretary of State grants permission subject to conditions, the owner may claim that the land can neither be put to a reasonably beneficial use in its existing state nor made capable of reasonably beneficial use by the carrying out of any development which has been or would be permitted. In these circumstances the owner may serve a purchase notice on the Council requiring the Council to purchase the owner's interest in the land in accordance with Part VI of the Town and Country Planning Act 1990.
- (3) **PROVISIONS FOR THE BENEFIT OF THE DISABLED.** Applicants are reminded that account needs to be taken of the statutory requirements of the Disability Discrimination Act 1995 to provide access and facilities for disabled people where planning permission is granted for any development which provides:
- (i) Buildings or premises to which the public are to be admitted whether on payment or otherwise. [Part III of the Act].
 - (ii) Premises in which people are employed to work as covered by the Health and Safety etc At Work Act 1974 and the Management of Health and Safety at Work Regulations as amended 1999. [Part II of the Act].
 - (iii) Premises to be used as a university, university college or college, school or hall of a university, or intended as an institution under the terms of the Further and Higher Education Act 1992. [Part IV of the Act].
- Attention is also drawn to British Standard 8300:2001 Disability Access, Access for disabled people to schools buildings – a management and design guide. Building Bulletin 91 (DfEE 99) and Approved Document M (Access to and use of buildings) of the Building Regulations 2000 or any such prescribed replacement.
- (4) **OTHER APPROVALS REQUIRED PRIOR TO THE IMPLEMENTATION OF PLANNING PERMISSION.** The granting of planning permission does not relieve the developer of the necessity for complying with any Local Acts, regulations, building by-laws and general statutory provisions in force in the area, or allow them to modify or affect any personal or restrictive covenants, easements, etc., applying to or affecting either the land to which the permission relates or any other land or the rights of any persons or authorities (including the London Borough of Southwark) entitled to the benefits thereof or holding an interest in the property concerned in the development permitted or in any adjoining property.
- (5) **WORKS AFFECTING THE PUBLIC HIGHWAY.** You are advised to consult the council's Highway Maintenance section [tel. 020-7525-2000] about any proposed works to, above or under any road, footway or forecourt.
- (6) **THE DULWICH ESTATE SCHEME OF MANAGEMENT.** Development of sites within the area covered by the Scheme of Management may also require the permission of the Dulwich Estate. If your property is in the Dulwich area with a post code of SE19, 21, 22, 24 or 26 you are advised to consult the Estates Governors', The Old College, Gallery Road SE21 7AE [tel. 020-8299-1000].
- (7) **BUILDING REGULATIONS.** You are advised to consult Southwark Building Control at the earliest possible moment to ascertain whether your proposal will require consent under the Building Act 1984 [as amended], Building Regulations 2000 [as amended], the London Building Acts or other statutes. A Building Control officer will advise as to the submission of any necessary applications, [tel. call centre number 0845 600 1265].
- (8) **THE PARTY WALL Etc. ACT 1996.** You are advised that you must notify all affected neighbours of work to an existing wall or floor/ceiling shared with another property, a new building on a boundary with neighbouring property or excavation near a

neighbouring building. An explanatory booklet aimed mainly at householders and small businesses can be obtained from the Department for Communities and Local Government (DCLG) Free Literature tel: 0870 1226 236 [quoting product code 02BR00862].

IMPORTANT: This is a **PLANNING PERMISSION** only and does not operate so as to grant any lease, tenancy or right of occupation of or entry to the land to which it refers.

- 1.2.2.8 provide a minimum of fifteen Adult Numeracy and Adult Literacy training opportunities for workless residents;
 - 1.2.2.9 provide a minimum of seventeen BTEC-level 1/2 Qualifications training opportunities for workless residents;
 - 1.2.2.10 provide a minimum of thirteen work experience placements during the construction of the Development
 - 1.2.2.11 provide a minimum of six site visits for local organizations
 - 1.2.2.12 provide a minimum of five school activities (Resource Days) within the local borough
 - 1.2.2.13 provide a minimum of one construction taster day during the construction of the Development
 - 1.2.2.14 provide the opportunity for a minimum of 150 site workers to be skill profiled for all boroughs of residency
 - 1.2.2.15 provide the opportunity for continual professional development for four individuals from the local borough
- 1.2.3 ensure through its Service Improvement Development Manager that the Trust complies with the Trust related targets set out below for apprentices or suitable opportunities for jobless residents within the Borough during the three year period commencing immediately on the Implementation Date: -
- 1.2.3.1 Hotel Services x 24
 - 1.2.3.2 Facilities Management x 6
 - 1.2.3.3 Engineering x 16
 - 1.2.3.4 Project Managers x 8
 - 1.2.3.5 Portering x 6
 - 1.2.3.6 Sterile Services Department x 8
- 1.2.4 In respect of each set of targets set out in paragraph 1.2.3 and 1.2.4, the Developer covenants to produce written reports regarding the delivery and outcomes of the initiatives and obligations set out in this paragraph on a regular basis the first report to be produced on the date three (3) months after the Implementation Date.

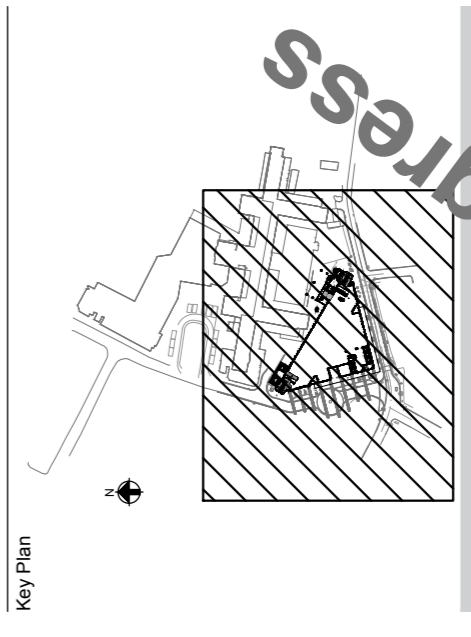
- Notes:**
- General arrangement plan presented on this drawing based on Gillespie drawing GIL-01-0100-GA-001, Rev P04 dated 30-10-2013 and Odyssey drawing 11-096-012.
 - General arrangement plan subject to review and approval by London Borough of Southwark and Guy's & St Thomas NHS Trust.
 - Traffic assessment completed by Odyssey in June 2012 confirmed Snowfields will not be used for any NHS loading/unloading.
 - All proposed Cancer Centre deliveries and waste collections are to be made from existing FM yard on Weston Street.
 - Highway Plan based on LBS Highway Highway Plan dated 20/05/2014, & RSHIP drawings as part of the Section 106 agreement. Final extent of highway boundary is subject to review and approval by London Borough of Southwark and Guy's & St Thomas NHS Trust. Refer to drawing ARUP-00-4400-GA-00-1100 for proposed surface finishes.
- Legend:**
- Existing Highway Boundary
 - Extent of on-site external works
 - Proposed Kerbs
 - Proposed Sub-surface Tree Pits
 - Proposed 400x400 Blister Paving
 - Proposed belisha beacon

P02	18/07/14	MJH	MJH	CNR
Updated to incorporate LBS comments				
P02	18/07/14	MJH	MJH	CNR
Updated to incorporate LBS comments				
P01	10/05/14	BA	MH	CR
For Information				
Issue	Date	By	Chkd	Appd

ARUP
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 www.arup.com

Client
Guys and St Thomas NHS Foundation Trust

Job Title
KHP Cancer Centre at Guys



Section 278 Highway Works
 General Arrangement

Scale at A1 1:200

Discipline: **Highway**

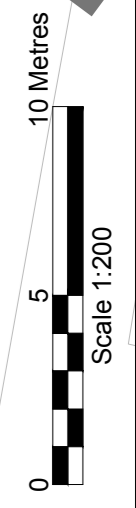
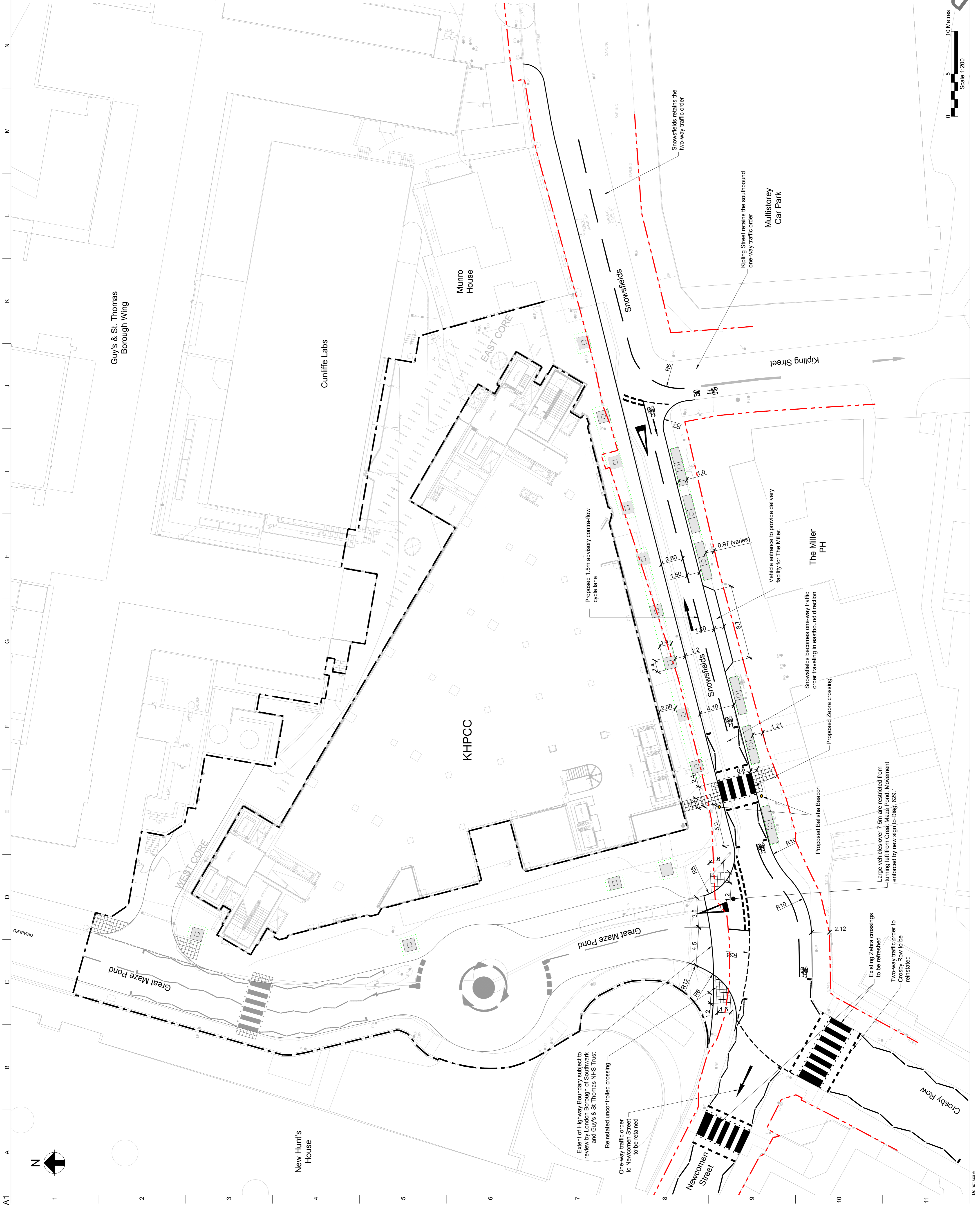
Job No: **21001-70**

Drawing Status: **Preliminary**

Issue No: **001**

Issue: **ARUP-00-4400-GA-00-110**

Issue: **P02**



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A B C D E F G H I J K L M N

London Borough of Southwark



Quietway Cycling Proposals

Site H – Rothsay Street

Public Consultation Summary

September 2014

London Borough of Southwark

Site H - Quietway Cycling Proposals Rothsay Street

Public Consultation Summary

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1.0 Introduction

1.1 Background

1.1.1 This document report has been produced by the London Borough of Southwark Public Realm Projects Group, to provide a summary of the consultation exercise for the Quietway Cycling Proposals for Site H in Rothsay Street. The measures are being drafted by the Public Realm Projects Team, with the project manager for this scheme being Chris Mascord (Senior Engineer).

1.1.2 The area under consideration is located within the SE1 district of Southwark (Chaucer Ward) in the north of the borough. See figure 1 below.

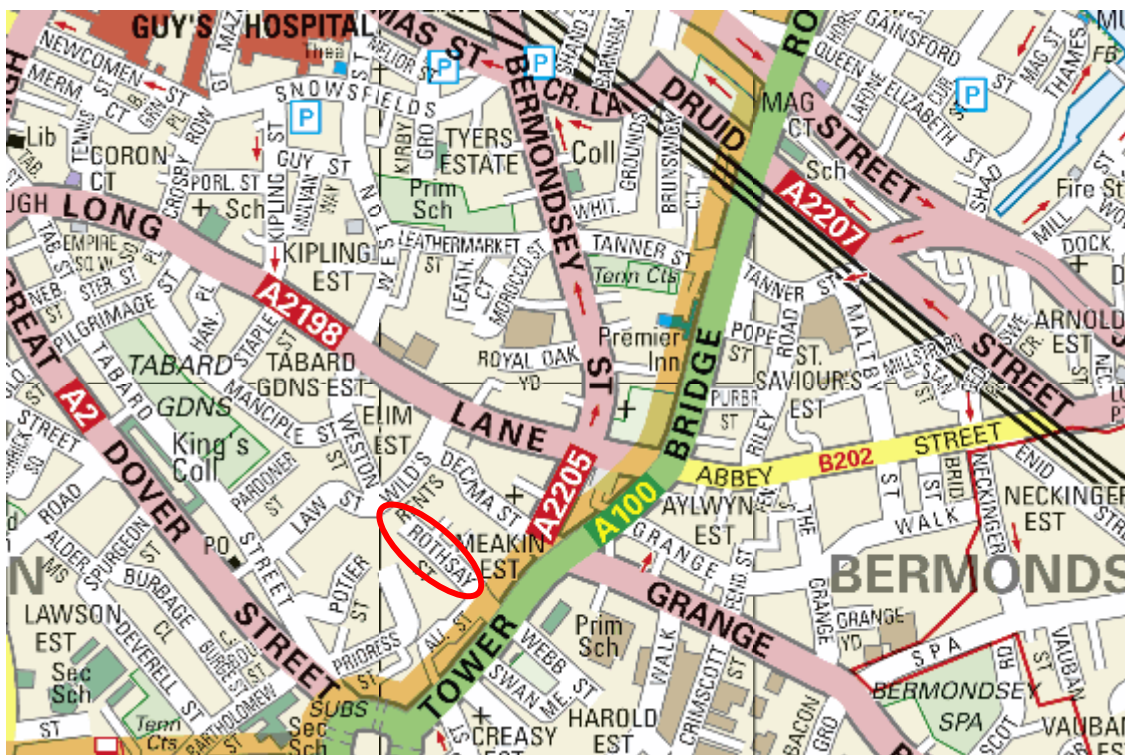


Figure 1: Location of proposed scheme

1.2 Project and Objectives

1.2.1 This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey Station in the east and traversing local roads to The Cut in the west (see figure 2). The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

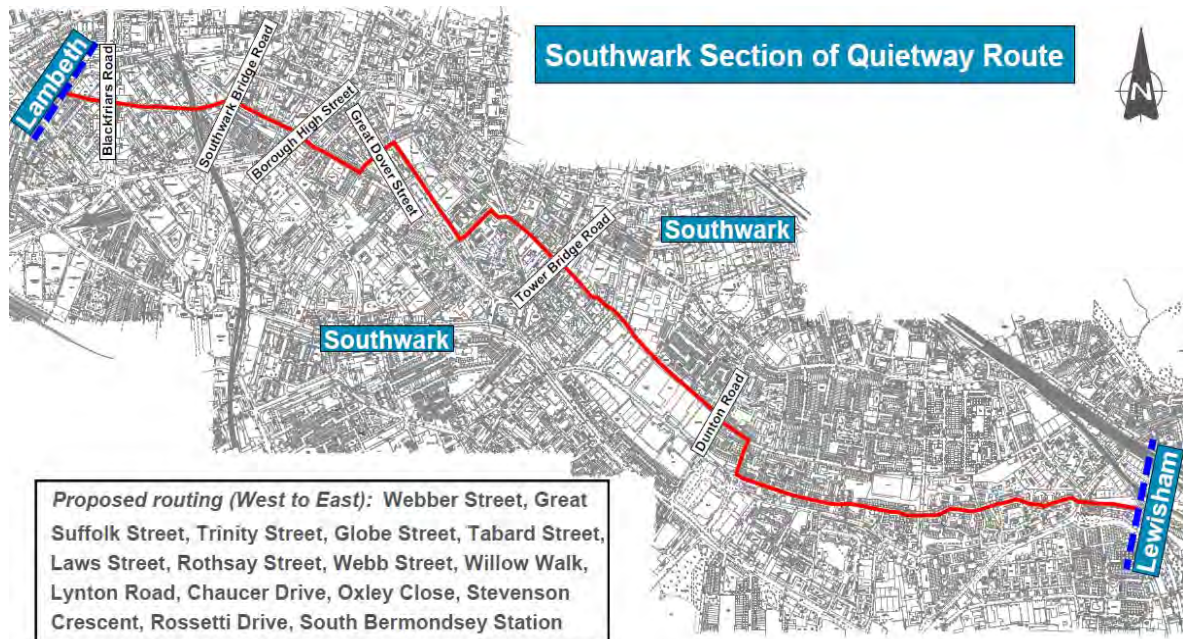


Figure 2: Southwark's section of Quietway Route

- 1.2.2 Quietways are a network of bike routes for less confident cyclists using residential streets with low traffic volumes. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.
- 1.2.3 The measures proposed in this consultation are part of the Council's ongoing commitment to make Southwark's streets safer and more accessible for all. The proposed measures will enhance the environment for all road users, reducing traffic speeds and improving pedestrian safety. Cycling proposals also have the added health benefits of improving the environment through reducing carbon emissions and getting more people onto bikes which in turn enhances their fitness and health.
- 1.2.4 The following measures were consulted upon to improve cycle safety and accessibility for Site H:

Section A

- Fully segregated cycle track between Rothsay Street and Law Street / Wilds Rents with existing pedestrian footway remaining in current location.
- Cycle track and footway to be separated by extension of central planting area. All existing mature trees to be retained.
- Existing raised carriageway table at junction of Wild's Rents / Weston Street / Law Street junction to be extended in Law Street to reduce traffic speeds and

assist cyclists accessing Rothsay Street. Carriageway table and surrounding footways to be repaved to improve the streetscape.

- Footways adjacent to the carriageway table to be built out with new tree planting introduced where possible to improve the streetscape. Wider footways will reduce traffic speeds and improve pedestrian accessibility.
- Law Street carriageway to be resurfaced and existing speed cushions to be converted to sinusoidal humps to improve the ride quality for cyclists using this section of the route.

Section B

- The junction of Tower Bridge Road / Rothsay Street / Webb Street is to be signalised as part of Transport for London's streetscape improvement scheme in Tower Bridge Road that has been consulted on separately.
- Rothsay Street to be one way westbound for general traffic from Tower Bridge Road to Alice Street. Cyclists will be able to travel eastbound and access Tower Bridge Road and Webb Street via a green traffic signal.
- Traffic in Rothsay Street and Alice Street will be able to access Tower Bridge Road via Green Walk.

(See Appendix A – Initial Scheme Design)

1.3 Consultation Procedure

- 1.3.1 The views of the local community and those of statutory consultees have been sought, prior to the development of measures to a detailed design stage. Active community participation was encouraged through the use of a consultation document and questionnaire (see Appendix B – Consultation Documents).
- 1.3.2 The consultation document included a covering letter describing the proposals and a request for comments (including information to assist in translation and large print versions of the consultation document), preliminary design drawings (A3 size) and a questionnaire/comment form that could be sent to the Public Realm Projects Group with a pre-paid address reply.
- 1.3.3 The consultation document was delivered to a geographical area centred on the Rothsay Street, Alice Street and Green Walk, using strategic roads and pedestrian desire lines as defined cut off points (See Appendix C – Location Plan and Extents of Consultation).
- 1.3.4 The distribution area was large enough to gain views from the wider community that may be considered to be affected by the proposed measures. A mailing list was established for the area by way of the Council's GIS database. In addition, the consultation documents and plans were supplied to the Council's established list of statutory consultees including London Buses, cycle groups and the Metropolitan Police. Please see Appendix D of list of addresses within the distribution area. Consultation documents were also delivered to the Southwark Housing Department and Grange Primary School.

- 1.3.5 The consultation documents were delivered by Royal Mail to 462 addresses detailed within the distribution list. The documents were delivered on the 5th August 2014, with a return deadline of the 5th September 2014, allowing 4 weeks for the consultation period. However due to the summer holiday period, responses were accepted online until the 12th September 2014.
- 1.3.6 The proposals were also available to view online using consultation section of the council's website, with an e-form questionnaire provided in order to capture responses.

2.0 Consultation Responses

2.1 Response Rate and Distribution

- 2.1.1 A total of 72 responses were received during the consultation period (45 returned questionnaires and 27 online responses), equating to a 16% response rate.
- 2.1.2 Four responses were received from Statutory Consultees (Southwark Living Streets, Southwark Cyclists, Lambeth Cyclists and Sustrans).

2.2 Questionnaire Analysis

- 2.2.1 The questionnaire element of the consultation document contained the following key questions and associated tick box options:

Q1. Are you a resident or business?

Q2. Do you support the proposals?

- 2.2.2 The following is a summary of replies received:

Question 1 - Are you a resident or business?

	Resident	Business
Replies	70	2
Total	97%	3%

Table 1: Returned questionnaire results for question 1

- 2.2.3 Table 1 indicates that the majority of responses received throughout the consultation period were from local residents, with only two businesses formally replying.

Question 2 – Do you support the proposals?

	Support	Opposed	No Opinion
Replies	29	41	2
Total	40%	57%	3%

Table 2: Returned questionnaire results for question 2

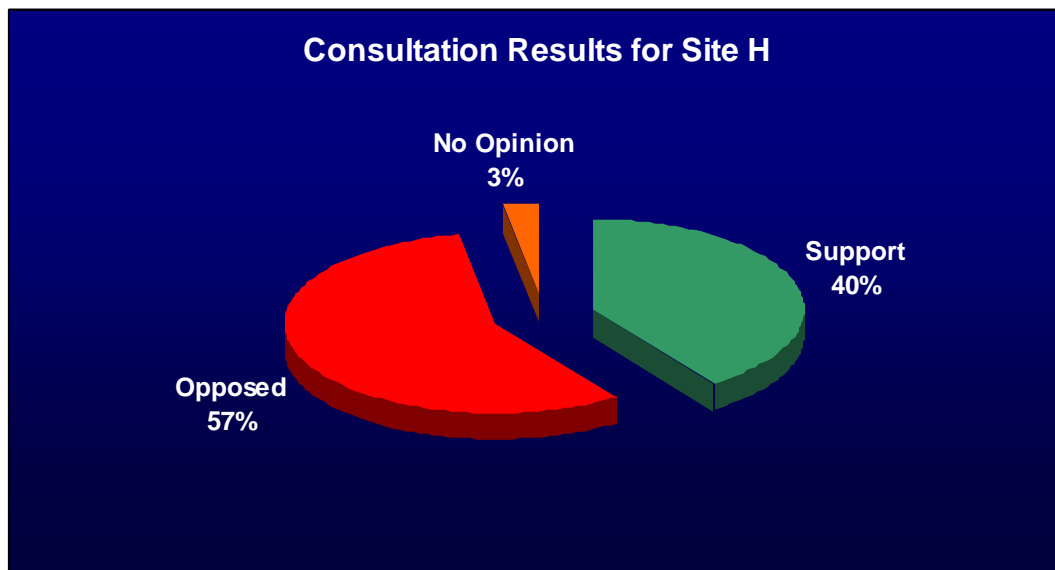


Figure 3: Graphical representation of consultation data for question 2

2.2.4 Table 2 and figure 2 indicate a majority opposed for question 2, with 57% objecting to the proposed measures.

2.3 Additional Comments

2.3.1 The questionnaire element of the consultation document invited consultees to attach any additional comments they may have on the proposals when returning the reply-paid questionnaire. Consultees were also able to reply to the consultation online using the council's website.

2.3.2 Many respondents highlighted that Quietways were a great idea and will enable many cyclists to enjoy their rides in safe, quiet and less polluted areas.

2.3.3 Respondents in support of the proposed measures discussed that they would help cyclists get safety across Tower Bridge Road and potentially save lives. Signalling Tower Bridge Road is essential as there have been significant accidents there.

2.3.4 A number of respondents were very supportive of the plans which illustrated strong forward thinking by Southwark Council and TfL.

- 2.3.5 Many respondents were in favour of the proposed cycle track between Rothsay Street and Weston Street, as the current arrangement leads cyclists onto the pavement with pedestrians.
- 2.3.6 A number of respondents commented that they were regular cyclists and welcomed the new, safer route between Southwark and Lambeth through the provision of new infrastructure specifically targeted at signalised road junctions where the majority of accidents occur.
- 2.3.7 A respondent commented that it is good to see you have taken into account the concerns of residents whose front doors are facing the planting area between Rothsay Street and Law Street. 'Thank you for keeping pedestrians and cyclists separate.'
- 2.3.8 A number of respondents, inquired if the congestion charge zone will stay the same, as it's not fair that you have to pay the congestion charge just to access the Jam Factory car park. *
- * In response, the congestion charge zone area and associated infrastructure is managed by Transport for London and there are no plans as part of the Quietway proposals to revise the zone.
- 2.3.9 A respondent in support of the scheme requested if the proposed one-way working could be reversed so that traffic could still exit Rothsay Street to turn right into Tower Bridge Road, which is safer than exiting out of Green Walk. *
- * In response, sightlines for vehicles exiting Green Walk onto Tower Bridge Road are adequate to ensure good visibility in both directions. There is no kerbside parking to the south of the junction and kerbside parking controls extending 15m to the north from the junction. There is also a pelican crossing near the Bricklayers Arms roundabout that creates gaps in northbound traffic on Tower Bridge Road that assists vehicles exiting Green Walk.
- 2.3.10 A comment was made regarding the loss of parking associated with the extension of the raised carriageway table in Law Street and that parking on the road in the area is already limited. *
- * In response, only one bay is proposed to be removed in order to create a safer junction for both cyclists and pedestrians. The extension of the raised table will assist with curtailing traffic speeds on approach to the cycle path / access and pedestrian desire line across Law Street. It is noted that there is off street resident parking provided on adjacent estates and therefore this proposal has minimal effect on the availability of parking in the area.
- 2.3.11 A number of comments were made discussing that consideration needs to be given to pedestrians crossing at the Wilds Rents / Law Street / Weston Street junction as there is a blind bend, it is dangerous and vehicles drive at speed. *
- * In response, the junction is being upgraded to improve pedestrian safety and accessibility as well as curtailing traffic speeds. In particular, the southern side of the junction where Law Street joins Wilds Rents and Weston Street is being tightened with kerb buildouts and tighter corner radii that will restrict the speed of vehicles turning into Law Street. In addition, new pedestrian crossing points are

being installed to improve pedestrian accessibility and are being installed at locations where there are adequate sightlines for approaching traffic.

- 2.3.12 A comment was received stating that Rothsay Street is not busy enough to warrant making it one way. *

* In response, whilst traffic volumes are minimal in Rothsay Street, as there is no through traffic and the road is only used to access to a number of residential car parks, the one way proposals will provide cyclists with a full phase of green signal for up to 8 seconds at the junction with Tower Bridge Road. This is particularly pertinent, as the number of cyclists using the route is expected to increase significantly. Sharing the signals with eastbound traffic in Rothsay Street could potentially reduce capacity for cyclists at the junction. Eastbound car traffic has a viable alternative routing via Alice Street and Green Walk to access Tower Bridge Road.

- 2.3.13 A respondent commented that the scheme will impact on the access to the underground car park and you will need to install mirrors to prevent accidents, as more cyclists and pedestrian will be using the route. *

* In response, Rothsay Street has no through traffic, minimal traffic volumes and there have been no reported issues with pedestrians and cyclists conflicting with traffic exiting car parks in Rothsay Street. The ingress and egress of vehicles from the Jam Factory car park is minimal and intermittent and therefore there is no requirement to install highway safety measures at this location.

- 2.3.12 Analysis of the additional comments from respondents that objected to the scheme highlighted the following concerns:

A number of objections were received stating that the proposals will be detrimental for anyone trying to drive out of Rothsay Street, as Green Walk and Alice Street are very narrow and difficult to traverse. *

* In response, Rothsay Street is a no through road and therefore the only traffic traversing the area is local traffic from residential car parks. The junction of Rothsay Street and Alice Street is adequate to cater for right turning traffic from Rothsay Street and there is ample carriageway width so that no head on conflict is experienced between opposing traffic movements at the junction.

Whilst Green Walk and Alice Street are narrow adjacent to existing parking bays, there are sections of carriageway that have been designed as waiting areas to allow for oncoming traffic to pass a vehicle travelling in the opposite direction. Both streets are regularly traversed by refuse vehicles and larger lorries delivering to the builders merchants and the recently installed road layout has been designed using swept path analysis to ensure that larger vehicles are able to traverse both roads without mounting the footway.

It must be noted that the narrow sections of carriageway alluded to by residents are on straight sections of road, not junctions, so the correct vehicle alignment can be undertaken when traversing past the inset parking bays.

A number of respondents objected on the grounds that the cycle route will create a lot of noise for residents and is a waste of tax payers money and council resources. *

* In response, there is no increase in traffic proposed along Rothsay Street which incorporates the Quietway route. Whilst the numbers of cyclists are expected to increase as a result of the route formalisation and upgrade in infrastructure, cycling does not result in an increase in ambient noise levels or levels of pollution.

The proposed measures along the Quietway route align with the council's emerging cycling strategy and the Mayor's Vision for Cycling. The Mayor has commitment to invest total of £913m over the next 10 years in cycling safety and infrastructure development to significantly increase the modal share in cycling as a safe, healthy and sustainable form of transport in London.

The London boroughs will play a central role in delivering this vision, by helping to develop, fund and deliver better and safer routes for cycling.

The council welcomes significant investment from Transport for London to take forward the Quietway programme that will not only significantly upgrade a number of existing cycle routes, but also make considerable improvements to the streetscape for the benefits of all road users and local residents.

A number of respondents objected stating that it is already dangerous turning out of Rothsay Street into Tower Bridge Road and it is even more dangerous turning out of Green Walk, especially with the speed of traffic coming off the roundabout. *

* In response, there are adequate sightlines for vehicles exiting Green Walk into Tower Bridge Road. To the south of the junction on Tower Bridge Road there is no parking, so the kerbline is free from parking and obstruction from Green Walk to the Bricklayers Arms roundabout. The operation of a pelican crossing facility at the roundabout also provides gaps in northbound traffic on Tower Bridge Road so vehicles can safely exit Green Walk. Sightlines to the north of the junction are also adequate, with red route parking prohibitions adjacent to the kerb for approximately 15m.

In peak traffic flow periods there is a significant amount of congestion on Tower Bridge Road which means traffic speeds are minimal. This will also assist with egress of vehicles from Green Walk. However it is noted that an additional keep clear marking may be required to ensure that vehicles on Tower Bridge Road do not block vehicles exiting Green Walk.

The amount of additional traffic proposed to use this junction as a result of the one-way proposal is minimal and is estimated at a maximum of 15 vehicles per hour, which is recorded turning count value of vehicles exiting Rothsay Street from 8am – 9am on weekday period.

A number of objections discussed that the proposals will increase traffic past residential properties in Alice Street and Green Walk, which will be detrimental safety and environmental impact on residents. *

* In response, as discussed above, recent traffic counts in peak periods undertaken by Transport for London that have been used as part of the base model development for the signalisation of the Rothsay Street / Tower Bridge Road junction clearly show that the traffic turning out of Rothsay Street is minimal, which is to be expected for a street that just services a number of residential car parks. Therefore the additional eastbound traffic that will traverse down Alice Street and Green walk will be minimal. As a result, there is expected to be no direct impact on the safety of pedestrians or amenity of local residents.

The council recently upgraded all footways in Alice Street and Green Walk to improve pedestrian accessibility and safety. The footways, raised table at the junction of Green Walk and Alice Street and pedestrian crossing locations junction will remain unaffected by the proposals. Narrow sections of footway in Alice Street and Green Walk are either protected by high kerbs that prevent vehicle overrun or parking bays which totally separate pedestrians from vehicle flows on the adjacent carriageways.

Respondents objected indicating that Alice Street regularly becomes impassable due to delivery vehicles at the gates to the Jam Factory and it will be impossible to get out. Alice Street and Green Walk are not wide enough to cope with two-way traffic. *

* In response, there are double yellow line parking prohibitions at the junction of Green Walk and Alice Street that should prevent indiscriminately parked vehicles. Parking adjacent to this junction clearly blocks sightlines for pedestrians using the pedestrian crossing locations and narrows carriageway width, potentially obstructing vehicle flows in both roads. It is illegal to park across a pedestrian dropped kerb and parking on the southern kerblines of Green Walk adjacent to the junction will result in those vehicles potentially committing an offence.

It is evident from responses received from numerous residents that there is an issue at this location, most likely related to deliveries to the Jam Factory. Such indiscriminate parking on or around the Green Walk / Alice Street junction, can be addressed with targeted enforcement of the parking prohibitions.

It must be noted that both Green Walk and Alice Street currently operate with two-way traffic. As outlined earlier the sections of narrow carriageway in both Alice Street and Green Walk are only for short lengths, with wider sections of carriageway at the terminus of the inset parking bays to allow for two way traffic movement and for vehicles to pass each other without conflict. Areas of wider carriageway width have been retained in Alice Street on approach to the Rothsay Street junction, along the central section of the road and at the junction of Green Walk in line with the potential conflict areas. Wider carriageway sections have been retained in Green Walk at the junctions of Tower Bridge Road and Alice Street.

It must also be noted that Rothsay Street is also a narrow road with similar sections of carriageway that are not wide enough to cater for two-way traffic, particularly to the west of Alice Street, where the majority of residential car parks

are located. There have been no reports of conflict difficulty from residents traversing this carriageway.

A respondent objected highlighting that traffic entering Green Walk will have to do a U-turn in Rothsay Street to leave, which will result in safety implications for pedestrians and cyclists. *

* In response, non-local traffic entering Green Walk will be unable to exit via Rothsay Street. However it is noted that the level of non-local traffic entering the area is minimal as the roads only provide access to residential car parks. As vehicles can enter Rothsay Street from Tower Bridge Road the only reason non-local traffic entering the area from Green Walk is to access the Jam Factory vehicle gates at the western end of Green Walk or access the loading bay / parking area at the rear of the hardware store in Alice Street. Both sites have facilities where vehicles can turn in order to leave via Green Walk to access Tower Bridge Road.

A much better routeing for non-local traffic would be to enter the area through Rothsay Street and then traverse along Alice Street into Green Walk to regain access to Tower Bridge Road. This would remove the requirement to turn in order to exit the area.

It can be argued that the proposed configuration of the roads as part of this scheme will actually reduce traffic movements at the junction of Alice Street and Rothsay Street and eastern section of Rothsay Street adjacent to Tower Bridge Road, which will actually improve pedestrian and cycle safety on the Quietway route.

A respondent objected saying that Rothsay Street is the main entrance into the Meakin Estate for emergency services. This will be affected as there will be no room for them in the first part of Rothsay Street. *

* In response, Rothsay Street is proposed to be one-way westbound from Tower Bridge Road to Alice Street and therefore access for emergency vehicles into Rothsay Street to access the Jam Factory or the Meakin Estate is not compromised. Proposed kerbside parking prohibitions along the eastern end of Rothsay Street will prevent obstruction of the carriageway. It is also noted that the contraflow cycle lane is only semi-segregated and that emergency vehicles can encroach into the cycle lane in an emergency if required.

A number of objections highlighted that the increased traffic in Alice Street will make it a more hazardous environment for the elderly and children and affect the quality of life of residents whose flats and homes overlook Alice Street. *

* In response, as outlined above, there is no evidence to suggest that there will be any adverse effect on the quality of life and safety of residents in Alice Street or Green Walk. The additional traffic that will traverse down Alice Street and Green Walk is minimal and certainly not a volume that will result in any safety concerns or loss of amenity to local residents. The fact that both streets successfully operate with two-way traffic also demonstrates that there should not be any issues with routeing eastbound traffic from Rothsay Street down Alice Street and Green Walk.

Objections were received stating that traffic on Tower Bridge Road will become even more impeded and congested. Everyday traffic backs up along Tower Bridge Road and installing signals will make it even worse. *

* In response, there is a genuine safety concern for cyclists crossing Tower Bridge Road at this location. Currently more cyclists use Rothsay Street and Webb Street than motorised traffic in the morning peak and on many occasions, due to high traffic saturation on Tower Bridge Road, are forced to weave across the junction between stationary traffic.

With cycle volumes expected to increase to over 400 in the next couple of years as the Quietway route become more popular, this situation will get even worse. This is particularly pertinent as the majority of accidents involving cyclists take place at busy junctions like the one at Tower Bridge Road, which effectively act as severance along the route and are off-putting to new and less confident cyclists.

Signalising the junction will significantly reduce the existing conflict issues experienced at this junction. Cyclists will be able to safely traverse the junction accessing either Webb Street or Rothsay Street via a green traffic signal. The fact that traffic movements at the junction have been rationalised so that only cyclists exit both Webb Street and Tower Bridge Road also greatly improves safety for cyclists.

Proposed upgrades to the existing pelican and raised carriageway tables at both Webb Street and Rothsay Street being taken forward by TfL will also improve pedestrian accessibility and safety.

The junction improvements are to be fully modelled and the site linked to both Bricklayers Arms roundabout and the Grange Road junction to ensure that traffic signals are coordinated to provide the most efficient operation to benefit all road users. Ultimately, the decision to install new signals at this junction will sit with TfL as the highway and traffic authority for Tower Bridge Road.

An objection was received from a local business indicating that they currently use Rothsay Street for loading and turning it to one way will severely affect their trade and access for customers. *

* In response, Transport for London's TLRN network (Red Route) extends into Rothsay Street adjacent to this business. Therefore the council is unable to change existing kerbside loading provision or propose new measures.

Loading provision for local businesses in Tower Bridge Road is currently being reviewed as part of TfL's Streetscape improvement upgrades. These improvements include rationalising all parking and loading provision. Therefore businesses are being given the opportunity to formally reply to TfL's consultation in order to achieve the right level of loading for local businesses and parking provision for customers.

2.3.9 15% respondents did not submit a further comment.

2.4 Levels of Consensus

2.4.1 The following majority levels of agreement have been given in relation to the questions contained within the consultation document:

- 40% of respondents support the introduction of the Quietway cycle route proposals for Site H;
- 57% of respondents are opposed to the proposals; and
- 3% of respondents had no opinion.

2.5 Statutory Consultee Replies

2.5.1 Four statutory consultees provided a reply to the consultation.

- a) **Southwark Living Streets** replied indicating strong support for the scheme and stated that crossing Tower Bridge Road will be much safer for cyclists. The use of the green space to incorporate a cycle lane segregated from pedestrians, whilst still retaining a section of garden area for local amenity is really good.
- b) **Lambeth Cyclists Cyclists** replied in support of the scheme and changes at Tower Bridge Road and new cycle access at the western end of Rothsay Street. A concern was raised regarding the proximity of the cycle route to the Wilds Rents / Weston Street junction. Consideration should be given to squaring up the junction making the cycle access a crossroads. *

* In response, the Law Street / Weston Street / Wilds Rents junction is being improved to reduce traffic speeds, particularly turning manoeuvres into Laws Street. This is being achieved through the introduction of footway buildouts and tighter corner radii. The measures will be fully safety audited and monitored as part of the post implementation process to ensure there are no adverse safety issues for cyclists.

- c) **Southwark Cyclists** replied indicating support for the scheme and requested that some form of right turn pocket marking is installed in Law Street adjacent to the cycle path leading into Rothsay Street, so it is clear to motorists that cyclists can wait if traffic is turning into Law Street. *

* In response, this request will be investigated as part of the detailed design process and if possible, incorporated into the final scheme design.

- d) **Sustrans** replied indicating support for the scheme and indicated that the solution at the western end of Rothsay Street is good for both pedestrians and cyclists.

2.5.2 No objections were received from Ward Members throughout the consultation period.

2.5.3 Detailed comments were received from the directorate of the Jam Factory Freehold Limited outlining their opposition to the proposals summarised below;

- The usage of the Jam Factory entrance onto Green Walk is extremely busy at all times of the day and therefore conflict may occur with additional traffic in Alice Street and Green Walk.

- Having a one way section in Rothsay Street to prevent motor traffic exiting onto Tower Bridge Road will create havoc and will make Alice Street and Green Walk unsafe and polluted as it will be difficult for vehicle to exit out onto Tower Bridge Road resulting in congestion.
- To cause disruption to residents for a transient cycling population who have no commitment to the area is disproportionate.

2.5.4 Detailed comments were received from the Jam Factory Residents' Association expressing its opposition to the proposed cycle route in Rothsay Street. The points of objection are summarised below;

- Increased risk to pedestrians from higher traffic flows along narrow roads.
- Increased traffic congestion and pollution especially in Green Walk.
- Increased danger to pedestrians at the Alice Street / Rothsay Street junction from large vehicles having to now turn into Alice Street and increased danger to pedestrians traversing Green Walk.
- Loss of amenity for those residents of the Jam Factory whose flats face Alice Street.
- Creation of a bottleneck at the Jam Factory entrance on Green Walk.

2.5.5 Officers' response to all of the issues raised by Jam Factory Freehold Ltd and Jam Factory Residents' Association is covered in the detailed commentary in section 2.3 below.

3.0 Recommendations

3.1 In light of the comments received by residents and stakeholders throughout the consultation process, the following recommendations have been made regarding scheme implementation;

i) Section A

- Although 57.5% of respondents objected to the proposals, in light of there being minimal traffic flow on Rothsay Street in peak periods that will have a negligible impact on Alice Street and Green Walk, and the very short nature of the proposed diversion, and the Council's desire to see a 'step-change' in levels of provision for cycling particularly on key routes such as this, it is recommended that this element of the scheme is progressed to implementation.

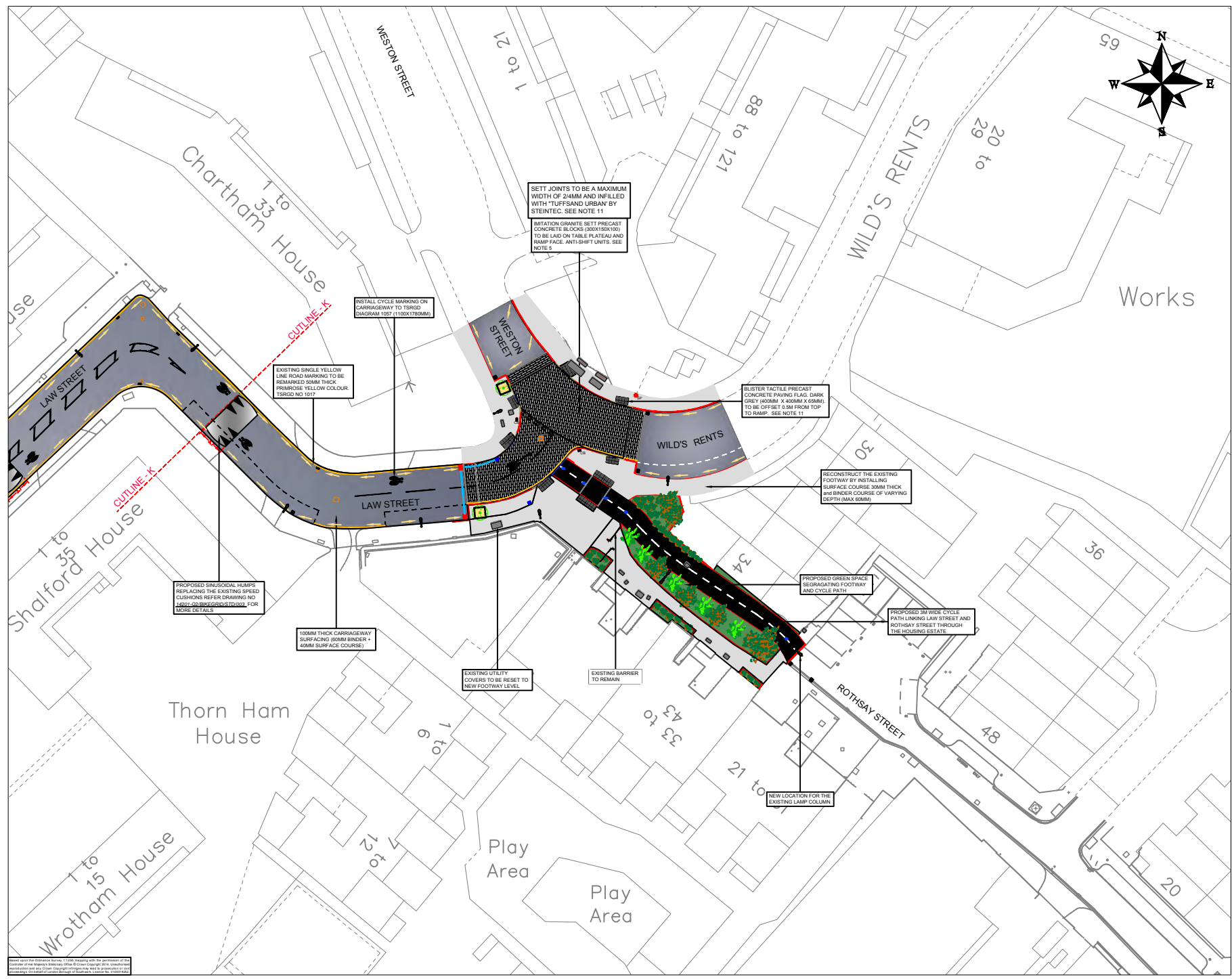
ii) Section B

- It is recommended that the proposals consulted upon for Section B are progressed to implementation.

Appendices

Appendix A:	Initial Scheme Design
Appendix B:	Consultation Documents
Appendix C:	Location Plan and Extents of Consultation
Appendix D:	List of Addresses within the Distribution Area

Appendix A: Initial Scheme Design



LEGEND / NOTES.

KEY:

- CARRIAGEWAY RECONSTRUCTION:** 40mm THICK TC/CS 510 REV 89 LAD ON 60mm THICK SAND 14 EN 4050 oes WTR 2 TO CL/37 OR CARRIAGEWAY REPAIR CONSTRUCTION.
- FOOTWAY RECONSTRUCTION:** PROVIDE AND LAY PCC FINING FLAG LAD ON 200mm THICK TYPE 1 SUBBASE. 30mm THICK LOW FINES BEDDING SAND.
- RAISED FEATURES:** PROVIDE AND LAY ANTI-SHIFT PC BLOCKS 300 x 150 x 100mm FOR RAISE TABLE CONSTRUCTION LAD ON 350mm THICK CR/CA/20 AND 30mm THICK LOW FINES BEDDING SAND.
- BLISTER PAVING:** PROVIDE AND LAY PCC BLISTER TACTILE PAVERS 400 x 400 x 75mm THICK (REF. T18) NEW/75) ON SPECIFIED FOOTWAY CONSTRUCTION LAD ON 200mm THICK CR/CA/20 SUBBASE AND 30mm THICK "TUFBED" LAYING MORTAR BY STEINTEC.
- IRONWORK:** EXISTING IRONWORK COVER AND FRAME TO BE REINFORCED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INSTALL FLAT KERB 150 x 300MM - UNITS SILVER GREY.
- STANDARD GRANITE KERBS:** INSTALL NEW STANDARD GRANITE KERBS (150MM X 300MM) LAD WITH 100MM UPSTAND ON ST4 (INCLUDING FRONT KERB HAUNCHING TO BE 100MM BELOW CARRIAGEWAY). REFER TO DRAWING LBS(C)03 KERBS AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1(SHEET 1 OF 4) TYPE R + TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE KERBS FINISHED:** INSTALL NEW STANDARD GRANITE KERBS (150MM X 300MM) LAD ON ST4 (INCLUDING FRONT KERB HAUNCHING TO BE 100MM BELOW CARRIAGEWAY). REFER TO DRAWING LBS(C)03 KERBS AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1(SHEET 1 OF 4) TYPE R + TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS. NEW PRIMARY TRAFFIC SIGNAL POST LOCATION.
- STANDARD GRANITE TRANSITION KERBS:** NEW STRAIGHT KERB TYPE T (K15-T) IN LBS(C)03. TRANSITION KERB TYPE K15-T(S) (S1) REFER TO DRAWING LBS(C)03 KERBS AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1(SHEET 1 OF 4) TYPE R + TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- NEW WHITE LINE ROAD MARKING.
- NEW YELLOW LINE ROAD MARKING.
- NEW DRAIN PIPE:** PROVIDE AND INSTALL SURFACE WATER DRAIN PIPE GROUP 6.150mm INTERNAL DIAMETER DEPTH OF INVERT $\leq 0.2m$. AVERAGE DEPTH OF 750MM ON BED TYPE 2. DEPTH OF 750MM ON BED TYPE Z.
- NEW GULLY:** PROVIDE AND INSTALL PRECAST CONCRETE TRAPPED GULLY WITH TYPE D COVER. GULLY DEPTH $\leq 750mm$ OR BUILD CATCH PIT WHERE SPECIFIED.
- EXISTING ROAD GULLY TO BE CONVERTED TO MANHOLE.
- EXISTING ROAD GULLY TO BE RAISED AND ADJUSTED.
- NEW LAMP COLUMN LOCATION.
- EXISTING LAMP COLUMN LOCATION.
- NEW TREE PIT CONSTRUCTION (REFER TO STANDARD DRAWING DETAILS NO. 14021-Q2/BIKEGRD/ST0001)
- EXISTING / PROPOSED LONGITUDINAL FALL ON CARRIAGEWAY
- EXISTING BELL BOLLARD TO BE RELOCATED AS SHOWN IN THE DRAWING
- PROPOSED LOCATION FOR THE SIGN POSTS
- PROPOSED CITY TYPE BOLLARD BY FURNITURES
- PROPOSED GREEN SPACE

Revision	Date	Amendment	Drawn	Design	Checked	Approved

PUBLIC REALM PROJECTS TEAM 150 TOOLEY STREET SE1P 6LX

Project
CENTRAL LONDON BIKE GRID
Q2 - BLUE ROUTE

Title
GENERAL ARRANGEMENT
SECTION-11
LAW STREET / ROTHSAY STREET

Contract No.	HW2013 NEC3	Drawn	CH
Scale	1 : 250 @ A1	Purpose	CONSTRUCTION
Drawing No.	14201-Q2/BIKEGRD/GA/011	Checked	CH
Date Drawn	JUNE 2014	Approved	CH
Date Issued	JULY 2014		

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Appendix B: Consultation Documents

We want your views

It is important for all consultees to respond to the consultation. We would be grateful if you could take the time to review the proposals outlined in this document and provide a response using the pre-paid envelope and questionnaire provided by **Friday 5th September 2014**

Alternatively, you can view the proposals at www.southwark.gov.uk/consultations and complete the online questionnaire.

Your views are essential for us to understand your requirements for the proposal and form a fundamental part of the scheme development process, whether you use public transport, cycle, walk or drive a private vehicle.



Quietway
Greenwich to Waterloo

Site H – Rothsay Street

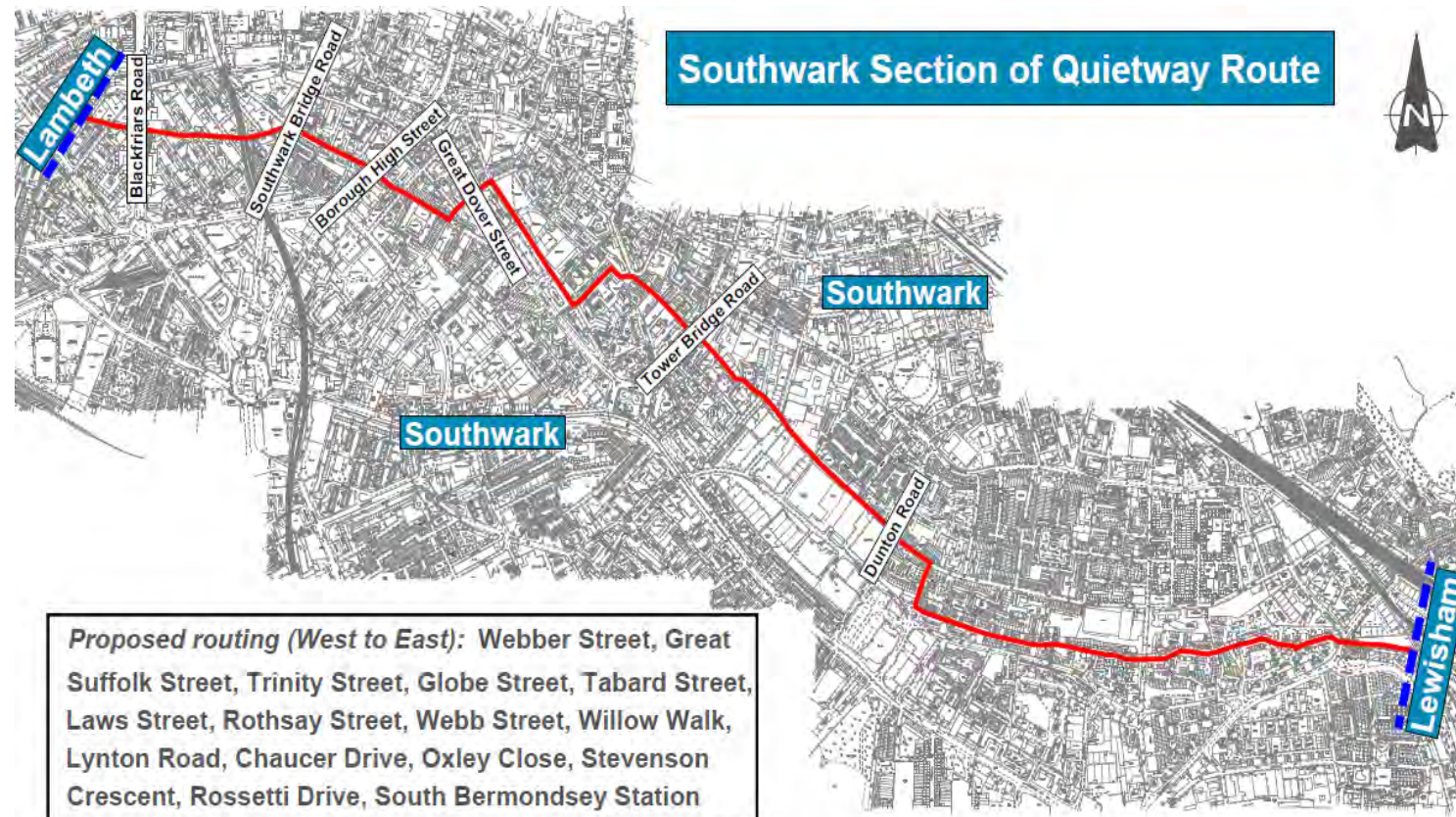
Traffic Management and Cycling Accessibility Measures

What happens next?

The responses to the questionnaire will be analysed and taken into account in the final design of the proposed works. As you will appreciate Southwark Council receives many comments from consultations and therefore are unable to respond personally to specific issues raised. However all comments and suggestions will be taken into consideration before a decision is made. The consultation results and recommendations will be presented at a local community council meeting in September 2014.

Should you require any further information regarding the proposed scheme please do not hesitate to contact Chris Mascord at chris.mascord@southwark.gov.uk

Further information on other schemes along the route in Southwark can also be found at: www.southwark.gov.uk/consultations



Have your say

Southwark Council is holding a consultation to receive residents' and key stakeholders' comments regarding proposals relating to the borough's section of the Quietway (Greenwich to Waterloo) cycle route. This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey station in the east and traversing local roads to The Cut in the west. The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

Background

Quietways are a network of bike routes for less confident cyclists using low-traffic back streets. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.

What are the proposed changes?

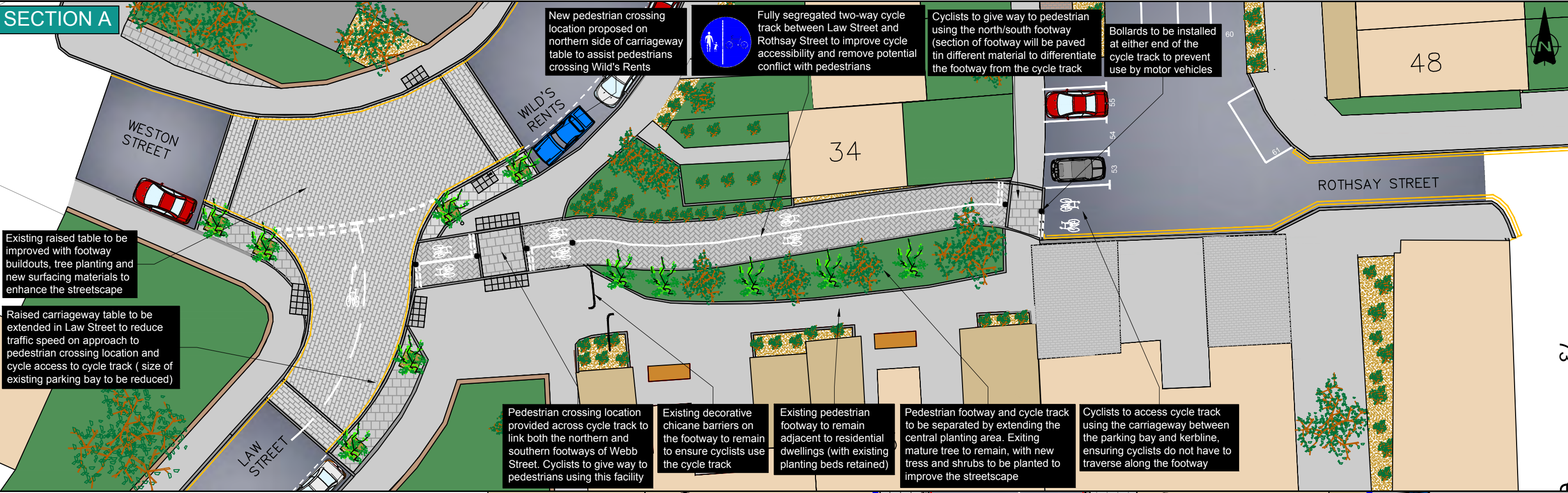
Section A

- Fully segregated cycle track between Rothsay Street and Laws Street / Wilds Rents with existing pedestrian footway remaining in current location.
- Cycle track and footway to be separated by extension of central planting area. All existing mature trees to be retained.
- Existing raised carriageway table at junction of Wild's Rents / Weston Street / Law Street junction to be extended in Law Street to reduce traffic speeds and assist cyclists accessing Rothsay Street. Carriageway table and surrounding footways to be repaved to improve the streetscape.
- Footways adjacent to the carriageway table to be built out with new tree planting introduced where possible to improve the streetscape. Wider footways will reduce traffic speeds and improve pedestrian accessibility.
- Law Street carriageway to be resurfaced and existing speed cushions to be converted to sinusoidal humps to improve the ride quality for cyclists using this section of the route.

Section B

- The junction of Tower Bridge Road / Rothsay Street / Webb Street is to be signalised as part of Transport for London's streetscape improvement scheme in Tower Bridge Road that has been consulted on separately.
- Rothsay Street to be one way westbound for general traffic from Tower Bridge Road to Alice Street. Cyclists will be able to travel eastbound and access Tower Bridge Road and Webb Street via a green traffic signal.
- Traffic in Rothsay Street and Alice Street will be able to access Tower Bridge Road via Green Walk.

SECTION A



New pedestrian crossing location proposed on northern side of carriageway table to assist pedestrians crossing Wild's Rents

Fully segregated two-way cycle track between Law Street and Rothsay Street to improve cycle accessibility and remove potential conflict with pedestrians

Cyclists to give way to pedestrian using the north/south footway (section of footway will be paved in different material to differentiate the footway from the cycle track)

Bollards to be installed at either end of the cycle track to prevent use by motor vehicles

Existing raised table to be improved with footway buildouts, tree planting and new surfacing materials to enhance the streetscape

Raised carriageway table to be extended in Law Street to reduce traffic speed on approach to pedestrian crossing location and cycle access to cycle track (size of existing parking bay to be reduced)

Pedestrian crossing location provided across cycle track to link both the northern and southern footways of Webb Street. Cyclists to give way to pedestrians using this facility

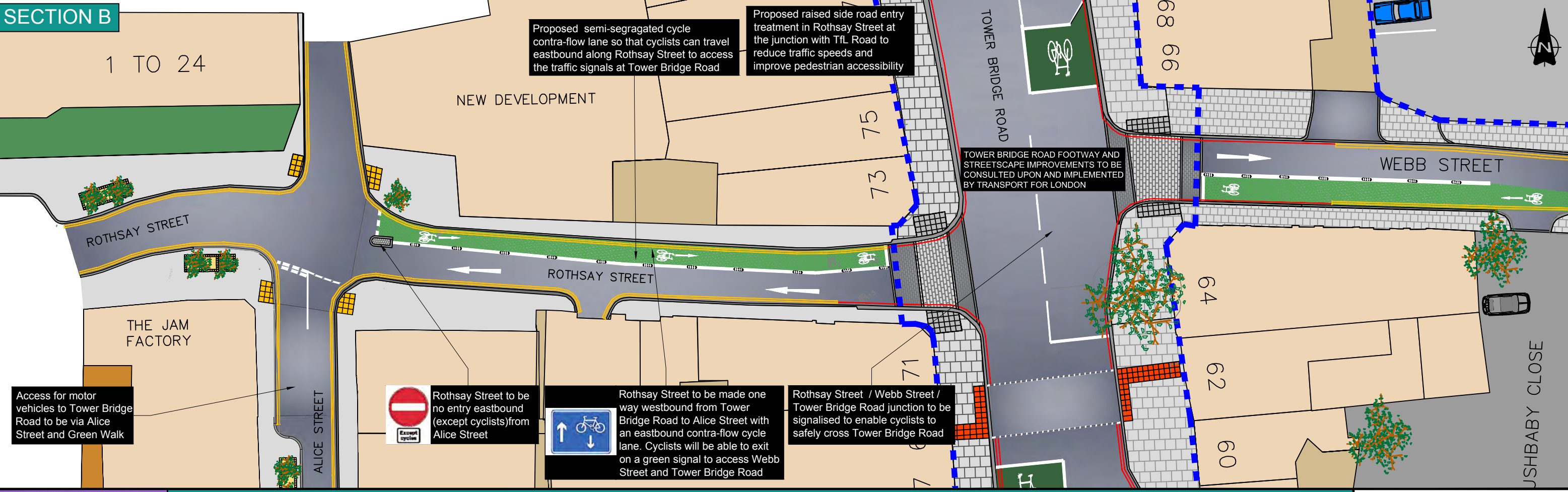
Existing decorative chicane barriers on the footway to remain to ensure cyclists use the cycle track

Existing pedestrian footway to remain adjacent to residential dwellings (with existing planting beds retained)

Pedestrian footway and cycle track to be separated by extending the central planting area. Existing mature tree to remain, with new trees and shrubs to be planted to improve the streetscape

Cyclists to access cycle track using the carriageway between the parking bay and kerbline, ensuring cyclists do not have to traverse along the footway

SECTION B



Proposed semi-segregated cycle contra-flow lane so that cyclists can travel eastbound along Rothsay Street to access the traffic signals at Tower Bridge Road

Proposed raised side road entry treatment in Rothsay Street at the junction with TfL Road to reduce traffic speeds and improve pedestrian accessibility

TOWER BRIDGE ROAD FOOTWAY AND STREETSCAPE IMPROVEMENTS TO BE CONSULTED UPON AND IMPLEMENTED BY TRANSPORT FOR LONDON

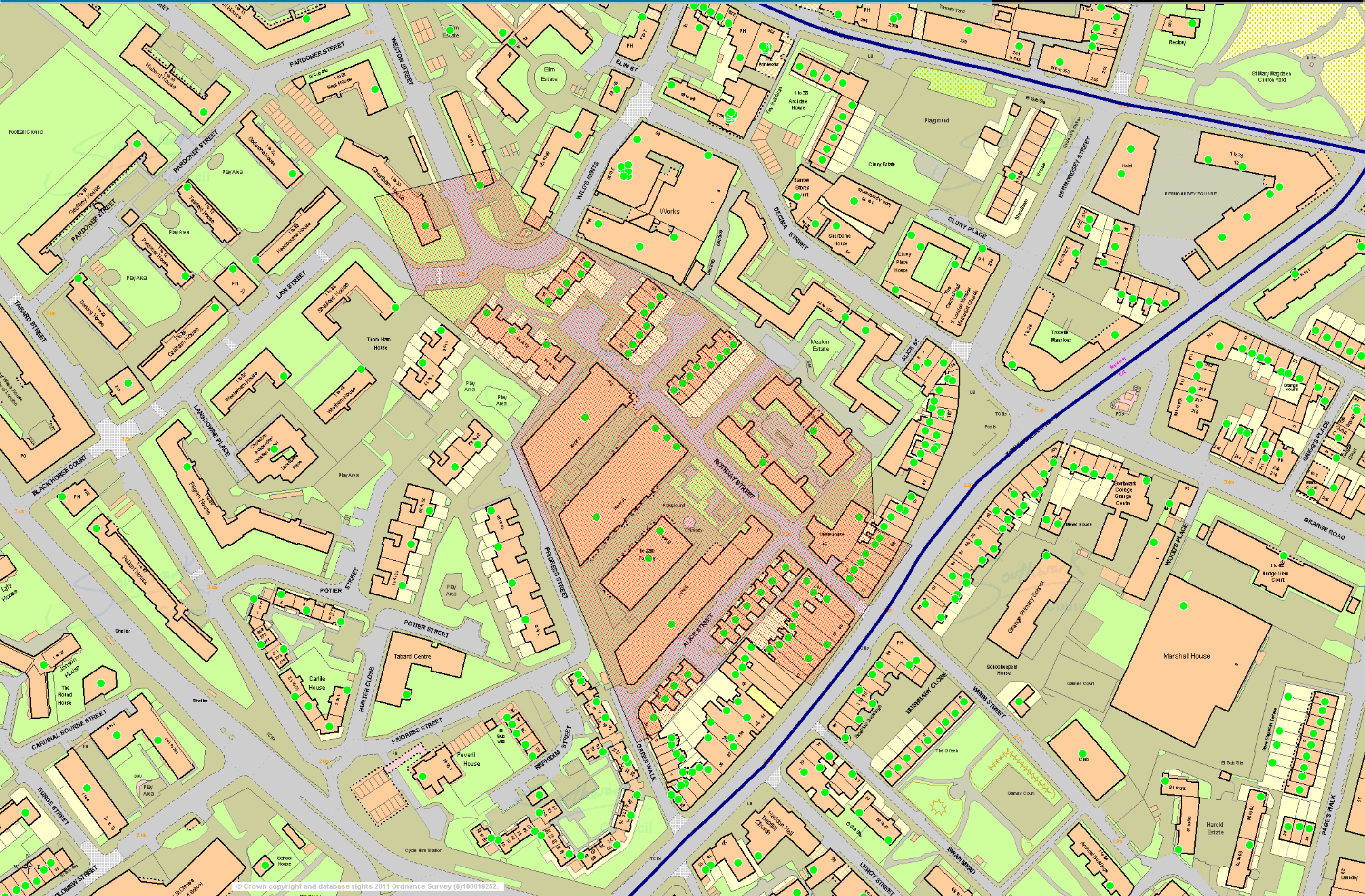
Access for motor vehicles to Tower Bridge Road to be via Alice Street and Green Walk

Rothsay Street to be no entry eastbound (except cyclists) from Alice Street

Rothsay Street to be made one way westbound from Tower Bridge Road to Alice Street with an eastbound contra-flow cycle lane. Cyclists will be able to exit on a green signal to access Webb Street and Tower Bridge Road

Rothsay Street / Webb Street / Tower Bridge Road junction to be signalised to enable cyclists to safely cross Tower Bridge Road

Appendix C: Location Plan and Extents of Consultation



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Appendix D: List of Addresses within Distribution Area

FLAT 202	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 203	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 204	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 306	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 307	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 308	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 401	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 402	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 403	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 404	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 405	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 406	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 407	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 408	21A	ROTHSAY STREET	LONDON SE1 4BF
FLAT 1	21A	ROTHSAY STREET	LONDON SE1 4BF
		32 WILDS RENTS	LONDON SE1 4QG
		33 WILDS RENTS	LONDON SE1 4QG
		30 ROTHSAV STREET	LONDON SE1 4UE
		32 ROTHSAV STREET	LONDON SE1 4UE
		34 ROTHSAV STREET	LONDON SE1 4UE
		36 ROTHSAV STREET	LONDON SE1 4UE
		40 ROTHSAV STREET	LONDON SE1 4UE
		42 ROTHSAV STREET	LONDON SE1 4UE
		55 ROTHSAV STREET	LONDON SE1 4UF
		26 ROTHSAV STREET	LONDON SE1 4UE
		46 ROTHSAV STREET	LONDON SE1 4UE
		47 ROTHSAV STREET	LONDON SE1 4UF
	61-63	TOWER BRIDGE ROAD	LONDON SE1 4TL
		21 ROTHSAV STREET	LONDON SE1 4UF
		25 ROTHSAV STREET	LONDON SE1 4UF
		1 ROTHSAV STREET	LONDON SE1 4UD
		73 TOWER BRIDGE ROAD	LONDON SE1 4TW
		79 TOWER BRIDGE ROAD	LONDON SE1 4TW
		81 TOWER BRIDGE ROAD	LONDON SE1 4TW
		83 TOWER BRIDGE ROAD	LONDON SE1 4TW
		ROTHSAV STREET	LONDON SE1 4UD
	1A	65 TOWER BRIDGE ROAD	LONDON SE1 4TL
		49 ROTHSAV STREET	LONDON SE1 4UF
		44 ROTHSAV STREET	LONDON SE1 4UE
	61A	TOWER BRIDGE ROAD	LONDON SE1 4TL
		59 TOWER BRIDGE ROAD	LONDON SE1 4TL
		30 WILDS RENTS	LONDON SE1 4QG
		20 ROTHSAV STREET	LONDON SE1 4UE
		22 ROTHSAV STREET	LONDON SE1 4UE
		17 ROTHSAV STREET	LONDON SE1 4UF
		19 ROTHSAV STREET	LONDON SE1 4UF
		27 ROTHSAV STREET	LONDON SE1 4UF
		31 ROTHSAV STREET	LONDON SE1 4UF
		35 ROTHSAV STREET	LONDON SE1 4UF
		37 ROTHSAV STREET	LONDON SE1 4UF
		39 ROTHSAV STREET	LONDON SE1 4UF
		41 ROTHSAV STREET	LONDON SE1 4UF
		45 ROTHSAV STREET	LONDON SE1 4UF
		31 WILDS RENTS	LONDON SE1 4QG
		75 TOWER BRIDGE ROAD	LONDON SE1 4TW
FLAT 11 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
	1B	ROTHSAV STREET	LONDON SE1 4UD
FLAT 102 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 106 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 201 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 301 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 203 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 603 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 103 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 1 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 2 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 606 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 607 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 4 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 5 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 6 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 7 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 1 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 3 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 12 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 13 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 15 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 8 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 9 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 610 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 611 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 612 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 205 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 305 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 304 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 303 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 302 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 104 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 404 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 411 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 501 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 201 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 202 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 104 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 105 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 106 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 107 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 3 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 5 LOWER GROUND BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 6		65 TOWER BRIDGE ROAD	LONDON SE1 4TL
FLAT 3		65 TOWER BRIDGE ROAD	LONDON SE1 4TL
FLAT 4		65 TOWER BRIDGE ROAD	LONDON SE1 4TL
FLAT 5		65 TOWER BRIDGE ROAD	LONDON SE1 4TL
FLAT 506 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 507 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 406 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 206 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 602 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
FLAT 603 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 604 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 605 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 606 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 301 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 203 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
FLAT 105 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 16 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 17 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 205 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 304 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 305 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
FLAT 306 BLOCK A		27 GREEN WALK	LONDON SE1 4TT

HASSAN BROS
GREGGS OF TWICKENHAM LTD
DOLPHIN GLOBAL SERVICES LTD
SOBO LTD

COSTCUTTER

HING LOONG NOODLE HOUSE

		31 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		15 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		20 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		22 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		23 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		24 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		3 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		5 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		6 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		7 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		8 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		1 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		10 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
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		1 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		10 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		11 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		12 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		14 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		15 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		16 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		18 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
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		2 ELIM ESTATE	WESTON STREET	LONDON SE1 4BY
		40 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		41 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
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		38 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		46 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		47 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		48 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		51 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		54 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		55 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		57 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		58 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
	FLAT 24A	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 29A	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 3	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 31	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 32	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 33	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 4	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 6	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 7	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 8	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
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	FLAT A		73 TOWER BRIDGE ROAD	LONDON SE1 4TW
		75A	TOWER BRIDGE ROAD	LONDON SE1 4TW
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		17 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		18 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		2 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
	FLAT 1	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 11	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 12	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 13	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
	FLAT 15	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
		6 TAYET TOWERS, 3	ROTHSAY STREET	LONDON SE1 4UH
		7 TAYET TOWERS, 3	ROTHSAY STREET	LONDON SE1 4UH
		11 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		12 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		13 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QN
		26 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		28 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		29 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		30 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
	FLAT 302 BLOCK C		27 GREEN WALK	LONDON SE1 4TQ
	FLAT 1		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 2		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 3		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 5		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 6		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
ESO RECRUITMENT SERVICES LTD (IN VOL LIQUIDATION)	BASEMENT AND GROUND FLOOR		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 101 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
	FLAT 4		63 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 306 BLOCK C		ROTHSAY STREET	LONDON SE1 4QW
	FLAT 304 BLOCK C	43 MEAKIN ESTATE	27 GREEN WALK	LONDON SE1 4TO
	FLAT 503 BLOCK C		27 GREEN WALK	LONDON SE1 4TO
IMAGE MAGIC			27 GREEN WALK	LONDON SE1 4TO
	FLAT 8 LOWER BLOCK B		71 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 7A BLOCK B		27 GREEN WALK	LONDON SE1 4TX
			27 GREEN WALK	LONDON SE1 4TX
			27 GREEN WALK	LONDON SE1 4TX
HOLBORN DIRECT MAIL CO		52 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		56 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		2-6	ROTHSAY STREET	LONDON SE1 4UD
	FLAT 30	CHARTHAM HOUSE	WESTON STREET	LONDON SE1 4DX
TOWER CAFE AND KEBAB HOUSE			57 TOWER BRIDGE ROAD	LONDON SE1 4TL
	FLAT 404 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
	FLAT 405 BLOCK B		27 GREEN WALK	LONDON SE1 4TX
	FLAT 510 BLOCK A		27 GREEN WALK	LONDON SE1 4TT
		32 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
		53 MEAKIN ESTATE	ROTHSAY STREET	LONDON SE1 4QW
			53 ROTHSAAY STREET	LONDON SE1 4UF
			29 ROTHSAAY STREET	LONDON SE1 4UF

London Borough of Southwark



Quietway Cycling Proposals

Site I – Tabard Street

Public Consultation Summary

September 2014

London Borough of Southwark

Site I - Quietway Cycling Proposals Tabard Street

Public Consultation Summary

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1.0 Introduction

1.1 Background

1.1.1 This document report has been produced by the London Borough of Southwark Public Realm Projects Group, to provide a summary of the consultation exercise for the Quietway Cycling Proposals for Site I on Tabard Street between Laws Street and Pilgrimage Street. The measures are being drafted by the Public Realm Projects Team, with the project manager for this scheme being Chris Mascord (Senior Engineer).

1.1.2 The area under consideration is located within the SE1 district of Southwark (Chaucer Ward) in the north of the borough. See figure 1 below.

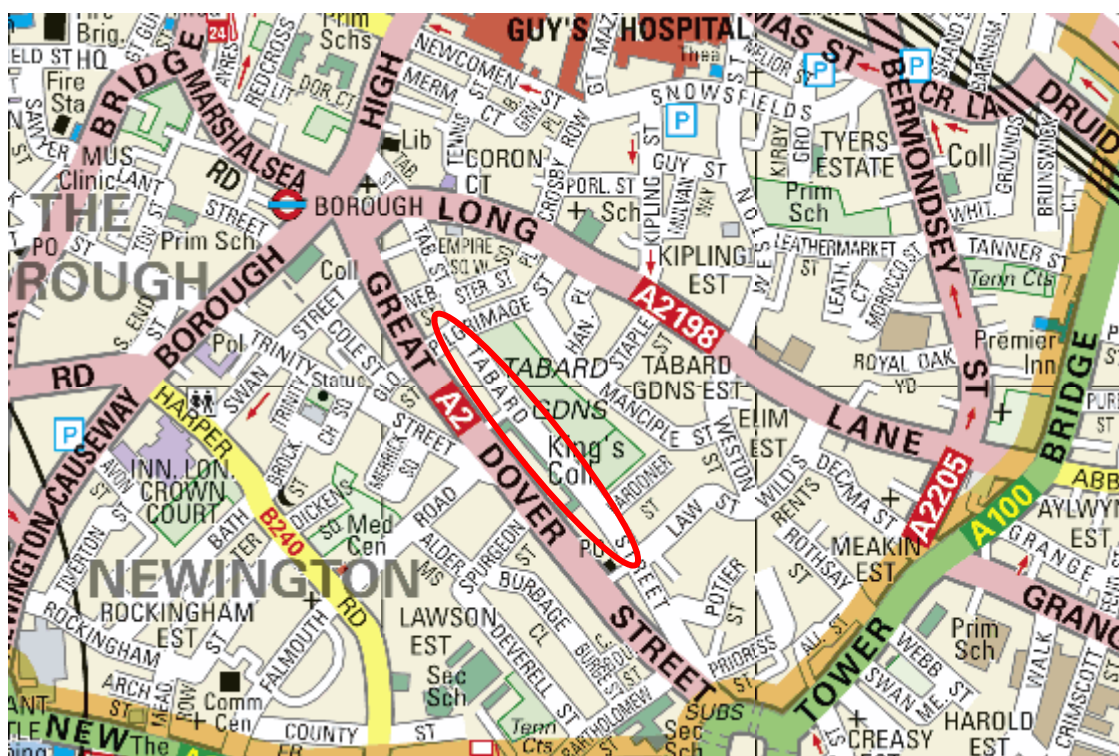


Figure 1: Location of proposed scheme

1.2 Project and Objectives

1.2.1 This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey Station in the east and traversing local roads to The Cut in the west (see figure 2). The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

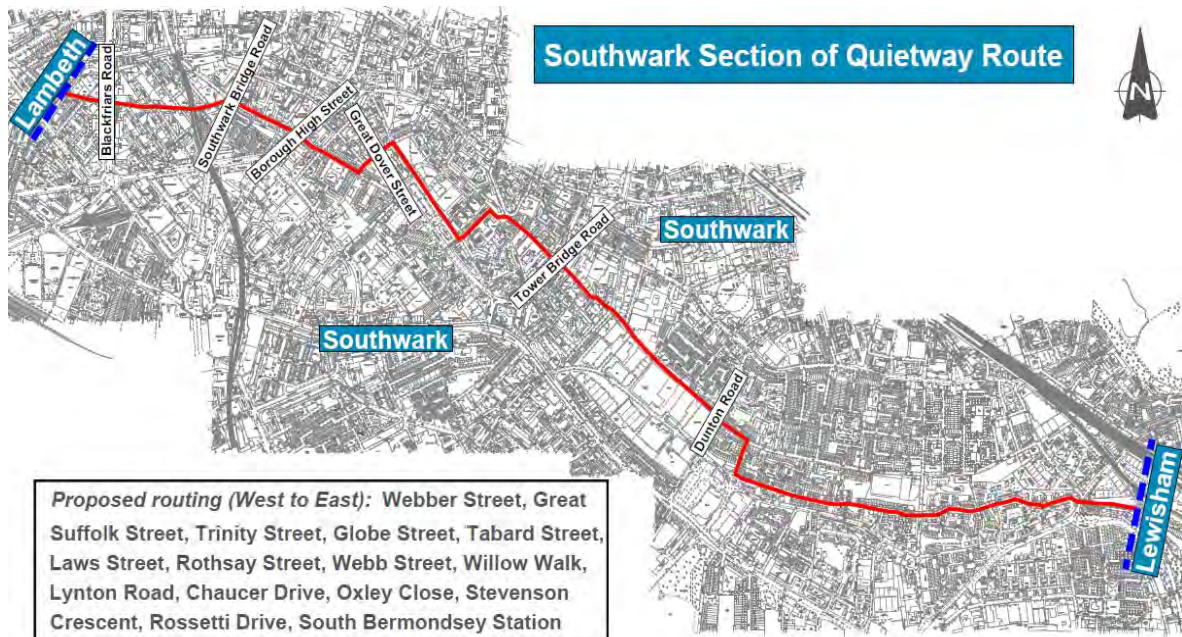


Figure 2: Southwark's section of Quietway Route

- 1.2.2 Quietways are a network of bike routes for less confident cyclists using residential streets with low traffic volumes. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.
- 1.2.3 The measures proposed in this consultation are part of the Council's ongoing commitment to make Southwark's streets safer and more accessible for all. The proposed measures will enhance the environment for all road users, reducing traffic speeds and improving pedestrian safety. Cycling proposals also have the added health benefits of improving the environment through reducing carbon emissions and getting more people onto bikes which in turn enhances their fitness and health.
- 1.2.4 The following measures were consulted upon to improve cycling and pedestrian accessibility and enhance the streetscape for Site I:
- Pilgrimage Street carriageway to be resurfaced between Tabard Street and Great Dover Street and existing speed cushions to be converted to sinusoidal humps to improve the ride quality for cyclists.
 - New proposed raised carriageway table at the junction of Pilgrimage Street and Tabard Street to reduce vehicle speeds and improve pedestrian accessibility. Table to also incorporate footway buildouts with new pedestrian crossing locations.
 - The existing one way section of Tabard Street to be extended to Becket Street. Eastbound contra-flow cycle lane to be extended to Becket Street and will be segregated from the eastbound carriageway.

- Existing parking bays on the northern side of Tabard Street between Becket Street and Pilgrimage Street to be removed to allow the extension of the contra-flow cycle lane.
- New Parking bays to be provided on the southern side of Tabard Street to the east of Becket Street.
- New proposed raised carriageway table at the junction of Becket Street and Tabard Street to reduce vehicle speeds, improve pedestrian accessibility and enhance the streetscape. The table will include new pedestrian crossing locations and planting areas.
- Existing raised carriageway table adjacent to pedestrian entrance into Tabard Gardens to be modified to accommodate footway buildouts to reduce crossing distance for pedestrians and provide adequate visibility to the crossing location past the proposed parking bays on the southern side of Tabard Street.
- Carriageway of Tabard Street between park entrance and Laws Street to be resurfaced.
- New parking bay proposed on Tabard Street opposite Dorking House to increase parking availability and narrow carriageway width to reduce vehicle speeds.
- Existing sets of speed cushions on Tabard Street to be converted to cycle friendly sinusoidal humps.
- The existing raised carriageway table at the junction of Law Street and Tabard Street is to be rebuilt using better quality materials to improve the structure of the table and streetscape.

(See Appendix A – Initial Scheme Design)

1.3 Consultation Procedure

- 1.3.1 The views of the local community and those of statutory consultees have been sought, prior to the development of measures to a detailed design stage. Active community participation was encouraged through the use of a consultation document and questionnaire (see Appendix B – Consultation Documents).
- 1.3.2 The consultation document included a covering letter describing the proposals and a request for comments (including information to assist in translation and large print versions of the consultation document), preliminary design drawings (A3 size) and a questionnaire/comment form that could be sent to the Public Realm Projects Group with a pre-paid address reply.
- 1.3.3 The consultation document was delivered to a geographical area centred on the Tabard Street between Laws Street and Pilgrimage Street, using strategic roads and pedestrian desire lines as defined cut off points (See Appendix C – Location Plan and Extents of Consultation).

- 1.3.4 The distribution area was large enough to gain views from the wider community that may be considered to be affected by the proposed measures. A mailing list was established for the area by way of the Council's GIS database. In addition, the consultation documents and plans were supplied to the Council's established list of statutory consultees including London Buses, cycle groups and the Metropolitan Police. Please see Appendix D of list of addresses within the distribution area. Consultation documents were also delivered to the Southwark Housing Department and Grange Primary School.
- 1.3.5 The consultation documents were delivered by Royal Mail to 511 addresses detailed within the distribution list. The documents were delivered on the 5th August 2014, with a return deadline of the 5th September, allowing 4 weeks for the consultation period. However due to the summer holiday period, responses were accepted online until the 12th September 2014.
- 1.3.6 The proposals were also available to view online using consultation section of the council's website, with an e-form questionnaire provided in order to capture responses.

2.0 Consultation Responses

2.1 Response Rate and Distribution

- 2.1.1 A total of 22 responses were received during the consultation period (12 returned questionnaires and 10 online responses), equating to a 4.3% response rate.
- 2.1.2 Four responses were received from Statutory Consultees (Southwark Living Streets, Southwark Cyclists, Lambeth Cyclists and Sustrans).

2.2 Questionnaire Analysis

- 2.2.1 The questionnaire element of the consultation document contained the following key questions and associated tick box options:

Q1. Are you a resident or business?

Q2. Do you support the proposals?

- 2.2.2 The following is a summary of replies received:

Question 1 - Are you a resident or business?

	Resident	Business
Replies	22	0
Total	100%	0%

Table 1: Returned questionnaire results for question 1

2.2.3 Table 1 indicates that all responses received throughout the consultation period were from local residents.

Question 2 – Do you support the proposals?

	Support	Opposed	No Opinion
Replies	13	7	2
Total	59%	32%	9%

Table 2: Returned questionnaire results for question 2

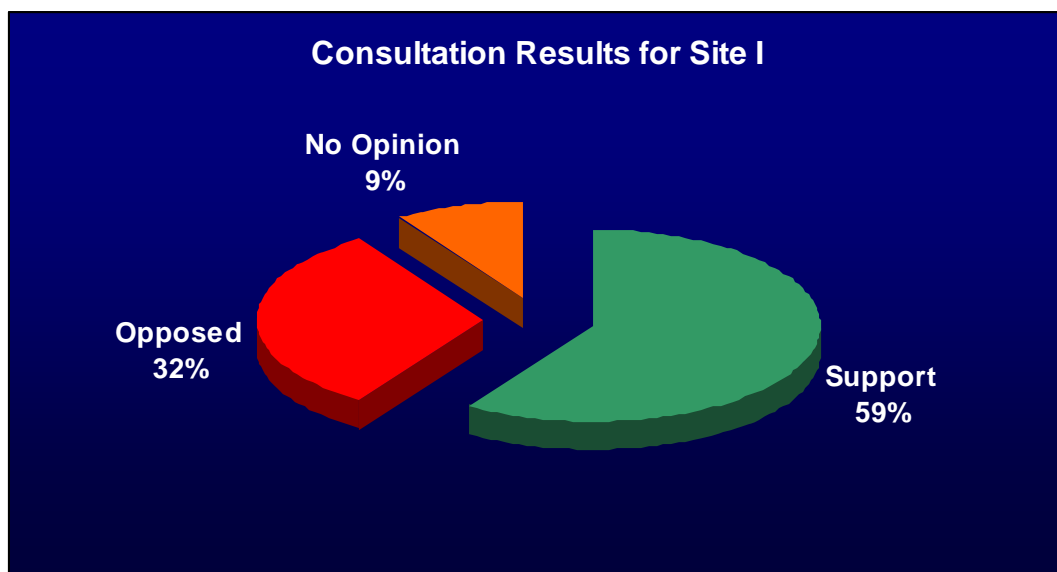


Figure 3: Graphical representation of consultation data for question 2

2.2.4 Table 2 and figure 2 indicate a majority of support for question 2, with 59% welcoming the proposed measures.

2.3 Additional Comments

2.3.1 The questionnaire element of the consultation document invited consultees to attach any additional comments they may have on the proposals when returning the reply-paid questionnaire. Consultees were also able to reply to the consultation online using the council's website.

2.3.2 The majority of respondents (59%) indicated support for the proposed measures, and discussed that they will make a great difference to the area and are a good improvement for cyclists.

2.3.3 A respondent commented that the cycle lane segregation in Tabard Street is welcome and will be a lot safer than the existing arrangement.

- 2.3.4 A comment was received that the scheme looks nice and requested that no further one way streets or closures to streets in the area take place other than what is proposed, as it is already difficult for residents to access their homes.
- 2.3.5 A respondent indicated that the proposals were good, especially ending the current farcical arrangement east of Pilgrimage Street junction which forces eastbound riders out into conflict with westbound traffic.
- 2.3.6 A comment was received stating that more attention should be given to the bend in the carriageway in Law Street, as there is a visibility issue and left hook risk. *
- * In response, there are adequate forward sightlines when traversing north towards the bend in the carriageway and an existing single yellow line parking restrictions keeps the running lane clear during peak cycle flow periods. It is noted that when traveling in the opposite direction towards the bend, parked vehicles at the end of the bay may potentially obstruct sightlines. As a result, subject to statutory consultation, it is proposed that the 5m rear section of the parking bay before the bend is removed and double yellow line parking controls installed that will extend around the bend to the start of the next parking bay.
- 2.3.7 A number of respondents commented that Tabard Street should be given priority at Pilgrimage Street. *
- * In response, this suggestion will be looked at as part of the scheme detailed design process and will be safety audited to ensure there are no adverse impacts on road safety. If not major issues arise, then a change of priorities will be considered, which would potentially benefit cyclists using the junction.
- 2.3.8 A suggestion was made to remove parking along the southern side of Tabard Street between Laws Street and Becket Street, which would the limit the risk to cyclists traversing westbound. *
- * In response, proposed sections of parking bays have been positioned along the southern kerb line of Tabard Street to offset the loss of parking along the northern side of Tabard Street between Becket Street and Pilgrimage Street associated with the contra-flow cycle lane. The bays have been positioned so that no sightline difficulties occur on approach to junctions or existing pedestrian crossing points. The carriageway of Tabard Street is also linear and therefore the introduction of the bays should not be problematic to cyclists traversing this section of the route. As will all changes to the highway layout, the parking bay arrangement and revised carriageway layout will be fully safety audited prior to implementation.
- 2.3.9 A respondent asked if full segregation is really necessary for the contra-flow cycle lane on Tabard Street. *
- * In response, the segregated contraflow cycle lane will greatly improve safety of cyclists traversing the route eastbound between Pilgrimage Street and Becket Street. Currently cyclists have to share a 3.3m traffic lane against opposing traffic. Removing the parking along the northern side of Tabard Street at this location will allow for a 1.5m wide segregated cycle lane to be installed that totally removes the head on conflict risk along this section of the route, which in turn will make the route more attractive to all cyclists regardless of ability.

2.3.10 Analysis of the additional comments from respondents that objected to the scheme highlighted the following concerns:

An objection was received highlighting that there is too much investment in a cycle route that does not lead anywhere. *

* In response, the proposed measures along the Quietway route align with the council's emerging cycling strategy and the Mayor's Vision for Cycling. The Mayor has commitment to invest total of £913m over the next 10 years in cycling safety and infrastructure development to significantly increase the modal share in cycling as a safe, healthy and sustainable form of transport in London.

The London boroughs will play a central role in delivering this vision, by helping to develop, fund and deliver better and safer routes for cycling.

The council welcomes significant investment from Transport for London to take forward the Quietway programme that will not only significantly upgrade a number of existing cycle routes, but also make considerable improvements to the streetscape for the benefits of all road users and local residents.

All measures will have significant benefit to not only cyclists, but also local residents and pedestrians. The proposals will significantly upgrade the street environment at this location.

The east to west routing of the Quietway scheme provides a direct path across the borough from South Bermondsey Station to commuter destinations in the west, including Waterloo. The route traverses parallel to a major arterial route into London (Old Kent Road) and provides a safe, convenient alternative to more heavily trafficked roads where the risk of collisions with other road users is greater. The routing has been made as a direct as possible, traversing both public highway and housing land to allow cyclists access to areas that are not open to motorised forms of transport. The route is already popular with cyclists and many roads from part of the previously established London Cycle Network.

An objection was received from a respondent asking why the parking on the north side needs to change. *

* In response, the parking on the northern side of Tabard Street between Becket Street and Pilgrimage Street has been removed to allow for an eastbound contra-flow cycle lane to be installed. Currently cyclists wishing to travel eastbound along this section of Tabard Street have to share the existing carriageway with opposing traffic flows which increases the risk of head-on conflict and is exacerbated by the narrow carriageway width between two rows of parking bays.

Removing the bays on the northern side of the road increases the available carriageway width by 1.8m, which allows for the introduction of a 1.5m contra-flow cycle lane and traffic island to provide segregation. This proposed arrangement still allows for a 3.2m general eastbound traffic lane and retention of existing parking bays on the southern side of Tabard Street.

The contra-flow proposal will make this section of Tabard Street safer for cyclists and more attractive to less confident cyclists who may be put off from using the current route due to inadequate protection from oncoming traffic.

A respondent objected to making the northern section of Pilgrimage Street no entry from Tabard Street. *

* In response, there is no proposal to implement a northbound no entry prohibition for vehicles on Pilgrimage Street at its junction with Tabard Street. Vehicles will still be able to enter Pilgrimage Street from both arms of Tabard Street and traverse northbound towards Long Lane.

A number of respondents objected stating that segregated cycle lanes are not needed on a Quietway Route, the solution is over engineered and will reduce future capacity for cycling the route in the future.' *

* In response, the existing road layout of Tabard Street between Pilgrimage Street and Becket Street is considered poor for cyclists. As described above, cyclists wishing to travel eastbound along this section of Tabard Street have to do so with the threat of head-on conflict from opposing vehicle movement, which is made worse by narrow carriageway width between two sets of kerbside parking bays.

In order to make the route safer and attractive to use by cyclists of all abilities, a contra-flow cycle lane has been proposed to provide a designated section of carriageway that totally removes the risk of head-on conflict. Whilst segregation is not a standard feature of a Quietway network, it is the best solution for this location. Implementing a mandatory contra-flow cycle lane with just signage, road markings and kerbside waiting controls, whilst in theory would provide a facility for cyclists to travel eastbound, it is likely that vehicles may still park indiscriminately in the lane (as kerbside parking demand is high along this section of Tabard Street), leading to obstruction and forcing cyclists into the opposing carriageway.

Proving segregation will ensure that the cycle lane will be free from obstruction at all times. In addition, if segregation was not installed, then the carriageway width of the westbound lane would be around 4m, which could lead to higher vehicle speeds and potentially result in safety issues for all road users. On this basis, it is the view of the council that this element of the scheme has not been over engineered and careful consideration has been given to both the safety and movement of cyclists at this location.

In addition to the contra-flow cycling facilities, the other measures have a clear benefit to local residents through improvements to pedestrian accessibility, traffic speed reduction and a visually enhanced streetscape, which in turn will add to the attractiveness of the route to cyclists.

The proposed contra-flow cycle lane will have a minimum width of 1.5m, which complies with current design standards and will ensure that there is no adverse impact on existing or future network capacity.

A number of objections highlighted that Tabard Street and Law Street still remain rat runs for non-local traffic. Both roads should be 'access only' and blocked off to through traffic. *

* In response, Law Street and Tabard Street carry low volumes of traffic compared to neighbouring roads and is therefore are the most appropriate route for the Quietway. Whilst closing both roads to through traffic would improve the

Quietway route further, a comprehensive feasibility study would be required to ascertain the likelihood of traffic displacement onto other residential streets in the area. Due to the controversial nature of preventing through traffic and the potential access changes to residential and commercial properties, a separate consultation specifically on this proposal would be required. This proposal can certainly be considered in the future and feasibility could potentially be explored as part of the post implementation monitoring of the Quietway scheme.

A number of objections mentioned the introduction of additional parking bays on the south side of Tabard Street will force cyclists to ride in the 'door zone' in both directions and are not needed. *

* In response, as discussed above, the parking bays on the south side of Tabard Street to the east of Becket Street have been proposed to offset the removal of the existing parking bay on the northern side of Tabard Street where the contra-flow cycle lane is proposed. There is a high demand for on-street parking in the area and to reduce kerbside parking availability for local residents would result in strong opposition to the scheme.

The size and position of the proposed parking bays has been carefully considered so that they do not compromise safety or sightlines on approach to junctions and pedestrian crossing locations. Passing places for general traffic have also been created to ensure that there is minimal chance of head-on conflict. It is noted that the current general carriageway width of Tabard Street is over 6m and linear in length from Law Street to Pilgrimage Street, which is conducive to high vehicle speeds. The parking bays will therefore potentially act as a traffic calming feature and which will assist with self enforcing the 20mph speed limit.

As traffic volumes on Tabard Street are low, the risk to cyclists traversing the eastern carriageway with parking bays enter side is minimal. Cyclists will be able to take the centre line of the carriageway away from the 'door zone'. If on occasion a vehicle is approaching in the opposite direction, the carriageway provided between the bays will be wide enough for a vehicle to pass a cyclist without head-on conflict. All aspects of the scheme will be assessed as part of a road safety audit to ensure that there are no adverse road safety issues resulting from the proposed changes in road layout.

2.3.9 17% respondents did not submit a further comment.

2.4 Levels of Consensus

2.4.1 The following majority levels of agreement have been given in relation to the questions contained within the consultation document:

- 59% of respondents support the introduction of the Quietway cycle route proposals for Site I;
- 32% of respondents are opposed to the proposals; and
- 9% of respondents had no opinion.

2.5 Statutory Consultee Replies

2.5.1 Four statutory consultees provided a reply to the consultation.

- a) **Southwark Living Streets** replied indicating strong support for the scheme and stated that the new / extended cycle contra-flow lane was a good idea and will improve safety for cyclists.
- b) **Lambeth Cyclists** replied in support of the scheme and stated that the four sets of bike logos approaching Pilgrimage Street should be in the centre of the lane and not the door zone. *

* In response, the consultation plans are just indicative and as part of the detailed design process, all cycle logos proposed adjacent to junctions and general carriageway will be suitably positioned in the carriageway so that cycles do not traverse along the nearside kerb on approach to side road junctions and will be positioned in the centre of the running lane.

- c) **Southwark Cyclists** replied indicating support for the scheme. They highlighted that Tabard Street is narrow and allowing daytime parking on both sides may make it difficult for motorists to pass cyclists and question the need for more parking to be introduced on the south side of the road. A suggestion was made to ban daytime parking on the north side so the contra-flow cycle lane can continue up to Laws Street. *

* In response, as discussed previously, additional parking bays have been proposed along the southern side of Tabard Street to the east of Becket Street to offset the loss of existing kerbside parking facilities associated with the introduction of the contra-flow cycle lane. Whilst there are some offset parking facilities in the adjacent estate, there is still a high demand for on-street parking facilities from local residents, which has been highlighted in many of the responses received as part of this consultation. Therefore omitting the proposed bays or removing the existing bays along the northern side of the carriageway would potentially be met with vociferous objections.

The proposed bays are shorter in length than the ones on the northern side of the road in order to provide appropriate sightlines to both pedestrian crossing facilities and junctions and in order to create passing spaces for vehicles. It must also be noted that the traffic volume in Tabard Street is minimal and with the proposed upgrades to existing traffic calming measures and introduction of new vertical deflection at junctions traffic speeds will be curtailed and assist with compliance with the 20mph speed limit, which will directly benefit cyclists. The carriageway width provided between the bays will also allow for vehicles to overtake cyclists without conflict.

All aspects of the scheme will be assessed as part of a road safety audit to ensure that there are no adverse road safety issues resulting from the proposed changes in road layout.

- d) **Sustrans** replied indicating support for the scheme but did not provide any specific questions or comments on the proposed measures.

2.5.2 No objections were received from Ward Members throughout the consultation period.

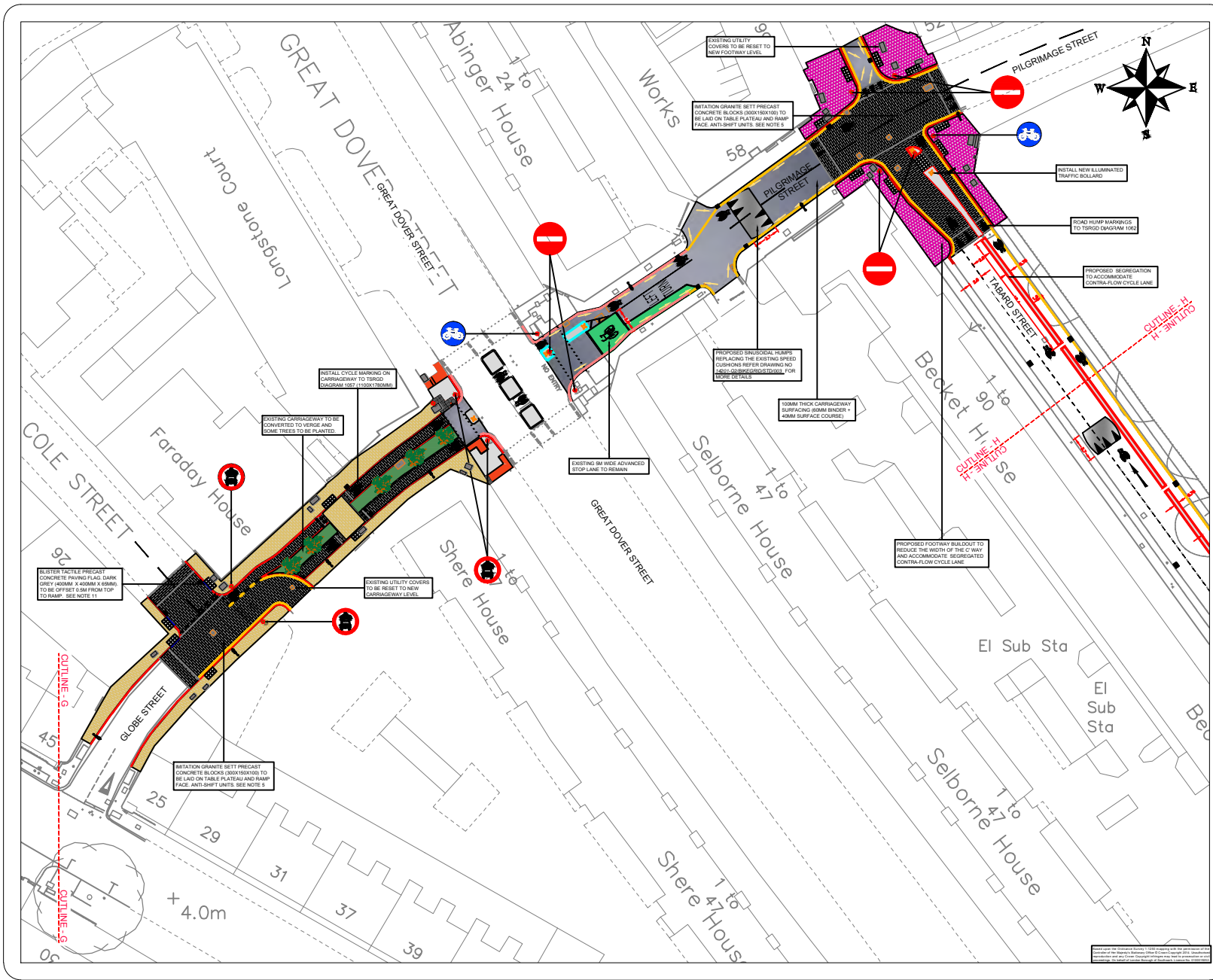
3.0 Recommendations

Due to the majority of respondents supporting the scheme and Southwark's on-going commitment to improve and promote cycling in the borough, it is recommended that the scheme proceed to implementation.

Appendices

Appendix A:	Initial Scheme Design
Appendix B:	Consultation Documents
Appendix C:	Location Plan and Extents of Consultation
Appendix D:	List of Addresses within the Distribution Area

Appendix A: Initial Scheme Design



LEGEND / NOTES.

KEY:

- CARRIAGEWAY RECONSTRUCTION:** 40mm THICK T265 210 REV 662 T265 OR 20mm THICK T265 T265 TABLE CONSTRUCTION LAY ON 200mm CARRIAGEWAY REPAIR CONSTRUCTION.
- FOOTWAY RECONSTRUCTION:** PROVIDE AND LAY PCC PAVING FLAG WITH 100mm THICK T265 T265 TABLE CONSTRUCTION LAY ON 200mm THICK TYPE 1 SUBBASE. 20mm THICK LOW FINES BEDDING SAND.
- RAISED FEATURES:** PROVIDE AND LAY ANTI-SHIFT PCC BLOCKS 300 x 750 x 75mm THICK TABLE CONSTRUCTION LAY ON 200mm THICK CG2M CG2 AND 20mm THICK LOW FINES BEDDING SAND.
- BULBER PAVING:** PROVIDE AND LAY PCC BULBER TACTILE PAVERS CONSTRUCTION LAY ON 200mm THICK CG2M CG2 SUBBASE AND 20mm THICK 'TUFFREB' LAYING MORTAR BY STENEC.
- TUFFREB LAYING:** SECTION OF FOOTWAY TO BE LAID ON 200mm THICK CG2M CG2 SUBBASE AND 20mm THICK 'TUFFREB' LAYING MORTAR BY STENEC.
- CYCLE LANE:** PROPOSED CYCLE LANE / ADVANCED STOP LINE RESTRICTION FOR INDICATIVE PURPOSE ONLY.
- EXISTING CYCLE SUPER HIGHWAY BY TRANSPORT FOR LONDON.**
- ROADWORK:** EXISTING ROADWORK COVER AND FRAME TO BE ADJUSTED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INITIAL FLAT KERBS 150 x 300mm UNITS IN NEW GREY.
- STANDARD GRANITE KERBS:** METAL NEW STANDARD GRANITE KERBS (150mm x 300mm) LAID WITH 100mm UPST AND ON S14 INCLUDING FRONT KERBS FINISHED TO BE 100mm BELOW CARRIAGEWAY. REFER TO DRAWING LBS-C010 KERBS AND EDGE RESTRAINT FOUNDATION DETAILS BY SHEET 1 OF 4 & TYPE B - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE KERBS (WITH NEW STANDARD GRANITE KERBS (150mm x 300mm) LAID ON S14 CARRIAGEWAY):** REFER TO DRAWING LBS-C010 KERBS AND EDGE RESTRAINT FOUNDATION DETAILS BY SHEET 1 OF 4 & TYPE B - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE TRANSITION KERBS:** NEW STRAIGHT KERBS TYPE T1 (SHEET 3 OF 4) REFER TO DRAWING LBS-C010 KERBS AND EDGE RESTRAINT FOUNDATION DETAILS BY SHEET 1 OF 4 & TYPE B - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- NEW WHITE LINE ROAD MARKING:**
- NEW YELLOW LINE ROAD MARKING:**
- REB ROAD MARKINGS TO BE REINSTALLED:**
- NEW GRASS:** PROVIDE AND METAL SURFACE WATER DRAIN WITH 100mm DEPTH INTERNAL DRAINER DEPTH OF 100mm ON BED TYPE Z. AVERAGE DEPTH OF 100mm ON BED TYPE Z.
- NEW GULLY:** PROVIDE AND METAL PRECAST CONCRETE TRAPPED GULLY WITH TYPE D COVER. GULLY DEPTH 100mm OR BUILD CATCH PIT WHERE SPECIFIED.
- EXISTING ROAD GULLY TO BE CONVERTED TO MANHOLE:**
- EXISTING ROAD GULLY TO BE PASSED AND ADJUSTED:**
- NEW CYCLE SEPARATOR OR ARMS/ISLES:**
- NEW TRAFFIC SIGNAL POST LOCATION (PRIMARY/SECONDARY):**
- NEW LAMP COLUMN LOCATION:**
- EXISTING LAMP COLUMN LOCATION:**
- NEW SHELFED CYCLE STANDS:**
- NEW TREE PIT CONSTRUCTION:** REFER TO STANDARD DRAWING DETAILS NO. 1001 - CONSTRUCTION.
- EXISTING / PROPOSED LONGITUDINAL FALL ON CARRIAGEWAY:**
- EXISTING BELL BOLLARD TO BE RELOCATED AS SHOWN IN THE DRAWING.**
- NEW VORSTONE PAVING:** PROVIDE AND LAY NEW VORSTONE PAVING SLABS (800mm x 800mm x 60mm) FOR FOOTWAY CONSTRUCTION LAY ON 200mm THICK LOW FINES BEDDING SAND AND 100mm THICK TYPE 1 SUBBASE.
- PROPOSED LOCATION FOR THE SIGN POSTS:**

Author	Date	Amendment	Drawn	Design	Checked	Approved

PUBLIC REALM PROJECTS TEAM 160 TOOLEY STREET SE1P 5LX

Project
CENTRAL LONDON BIKE GRID
Q2 - BLUE ROUTE

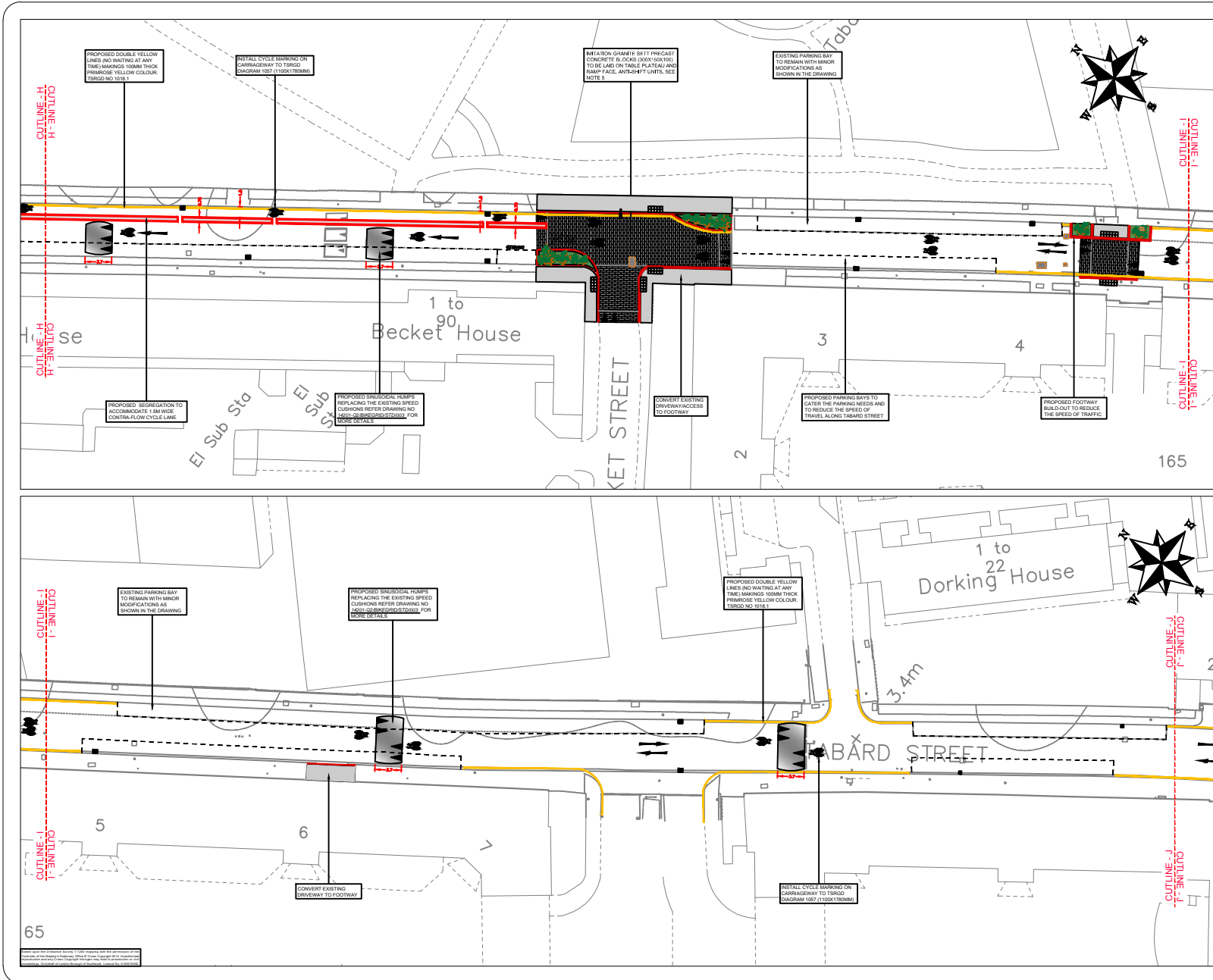
Title
GENERAL ARRANGEMENT
SECTION-8
GLOBE STREET / PILGRIMAGE WAY

Contract No: HW2013 NEC3 **Drawn:** [initials] **OK**

Scale: 1:250 @ A1 **Purpose:** CONSTRUCTION **Checked:** [initials] **OK**

Drawing No: 14201-Q2/BIKEGRID/GA/008 **Approved:** [initials] **OK**

Date Issued: JUNE 2014 **Date Issued:** JULY 2014



LEGEND / NOTES.

- KEY:**
- CARRIAGEWAY RECONSTRUCTION:** 40mm THICK TRCS ST10 P50 68+ LAD ON 100mm THICK BEDDING SAND WITH 1:10 CURB OR CARRIAGEWAY REPAIR CONSTRUCTION.
 - FOOTWAY RECONSTRUCTION:** PROVIDE AND LAY PCC PAVING FLAG LAD ON 200mm THICK TYPE 1 SUBBASE. 30mm THICK LOW FINES BEDDING SAND.
 - PAVED FEATURES:** PROVIDE AND LAY ANTI-SHIFT PCC BLOCKS 300 x 100 x 50mm FOR HIGH TABLE CONSTRUCTION. LAD ON 300mm THICK CG3M C20 AND 30mm THICK LOW FINES BEDDING SAND.
 - BLESTER PAVING:** PROVIDE AND LAY PCC BLESTER TACTILE PAVERS 200mm x 200mm THICK FREE FIBRETEXT ON SPECIFIED FOOTWAY CONSTRUCTION. LAD ON 200mm THICK CG3M C20 SUBBASE AND 30mm THICK TUFFBEST LAYING MORTAR BY STANTEC.
 - TUFFBEST LAYING:** SECTION OF FOOTWAY TO BE LAD ON 200mm THICK CG3M C20 SUBBASE AND 30mm THICK TUFFBEST LAYING MORTAR BY STANTEC.
 - CYCLE LANE:** PROPOSED CYCLE LANE / ADVANCED STOP LINE CONSTRUCTION FOR INDICATIVE PURPOSE ONLY.
 - EXISTING CYCLE SUPER HIGHWAY BY TRANSPORT FOR LONDON**
 - BROWWORK:** EXISTING BROWWORK COVER AND FRAME TO BE ADJUSTED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INSTALL FLAT KERB 150 x 300MM UNIT SILVER GREY.
 - STANDARD GRANITE KERBS:** INSTALL NEW STANDARD GRANITE KERB (1000 x 1000) WITH 100MM UPSTAND ON 214 INCLUDING FRONT KERB FINISHING TO BE 100MM BELOW CARRIAGEWAY. REFER TO DRAWING LBS/C20 KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE B/SHEET 1 OF 4 & TYPE E - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
 - STANDARD GRANITE KERBS BUSHES:** INSTALL NEW STANDARD GRANITE KERBS (1000 x 300MM) LISHED ON 214 INCLUDING FRONT KERB FINISHING TO BE 100MM BELOW CARRIAGEWAY. REFER TO DRAWING LBS/C20 KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE B/SHEET 1 OF 4 & TYPE E - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS. NEW PRIMARY TRAFFIC SIGNAL POST LOCATION.
 - STANDARD GRANITE TRANSITION KERBS:** NEW STRAIGHT KERB TYPE T/K2 (TYPE LBS/C20). TRANSITION KERB TYPE B/SHEET T11. REFER TO DRAWING LBS/C20 KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE B/SHEET 1 OF 4 & TYPE E - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
 - NEW WHITE LINE ROAD MARKING:**
 - RED ROAD MARKINGS TO BE REINSTALLED:**
 - NEW DRAIN:** PROVIDE AND INSTALL SURFACE WATER DRAIN WITH 100MM INTERNAL DIAMETER. DEPTH OF INVERT +0.0. FINISHING DEPTH OF 750MM ON BED TYPE 2. DEPTH OF 750MM ON BED TYPE 2.
 - NEW GULLY:** PROVIDE AND INSTALL PRECAST CONCRETE TRAPPED GULLY WITH TYPE 2 COVER. GULLY DEPTH +0.050m OR ROAD CATCH PIT WHERE SPECIFIED.
 - EXISTING ROAD GULLY TO BE CONVERTED TO MANHOLE:**
 - EXISTING ROAD GULLY TO BE REMOVED AND ADJUSTED:**
 - NEW CYCLE SEPARATOR OR ANTI-COLLISION:**
 - NEW TRAFFIC SIGNAL POST LOCATION (PRIMARY/SECONDARY):**
 - NEW LAMP COLUMN LOCATION:**
 - EXISTING LAMP COLUMN LOCATION:**
 - NEW SHEFFIELD CYCLE STANDS:**
 - NEW TREE PIT CONSTRUCTION (REFER TO STANDARD DRAWING DETAILS AND SIGN CONSTRUCTION 2020):**
 - EXISTING / PROPOSED LONGITUDINAL FALL ON CARRIAGEWAY:**
 - EXISTING BELL BOLLARD TO BE RELOCATED AS SHOWN IN THE DRAWING:**
 - NEW CONCRETE SLAB:** PROVIDE AND LAY NEW VORSTONE PAVING SLAB (800MM x 800MM x 63MM) FOR FOOTWAY CONSTRUCTION. LAD ON 200mm THICK LOW FINES BEDDING SAND AND 100MM THICK TYPE 1 SUBBASE.
 - PROPOSED LOCATION FOR THE SIGN POSTS:**

Function	Date	Assessment	Drawn	Design	Checked	Approved

PUBLIC REALM PROJECTS TEAM 160 TOOLEY STREET SE1P 5LX

Project		CENTRAL LONDON BIKE GRID Q2 - BLUE ROUTE			
Title		GENERAL ARRANGEMENT SECTION-9 TABARD STREET			
Contract No.	HW2013 NEC3	Drawn			
Scale	1 : 250 @ A1	Purpose	CONSTRUCTION	Checked	
Drawing No.	14201-Q2/BIKEGRID/GA/009	Approved			
Date Issued	JUNE 2014	Date Issued	JULY 2014		

Appendix B: Consultation Documents

- New parking bay proposed on Tabard Street opposite Dorking House to increase parking availability and narrow carriageway width to reduce vehicle speeds.
- Existing sets of speed cushions on Tabard Street to be converted to cycle friendly sinusoidal humps.
- The existing raised carriageway table at the junction of Law Street and Tabard Street is to be rebuilt using better quality materials to improve the structure of the table and streetscape.



Site I – Tabard Street

Cycling Accessibility and Streetscape Improvements

We want your views

It is important for all consultees to respond to the consultation. We would be grateful if you could take the time to review the proposals outlined in this document and provide a response using the pre-paid envelope and questionnaire provided by **Friday 5th September 2014**

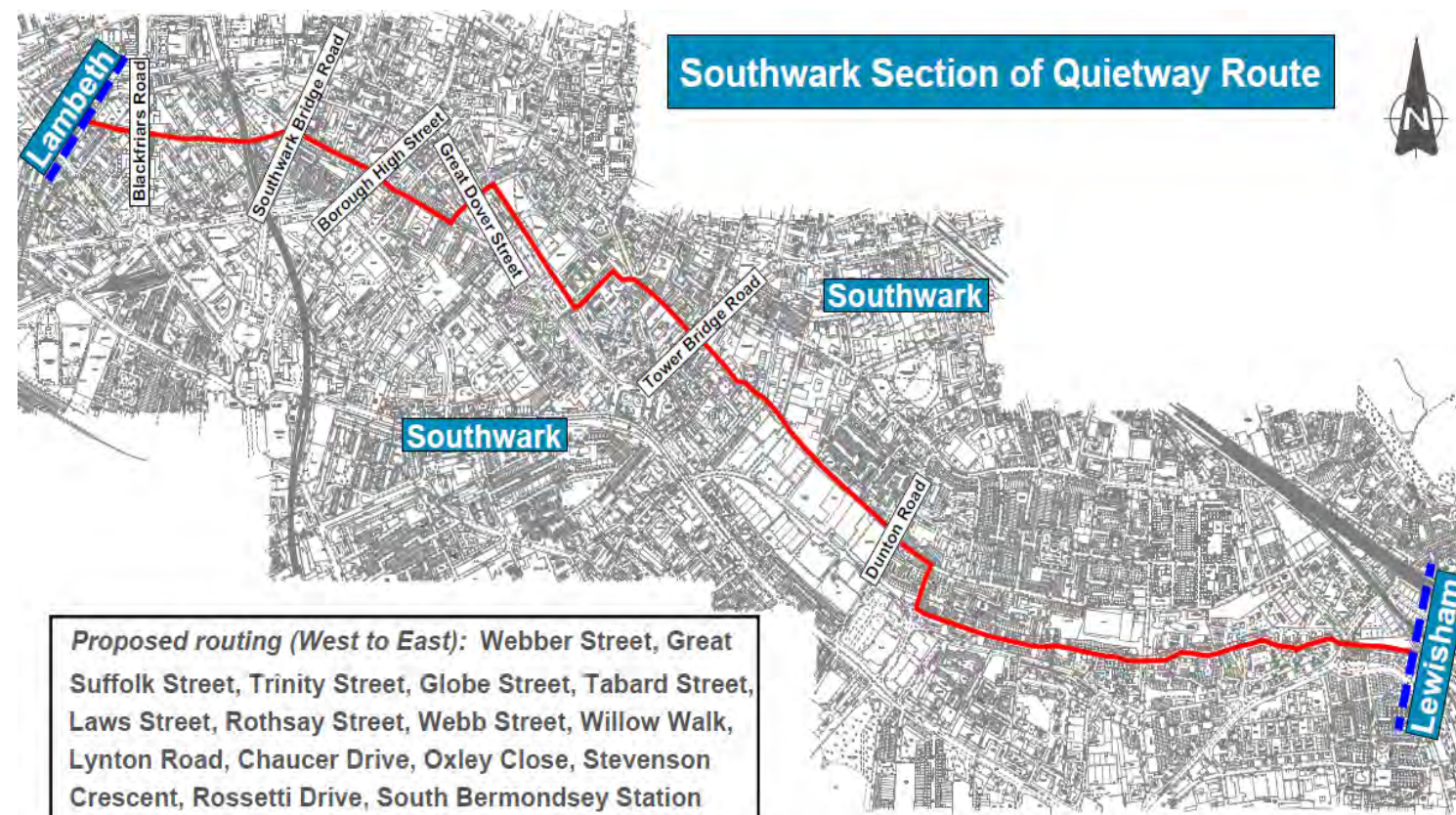
Alternatively, you can view the proposals at www.southwark.gov.uk/consultations and complete the online questionnaire.

What happens next?

The responses to the questionnaire will be analysed and taken into account in the final design of the proposed works. As you will appreciate Southwark Council receives many comments from consultations and therefore are unable to respond personally to specific issues raised. However all comments and suggestions will be taken into consideration before a decision is made. The consultation results and recommendations will be presented at a local community council meeting in September 2014.

Should you require any further information regarding the proposed scheme please do not hesitate to contact Chris Mascord at chris.mascord@southwark.gov.uk

Further information on other schemes along the route in Southwark can also be found at: www.southwark.gov.uk/consultations



Have your say

Southwark Council is holding a consultation to receive residents' and key stakeholders' comments regarding proposals relating to the borough's section of the Quietway (Greenwich to Waterloo) cycle route. This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey station in the east and traversing local roads to The Cut in the west. The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

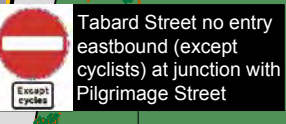
Background

Quietways are a network of bike routes for less confident cyclists using low-traffic back streets. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.

What are the proposed changes?

- Pilgrimage Street carriageway to be resurfaced between Tabard Street and Great Dover Street and existing speed cushions to be converted to sinusoidal humps to improve the ride quality for cyclists.
- New proposed raised carriageway table at the junction of Pilgrimage Street and Tabard Street to reduce vehicle speeds and improve pedestrian accessibility. Table to also incorporate footway buildouts with new pedestrian crossing locations.
- The existing one way section of Tabard Street to be extended to Becket Street. Eastbound contra-flow cycle lane to be extended to Becket Street and will be segregated from the eastbound carriageway.
- Existing parking bays on the northern side of Tabard Street between Becket Street and Pilgrimage Street to be removed to allow the extension of the contra-flow cycle lane.
- New Parking bays to be provided on the southern side of Tabard Street to the east of Becket Street.
- New proposed raised carriageway table at the junction of Becket Street and Tabard Street to reduce vehicle speeds, improve pedestrian accessibility and enhance the streetscape. The table will include new pedestrian crossing locations and planting areas.
- Existing raised carriageway table adjacent to pedestrian entrance into Tabard Gardens to be modified to accommodate footway buildouts to reduce crossing distance for pedestrians and provide adequate visibility to the crossing location past the proposed parking bays on the southern side of Tabard Street.
- Carriageway of Tabard Street between park entrance and Laws Street to be resurfaced.

SECTION A



Tabard Street no entry eastbound (except cyclists) at junction with Pilgrimage Street

Proposed raised block carriageway table at the junction of Tabard Street and Pilgrimage Street to reduce traffic speeds and improve pedestrian accessibility with two new pedestrian crossing points across Pilgrimage Street



Proposed segregated eastbound contraflow cycle lane to be implemented on Tabard Street from Pilgrimage Street to Becket Street

New raised carriageway table to include planting areas to improve the streetscape, with new pedestrian crossing point proposed on the western side of the junction

Redundant vehicle crossover to be removed and converted to footway

Footway to be built out adjacent to the park to improve visibility on approach to the carriageway table pedestrian crossing location. Footway buildout to incorporate tree planting to improve the streetscape



SECTION A

SECTION B

SECTION B

The carriageway in Pilgrimage Street between Tabard Street and Great Dover Street is to be resurfaced to improve the ride quality for cyclists. All sets of speed cushions to be converted to cycle friendly sinusoidal humps

Tabard Street to be made one way westbound for general traffic from the junction of Becket Street and Pilgrimage Street

Proposed raised block carriageway table at the junction of Tabard Street and Becket Street to reduce traffic speeds and improve pedestrian accessibility

Proposed permit holder parking bay on the southern side of Tabard Street

In conjunction with the footway buildout work, the surface to the existing raised carriageway table is to be renewed using better quality materials

SECTION B

GEOFFERY HOUSE

1 TO 22 DORKING HOUSE

The carriageway in Law Street is to be resurfaced to improve the ride quality for cyclists. All sets of speed cushions to be converted to cycle friendly sinusoidal humps

SECTION B

SIDNEY WEBB HOUSE

SIDNEY WEBB HOUSE (159 GREAT DOVER STREET)

Proposed permit holder parking bay on the southern side of Tabard Street

Existing sets of speed cushions on Tabard Street to be converted to cycle friendly sinusoidal road humps

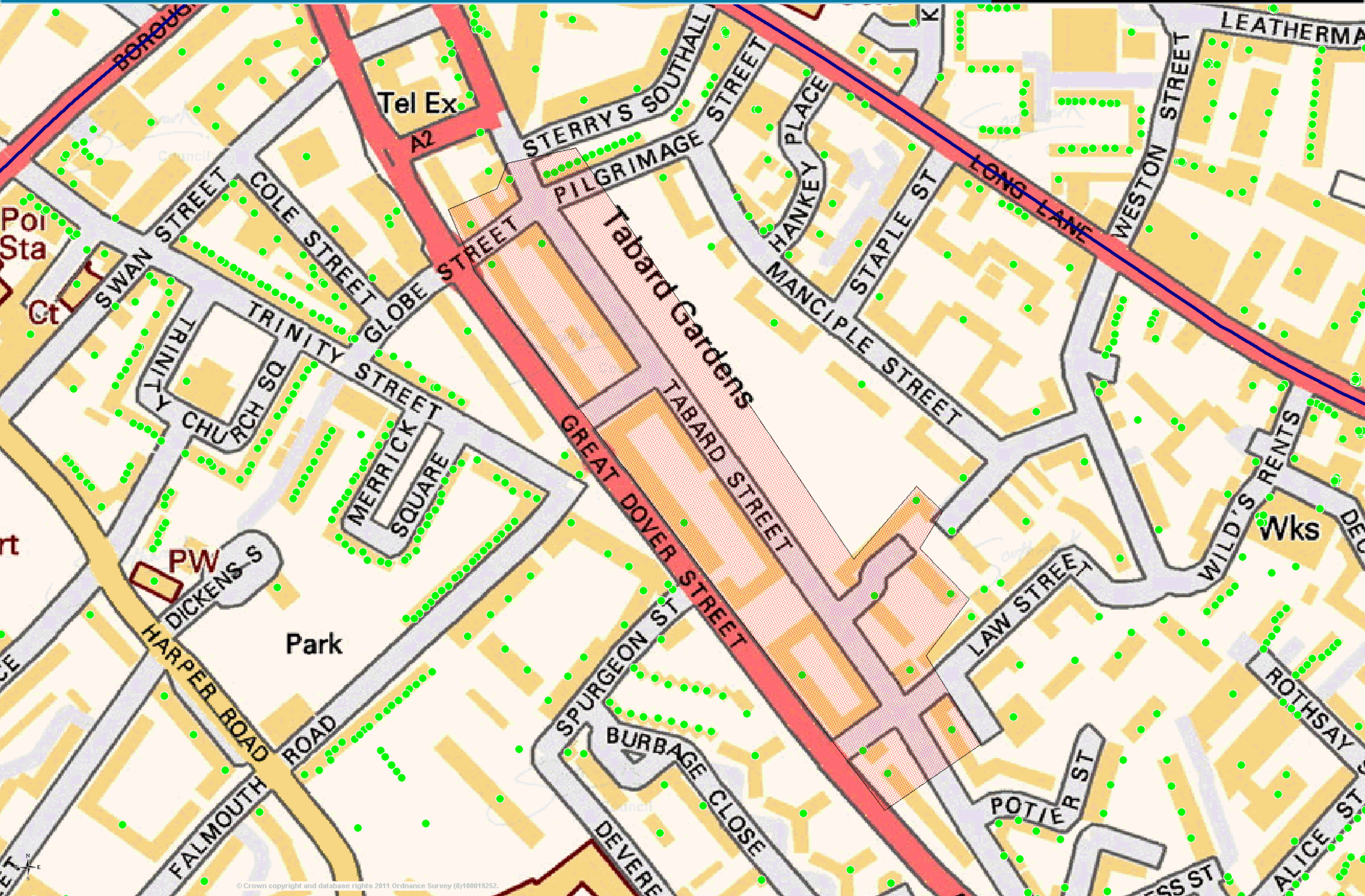
Proposed permit holder parking bay on the southern side of Tabard Street

Existing carriageway of Tabard Street between Law Street and Pardoner Street to be resurfaced

Existing raised carriageway table to be renewed to address subsidence and improve the visual quality of the streetscape

Existing footway to be built out to with proposed tree planting

Appendix C: Location Plan and Extents of Consultation



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Appendix D: List of Addresses within Distribution Area

ORGANISATION	SAO	PAO	STREET	TOWN	POSTCODE
	FLAT 102	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 14	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 85	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 31	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 36	GEOFFREY HOUSE	PARDONER STREET	LONDON	SE1 4DW
	FLAT 2	DORKING HOUSE	PARDONER STREET	LONDON	SE1 4DP
	FLAT 16	GEOFFREY HOUSE	PARDONER STREET	LONDON	SE1 4DW
	FLAT 51	GEOFFREY HOUSE	PARDONER STREET	LONDON	SE1 4DW
	FLAT 10	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 50	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
		44	PILGRIMAGE STREET	LONDON	SE1 4LL
		SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4GZ
		58	TABARD STREET	LONDON	SE1 4LG
COSTCUTTERS LONDON COLLEGE OF ACCOUNTANCY	FLAT 40	165	GREAT DOVER STREET	LONDON	SE1 4WY
	FLAT 8	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 59	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 107	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 111	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 115	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 96	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 100	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 105	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 22	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 26	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 30	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 35	165	GREAT DOVER STREET	LONDON	SE1 4WY
	FLAT 10	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 15	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 2	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 24	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 61	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 5	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 13	DORKING HOUSE	PARDONER STREET	LONDON	SE1 4DP
	FLAT 17	DORKING HOUSE	PARDONER STREET	LONDON	SE1 4DP
	FLAT 22	DORKING HOUSE	PARDONER STREET	LONDON	SE1 4DP
	FLAT 7	DORKING HOUSE	PARDONER STREET	LONDON	SE1 4DP
	FLAT 45	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 50	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 54	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 46	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 15	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 2	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 23	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 11	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 1	PARDONER HOUSE	PARDONER STREET	LONDON	SE1 4DR
	FLAT 3	PARDONER HOUSE	PARDONER STREET	LONDON	SE1 4DR
	FLAT 7	PARDONER HOUSE	PARDONER STREET	LONDON	SE1 4DR
	FLAT 32	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 36	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 40	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	LIVING ACCOMMO DATION	254	TABARD STREET	LONDON	SE1 4UN
	FLAT 28	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 19	165	GREAT DOVER STREET	LONDON	SE1 4WY
	FLAT 20	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 707	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 34	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 507	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 512	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 604	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 609	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 45	165	GREAT DOVER STREET	LONDON	SE1 4WY
	FLAT 49	165	GREAT DOVER STREET	LONDON	SE1 4WY
	FLAT 16	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 8	ABINGER HOUSE	GREAT DOVER STREET	LONDON	SE1 4XU
	FLAT 11	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 16	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 12	GEOFFREY HOUSE	PARDONER STREET	LONDON	SE1 4DW
	FLAT 18	GEOFFREY HOUSE	PARDONER STREET	LONDON	SE1 4DW
	FLAT 702	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 70	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 6	GEOFFREY HOUSE	PARDONER STREET	LONDON	SE1 4DW
	FLAT 54	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 59	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 63	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 68	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 303	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 70	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 77	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 81	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 25	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 6	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XY
	FLAT 79	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 89	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 203	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 207	SIDNEY WEBB HOUSE, 159	GREAT DOVER STREET	LONDON	SE1 4WW
	FLAT 39	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 44	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 48	BECKET HOUSE	TABARD STREET	LONDON	SE1 4XZ
	FLAT 2	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 24	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 3	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 34	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 39	SELBOURNE HOUSE	GREAT DOVER STREET	LONDON	SE1 4XX
	FLAT 87	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 92	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 54	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 63	165	GREAT DOVER STREET	LONDON	SE1 4WZ
	FLAT 12	PILGRIM HOUSE	TABARD STREET	LONDON	SE1 4UP
	FLAT 22	165	GREAT DOVER STREET	LONDON	SE1 4WY
	FLAT 26	165	GREAT DOVER STREET	LONDON	SE1 4WY

London Borough of Southwark



Quietway Cycling Proposals

Site J – Globe Street and Trinity Street

Public Consultation Summary

September 2014

London Borough of Southwark

Site J - Quietway Cycling Proposals Globe Street and Trinity Street

Public Consultation Summary

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1.0 Introduction

1.1 Background

1.1.1 This document report has been produced by the London Borough of Southwark Public Realm Projects Group, to provide a summary of the consultation exercise for the Quietway Cycling Proposals for Site J in Globe Street and Trinity Street. The measures are being drafted by the Public Realm Projects Team, with the project manager for this scheme being Chris Mascord (Senior Engineer).

1.1.2 The area under consideration is located within the SE1 district of Southwark (Chaucer Ward) in the north of the borough. See figure 1 below.

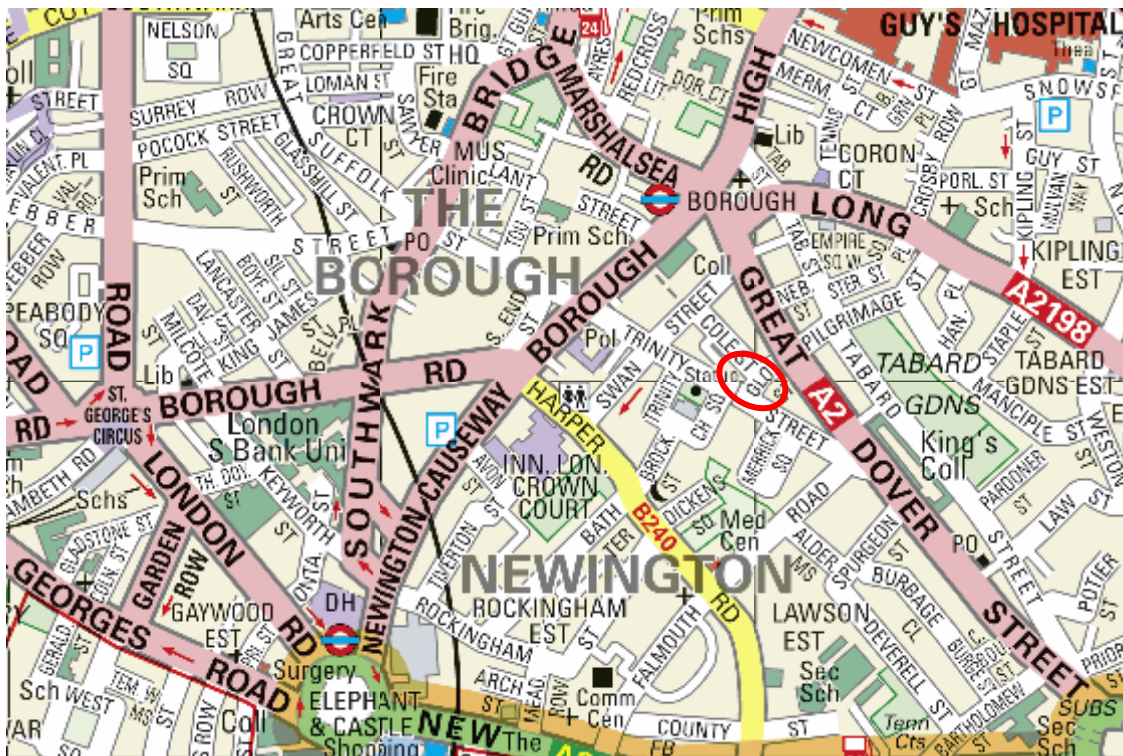


Figure 1: Location of proposed scheme

1.2 Project and Objectives

1.2.1 This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey Station in the east and traversing local roads to The Cut in the west (see figure 2). The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

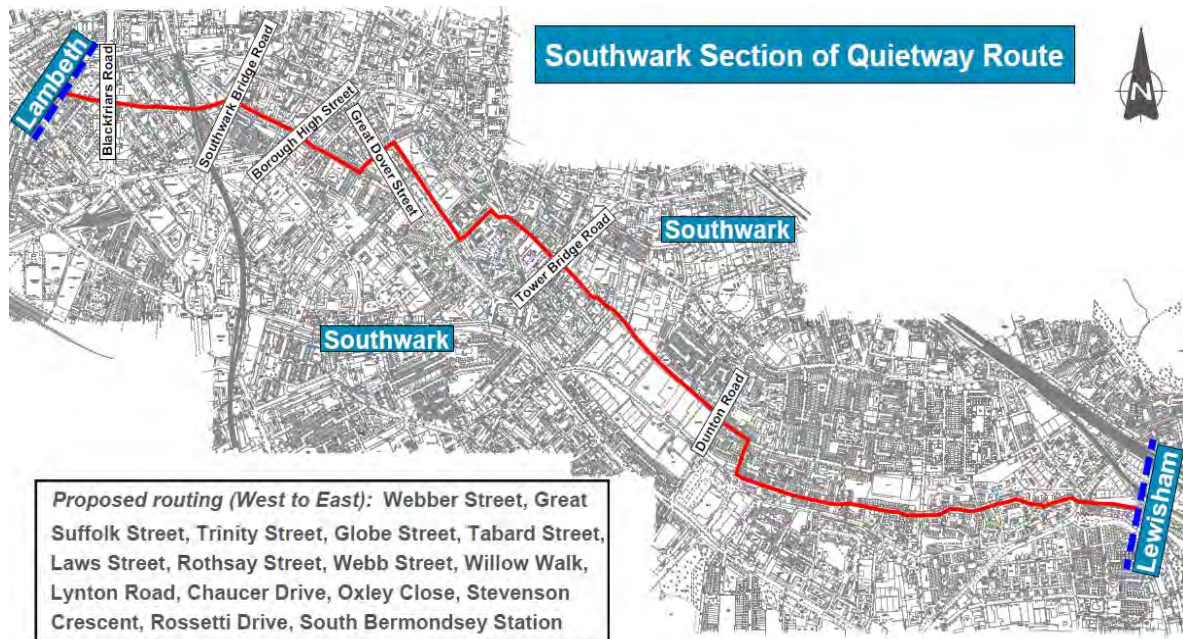


Figure 2: Southwark's section of Quietway Route

- 1.2.2 Quietways are a network of bike routes for less confident cyclists using residential streets with low traffic volumes. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.
- 1.2.3 The measures proposed in this consultation are part of the Council's ongoing commitment to make Southwark's streets safer and more accessible for all. The proposed measures will enhance the environment for all road users, reducing traffic speeds and improving pedestrian safety. Cycling proposals also have the added health benefits of improving the environment through reducing carbon emissions and getting more people onto bikes which in turn enhances their fitness and health.
- 1.2.4 The following measures were consulted upon to improve cycling ,pedestrian accessibility and enhance the streetscape for Site J:
- The section of Globe Street between Cole Street and Great Dover Street is to become a new public space for pedestrians and cyclists, with the carriageway removed and area paved to create new footways, tree planting and segregated cycle tracks.
 - No entry (except cyclists) on Globe Street north of Cole Street (exit only out of Shere House parking area).

- New raised carriageway table at the junction of Globe Street and Cole Street to reduce traffic speeds and improve pedestrian accessibility. Table surface to be block paving to improve the visual quality of the streetscape.
- Both footways in Globe Street to be paved on yorkstone paving units to improve the quality of the streetscape and to tie in with existing yorkstone paving at the Trinity Street junction.
- Existing footway bollards on the western side of Globe Street and northern side of Trinity Street to be removed to increase footway width and remove obstruction for pedestrians.
- Existing carriageway chicanes at the gated closure of Trinity Street to be adjusted to increase the gap width from 1.2m to 1.5m in line with current cycling accessibility guidelines.

(See Appendix A – Initial Scheme Design)

1.3 Consultation Procedure

- 1.3.1 The views of the local community and those of statutory consultees have been sought, prior to the development of measures to a detailed design stage. Active community participation was encouraged through the use of a consultation document and questionnaire (see Appendix B – Consultation Documents).
- 1.3.2 The consultation document included a covering letter describing the proposals and a request for comments (including information to assist in translation and large print versions of the consultation document), preliminary design drawings (A3 size) and a questionnaire/comment form that could be sent to the Public Realm Projects Group with a pre-paid address reply.
- 1.3.3 The consultation document was delivered to a geographical area centred on Globe Street and the area adjacent to the junction of Trinity Street and Globe Street, using strategic roads and pedestrian desire lines as defined cut off points (See Appendix C – Location Plan and Extents of Consultation).
- 1.3.4 The distribution area was large enough to gain views from the wider community that may be considered to be affected by the proposed measures. A mailing list was established for the area by way of the Council's GIS database. In addition, the consultation documents and plans were supplied to the Council's established list of statutory consultees including London Buses, cycle groups and the Metropolitan Police. Please see Appendix D of list of addresses within the distribution area. Consultation documents were also delivered to the Southwark Housing Department and Grange Primary School.
- 1.3.5 The consultation documents were delivered by Royal Mail to 302 addresses detailed within the distribution list. The documents were delivered on the 5th August 2014, with a return deadline of the 5th September, allowing 4 weeks for the consultation period. However due to the summer holiday period, responses were accepted online until the 12th September 2014.

- 1.3.6 The proposals were also available to view online using consultation section of the council's website, with an e-form questionnaire provided in order to capture responses.

2.0 Consultation Responses

2.1 Response Rate and Distribution

- 2.1.1 A total of 45 responses were received during the consultation period (32 returned questionnaires, 13 online responses and one email), equating to a 15% response rate.
- 2.1.2 Five responses were received from Statutory Consultees (Southwark Living Streets, Southwark Cyclists, Lambeth Cyclists, Wheels for Wellbeing and Sustrans).

2.2 Questionnaire Analysis

- 2.2.1 The questionnaire element of the consultation document contained the following key questions and associated tick box options:

Q1. Are you a resident or business?

Q2. Do you support the proposals?

- 2.2.2 The following is a summary of replies received:

Question 1 - Are you a resident or business?

	Resident	Business
Replies	43	2
Total	96%	4%

Table 1: Returned questionnaire results for question 1

- 2.2.3 Table 1 indicates that the majority of responses received throughout the consultation period were from local residents, with only two businesses formally replying.

Question 2 – Do you support the proposals?

	Support	Opposed	No Opinion
Replies	26	16	3
Total	57.5%	35.5%	7%

Table 2: Returned questionnaire results for question 2

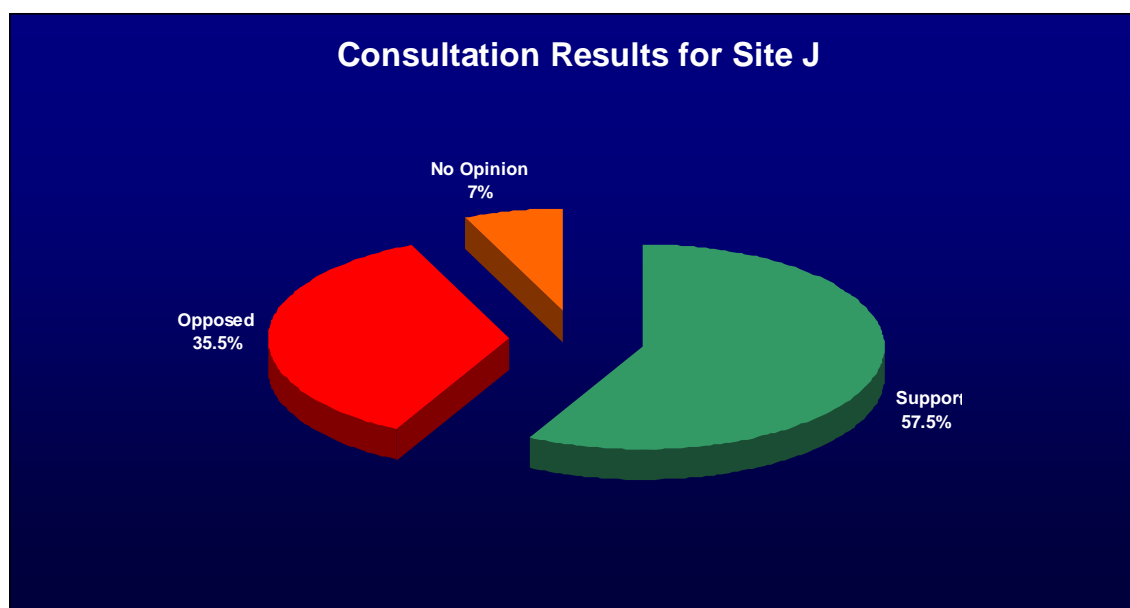


Figure 3: Graphical representation of consultation data for question 2

2.2.4 Table 2 and figure 2 indicate a majority of support for question 2, with 57.5% welcoming the proposed measures.

2.3 Additional Comments

2.3.1 The questionnaire element of the consultation document invited consultees to attach any additional comments they may have on the proposals when returning the reply-paid questionnaire. Consultees were also able to reply to the consultation online using the council's website.

2.3.2 The majority of respondents (57.5%) indicated support for the proposed measures, stating that the design looks great and the improvements are beneficial to both cyclists and residents.

2.3.3 Respondents were happy to see a safer pathway for bikes in Globe Street and also the trees and new footways are greatly needed and for all to enjoy.

2.3.4 A comment was received highlighting that the proposed yorkstone paving in Globe Street will integrate the space with Trinity Street.

2.3.5 A number of comments were received that the scheme will benefit cyclists and it is fantastic that more trees and less bollards are proposed.

2.3.6 A respondent commented that the proposal is fantastic and the idea of a raised table at the junction of Cole Street should help slow vehicles down and the cycle lanes should encourage more people to use their bikes more often.

2.3.7 A request was made for cycle lanes to be installed in Great Dover Street as they are badly required. *

* In response, Great Dover Street is controlled and maintained by Transport for London and not the council. Therefore the council is unable to investigate this request further.

2.3.8 A concern was raised about the 'exit only' proposal for vehicles exiting from Shere House as it will make accessibility to this area even more difficult than it already is. *

* In response, there is already a no entry restriction (except cyclists) to the north of Cole Street. Therefore technically motor vehicles are not permitted to access Shere House via Globe Street. Motor vehicles will still be able to access Shere House via Trinity Street. Formalising the existing arrangement will also reduce the potential for head on conflict between vehicles and cyclists using the Quietway route in Globe Street, simply traffic movement at the junction and improve safety and accessibility for pedestrians.

2.3.9 A comment was made that there is no real requirement for a raised table at the junction of Globe Street and Cole Street. *

* In response, a raised carriageway table at this junction will curtail traffic speeds and improve safety and accessibility for all road users, particularly pedestrians and cyclists. Currently the junction is inaccessible for mobility impaired pedestrians, with no formal crossing facilities or dropped kerbs to cross Globe Street from east to west.

The raised table allows for the implementation of an east to west pedestrian crossing location across Globe Street and upgrade of the existing informal dropped kerb across Cole Street. The table will provide a level surface for pedestrians to cross the road and will tie in with the adjacent streetscape proposals in Globe Street between Cole Street and Great Dover Street. The use of high quality materials will also add visually to the streetscape and be in keeping with the adjacent heritage zoned area of Trinity Street.

2.3.10 A number of concerns were raised about motorcyclists using the Trinity Street access gate and if the spaces between the gates are widened, then this will make the situation worse and encourage more motorcyclists to illegally use the gate. *

* In response, numerous site observations took place at the site and no motorcyclists were observed using the gates. Since Trinity Street was closed at its junction with Great Dover Street, Trinity Street no longer provides a direct link between Borough High Street and Great Dover Street, which was an attractive route to all vehicles to bypass the traffic signal junction at Great Dover Street and Borough High Street. Therefore the likelihood that increased contraventions will

take place at the gate involving motorcycles if the gate is either removed or altered is minimal.

- 2.3.11 A comment was made that the barriers in Trinity Street need to be removed as their presence is not in keeping with the rest of the route and will create a serious pinch point as more cyclists start to use the route. The 1.5m gap proposed is not wide enough for mobility bikes. *

* In response, the proposed 1.5m gap between the panels adjacent to the gate will improve current situation, making it easier for able bodied cyclists to negotiate this obstacle. However it is recognised that this may still not be sufficient to ensure that this section of Quietway route is accessible to cyclists of all abilities. Therefore the presence of the barrier could potentially act as a form of severance on the route and disabled cyclists may find it extremely difficult to manoeuvre and traverse through this space. This has been confirmed by Wheels for Wellbeing (cycling disability charity), which is detailed later in the report.

- 2.3.12 Analysis of the additional comments from respondents that objected to the scheme highlighted the following concerns:

A number of objections were received stating that the proposals are a waste of money, will not improve the area and will only benefit a few.*

* In response, the proposed measures along the Quietway route align with the council's emerging cycling strategy and the Mayor's Vision for Cycling. The Mayor has commitment to invest total of £913m over the next 10 years in cycling safety and infrastructure development to significantly increase the modal share in cycling as a safe, healthy and sustainable form of transport in London.

The London boroughs will play a central role in delivering this vision, by helping to develop, fund and deliver better and safer routes for cycling.

The council welcomes significant investment from Transport for London to take forward the Quietway programme that will not only significantly upgrade a number of existing cycle routes, but also make considerable improvements to the streetscape for the benefits of all road users and local residents.

The proposals in Globe Street will provide a significant improvement to the streetscape, with specific benefits to cyclists and pedestrians. The section of Globe Street between Cole Street and Great Dover Street is currently in disrepair, with narrow pedestrian footways and a wide carriageway that attracts indiscriminate parking that blocks cycle access. The junction with Cole Street currently has poor pedestrian accessibility with high kerb up-stands and the absence of adequate pedestrian crossing locations. The footways in Globe Street are also cluttered with bollards which narrow footway width and present obstacles for pedestrians.

The proposed measures in Globe Street will provide wider footways for pedestrians and unobstructed, segregated cycle tracks to allow cyclists to effectively traverse the space without conflicting with pedestrians. The raised table at Cole Street junction will curtail traffic speeds and greatly improve pedestrian accessibility.

The above improvements will be implemented in high quality materials that will compliment the adjacent heritage area of Trinity Street. In addition, tree planting will create a more pleasant environment for all road users, especially local residents of Shere House that will have a direct access onto the space.

Making proposed changes to the layout of the existing access gate on Trinity Street will not only benefit cyclists, but also discourage cyclists from using adjacent local footways to bypass the gate, which has a clear benefit for pedestrians and local residents in Trinity Street living adjacent to the gate.

A respondent objected stating the proposed development is unnecessary and overcomplicated. Globe Street is already just for cyclists and there is no need to reduce traffic speeds or widen the gate at in Trinity Street.*

* In response, as outlined above, the street environment of Globe Street is poor compared with the streetscape in adjacent areas. As there is no traffic traversing this section of Globe Street, there is an excellent opportunity as part of the Quietway initiatives to upgrade the streetscape into a high quality public space that can be enjoyed by all road users.

The proposed layout of the Cole Street junction will simplify traffic operation by formalising the northbound no entry prohibition on Globe Street and vehicular exit out of Shere House. The colour differentiation between materials used for carriageway sections and footway will delineate the areas, which will be clearly identifiable to pedestrians, motorists and cyclists. Signage will also be rationalised to make the road layout and restrictions legible.

Whilst the section of Globe Street between Cole Street and Great Dover Street is already technically just for cyclists, vehicles still park on the existing single yellow line restrictions adjacent to both kerblines, especially in the evening. This creates obstruction of cyclists. As outlined above, this section is also a busy pedestrian thoroughfare and the existing footways are narrow, with many pedestrians walking in the carriageway, which presents potential conflict risks with cyclists.

Due to the carriageway gate in Trinity Street, vehicles traverse Globe Street, Cole Street and Swan Street to access areas either side of Trinity Church Square. Although the streets have a 20mph speed limit, there are currently no vertical deflection measures to assist with enforcing this speed limit. The raised table proposed at Cole Street will act as a self enforcing speed measure and also curtail traffic speeds adjacent to pedestrian crossing locations, which improves safety and reduces the likelihood and severity of potential collisions with pedestrians.

The existing layout of the access gate in Trinity Street makes it difficult for less confident and mobility impaired cyclists to ride through the gaps without dismounting or colliding with the infrastructure. Therefore modifications are required to ensure this issue is addressed so that the Quietway route is fully inclusive and the barrier does not present an undesirable obstacle to this important cycle route.

A number of respondents objected stating that more cyclists will use the footway to avoid the carriageway barrier endangering pedestrians and children. *

* In response, modifications to the existing barrier will potentially alleviate this issue by making it easier for cyclists to traverse through without conflict.

A number of responses highlighted that many cyclists traverse the northern and southern pedestrian footways of Trinity Street in order to bypass the barrier in the carriageway. This presents a safety issue for pedestrians and creates a potential conflict zone. The footways, even with a chicane barrier on the southern footway, present less of an obstacle to cyclists that using the carriageway barrier that has a chicane barrier each side of the main gates.

With cycling levels increasing and the Quietway route potentially carrying a significant amount of cycle traffic, more cyclists could potentially traverse the footway to bypass the barrier. Therefore changes to the barrier are essential not only to improve cycling accessibility, but to also ensure cyclists traverse along the carriageway and not use adjacent footways.

Numerous objections were received relating to the proposal to widen the barrier on Trinity Street. *

* In response, as discussed previously, the existing layout of the barrier presents an accessibility issue to the Quietway route. The existing gap is not appropriate to accommodate considerable volumes of cyclists in morning and afternoon peak periods and provides an obstruction to mobility cycles (which is discussed later in section 2.5.1).

The width of the access points either side of the gate are also below the minimum standard for a cycle route (being 1.5m). It is noted that whilst 1.5m is the minimum requirement, this may still be an issue for disabled cyclists, as the current layout of the gate and access panels creates a double chicane that is more difficult to negotiate than a normal type barrier.

The access gate was originally installed to prevent non-local traffic traversing Trinity Street that entered from Great Dover Street in the east to access Borough High Street in the west. This route provided a means for traffic to bypass the congested Long Lane and Borough High Street / Marshalsea Road junctions. A number of years following the implementation of the gate, the Trinity Street / Great Dover Street junction was closed to vehicular traffic and pedestrianised. Therefore the access into the eastern end of Trinity Street has been totally cut off from Great Dover Street (also taking into account the existing closure of Globe Street at its junction with Great Dover Street). As there are no direct routes through to Great Dover Street and more direct routes to access Old Kent Road (via Harper Road), the attractiveness of traversing Trinity Street to access roads in the east or west is negligible.

As discussed above, due to the complexity of the existing barrier and gate configuration, a number of cyclists prefer to traverse the adjacent pedestrian footways to the bypass the carriageway gate. It is noted that the proposed widening of the gate panels will potentially assist with addressing this issues, as the gates and barrier will be easier to traverse through.

It is noted that there is an existing section 6 prescribed route traffic order to fully enforce the no entry restrictions (except cyclists) at the Trinity Street barrier. If in the unlikely occurrence that motorcyclists are observed traversing through the access gate following the modifications, then the site can be enforced using CCTV in order to issue automatic penalty charge notices to the offending riders.

Numerous respondents objected on the grounds that the retention of the existing barrier is a major flaw in the scheme. It is extremely awkward to negotiate, even on a normal bike let alone a mobility bike. The barrier is an insurmountable hurdle to disabled cyclists and needs to be redesigned for DDA reasons. Use a row of bollards instead. *

* In response, widening the barrier width adjacent to the gate will assist the passage of cyclists, making it easier to manoeuvre through this infrastructure without conflict.

Whilst it is noted that removal of the barrier and installation of bollards would clearly be a better solution for cycling accessibility purposes, the council wishes in the first instance, to monitor the effectiveness of increasing the gap to 1.5m either side of the barrier. If it is found that this is still not adequate to cater for large cycling volumes or mobility cycles, then the site will be revisited with further solutions proposed to improve accessibility.

A objection was received highlighting that the proposals actually make condition worse for cyclists by introducing more give way points on Globe Street (giving priority to pedestrians and traffic existing Shere House. Traffic on Trinity Street should give way to cyclists entering / exiting Globe Street.

* In response, changing the priorities at the junction of Trinity Street and Globe Street will have little benefit, as traffic only approaches Globe Street from the left due to the presence of the carriageway barrier to the western side of the junction. The change in priorities would also involve the removal of a parking bay on the southern side of Trinity Street in order for give way priority markings to be installed. This could lead to further objections from local residents, as there is no alternative location in close proximity to relocate the parking bay.

Cycling logos will be painted on the carriageway of the junction to increase driver awareness of the likelihood of cyclists traversing the junction. Traffic speeds and volumes are also low at this location and therefore the risk of collisions with cyclists is minimal.

2.3.9 14% respondents did not submit a further comment.

2.4 Levels of Consensus

2.4.1 The following majority levels of agreement have been given in relation to the questions contained within the consultation document:

- 57.5% of respondents support the introduction of the Quietway cycle route proposals for Site J;
- 35.5% of respondents are opposed to the proposals; and
- 7% of respondents had no opinion.

2.5 Statutory Consultee Replies

2.5.1 Five statutory consultees provided a reply to the consultation.

a) **Wheels for Wellbeing** replied, making specific reference to the carriageway barrier in Trinity Street. The organisation confirmed that the barrier makes it extremely difficult for non-standard cyclists to manoeuvre through the gap and is preventive to inclusive cycling. Further comments are summarised below;

- Quietway routes are meant to be for less confident cyclists. However negotiating the barriers takes a great deal of skill and is therefore off-putting to less confident cyclists.
- Larger mobility bikes are likely to become stuck between the barriers. Disabled cyclists are less likely to dismount and walk their bike around the obstacle and larger tandem and cargo bikes are also not likely to negotiate the acute angle required to pass through the gate.
- The scheme therefore unreasonable disadvantages disabled cyclists.
- The 1.2m existing gap is certain not sufficient and whilst the 1.5m proposed gap would be an improvement, it is far more preferable not to have a barrier at all.

* In response, in light of the above comments, the council will closely monitor the proposed changes to ascertain if they meet the objectives of improving access for cyclists of all abilities. If further accessibility issues arise following the implementation of the barrier widening, then investigations will take to identify potential further modifications.

b) **Southwark Living Streets** replied indicating strong support for the streetscape improvements, as they will make a fantastic difference to the functionality and appearance of Globe Street. However a concern was raised as to the validity of the barrier in Trinity Street remaining, as it is not appropriate for main cycle route.

c) **Lambeth Cyclists** replied in support of the scheme but expressed concern that given a limited budget, and the extent of works proposed in Globe Street, money would be better spent elsewhere. Cyclists should also not have to give way to pedestrians or vehicles exiting Shere House. The barriers in Trinity Street should be removed and replaced with bollards. *

* In response, an integral part of developing Quietway routes is improving the streetscape that not only improves conditions for cyclists, but also assists other road users, particularly pedestrians. Such improvements also make the route more attractive and pleasant to use. As outlined previously, the proposals in Globe Street will benefit cyclists, pedestrians and local residents. The measures will also improve pedestrian accessibility and road safety by curtailing traffic speeds at the Cole Street junction. The materials proposed compliment the adjacent heritage area and recently implemented streetscape schemes in Great Dover Street.

As part of the detailed design process, the council will review road user priority, particularly on the cycle tracks and exit out of Shere House. If there are no issues raised as part of the road safety audit process, the priorities will be

revised so that cyclists have priority for the entire length of the segregated cycle path, including at the exit from Shere House.

The council has noted the comment regarding the request to remove the barrier and replace it with bollards, which is technically the best solution for cyclists, whilst still preventing motor vehicle access.

- d) **Southwark Cyclists** replied indicating support for the majority of the scheme. However they confirmed that the gated closure into Trinity Church Square is an unnecessary barrier. They discussed that as indicated in the London Cycle Design Standards, chicanes must be avoided and Trinity Street barrier is a double chicane which is ridiculously narrow. A request was made to remove the barrier and install bollards in light of the fact that the proposed 1.5m gaps will still not be enough to cater for all cyclists or the projected number of cyclists that will use the Quietway route. In addition they mentioned that cyclists should have priority over vehicle exiting Shere House. *

* In response, as outlined above, road user priority will be revised in Globe Street with a view to give cyclists using the cycle tracks priority over vehicles exiting Shere House.

Noting the local opposition to changing or removing the existing barrier in Trinity Street, the council wishes in the first instance, to monitor the proposed gap widening to ascertain its effectiveness.

- e) **Sustrans** replied indicating support for the scheme but did not provide any specific questions or comments on the proposed measures.

2.5.2 A reply was received from Chaucer Ward councillor Claire Maugham and comments regarding this consultation are summarised below;

- Widening the gates would not deter motorbikes and residents are very concerned about a potential return to the Square being used as a rat run.
- Given that there has been no convincing case made for altering the gates, the Labour group gives residents its full support in not altering the current layout.
- The existing gate layout allows bikes to easily pass through and Southwark Cyclists are fully supportive of the gates remaining as they are.
- There are no residents or local groups asking for this widening.

2.5.3 A written response was received from the Trinity Newington Residents' Association. Their comments are summarised below;

- Support for the measures in Globe Street and happy with the quality of the proposed changes, especially the introduction of yorkstone on Globe Street footways.
- The removal of existing footway bollards in Globe Street and Trinity Street is also welcomed.
- Strongly object to the proposed changes to the Trinity Street gate as it will make it easier for mopeds and motorcyclists to use the gate resulting in danger to road users.

3.0 Recommendations

i) Globe Street

Due to the majority of respondents supporting the scheme proposals for Globe Street and Southwark's on-going commitment to improve and promote cycling in the borough, it is recommended that the scheme proceed to implementation.

ii) Trinity Street

Due to the comments received from key cycling group stakeholders regarding accessibility and potential obstruction of disabled cyclists, it is proposed that the barrier either side of the carriageway gates on Trinity Street are removed and replaced with bollards. This will be done experimentally and monitored using ANPR cameras over a periods of 6 months.

Appendices

Appendix A:	Initial Scheme Design
Appendix B:	Consultation Documents
Appendix C:	Location Plan and Extents of Consultation
Appendix D:	List of Addresses within the Distribution Area

Appendix A: Initial Scheme Design

Appendix B: Consultation Documents

We want your views

It is important for all consultees to respond to the consultation. We would be grateful if you could take the time to review the proposals outlined in this document and provide a response using the pre-paid envelope and questionnaire provided by **Friday 5th September 2014**

Alternatively, you can view the proposals at www.southwark.gov.uk/consultations and complete the online questionnaire.

Your views are essential for us to understand your requirements for the proposal and form a fundamental part of the scheme development process, whether you use public transport, cycle, walk or drive a private vehicle.



Quietway
Greenwich to Waterloo

Site J – Globe Street and Trinity Street

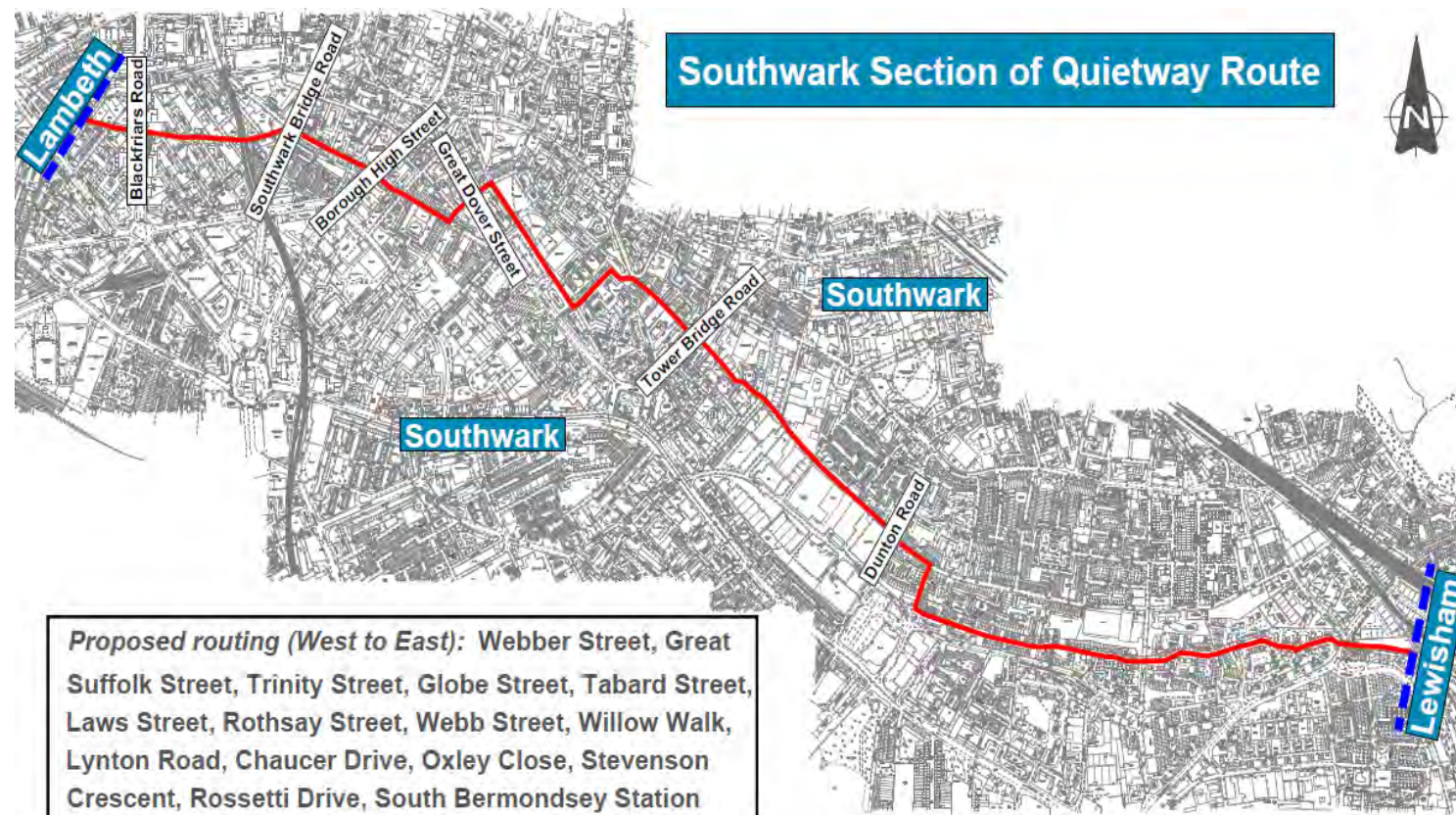
Cycling Accessibility and Streetscape Improvements

What happens next?

The responses to the questionnaire will be analysed and taken into account in the final design of the proposed works. As you will appreciate Southwark Council receives many comments from consultations and therefore are unable to respond personally to specific issues raised. However all comments and suggestions will be taken into consideration before a decision is made. The consultation results and recommendations will be presented at a local community council meeting in September 2014.

Should you require any further information regarding the proposed scheme please do not hesitate to contact Chris Mascord at chris.mascord@southwark.gov.uk

Further information on other schemes along the route in Southwark can also be found at: www.southwark.gov.uk/consultations



Have your say

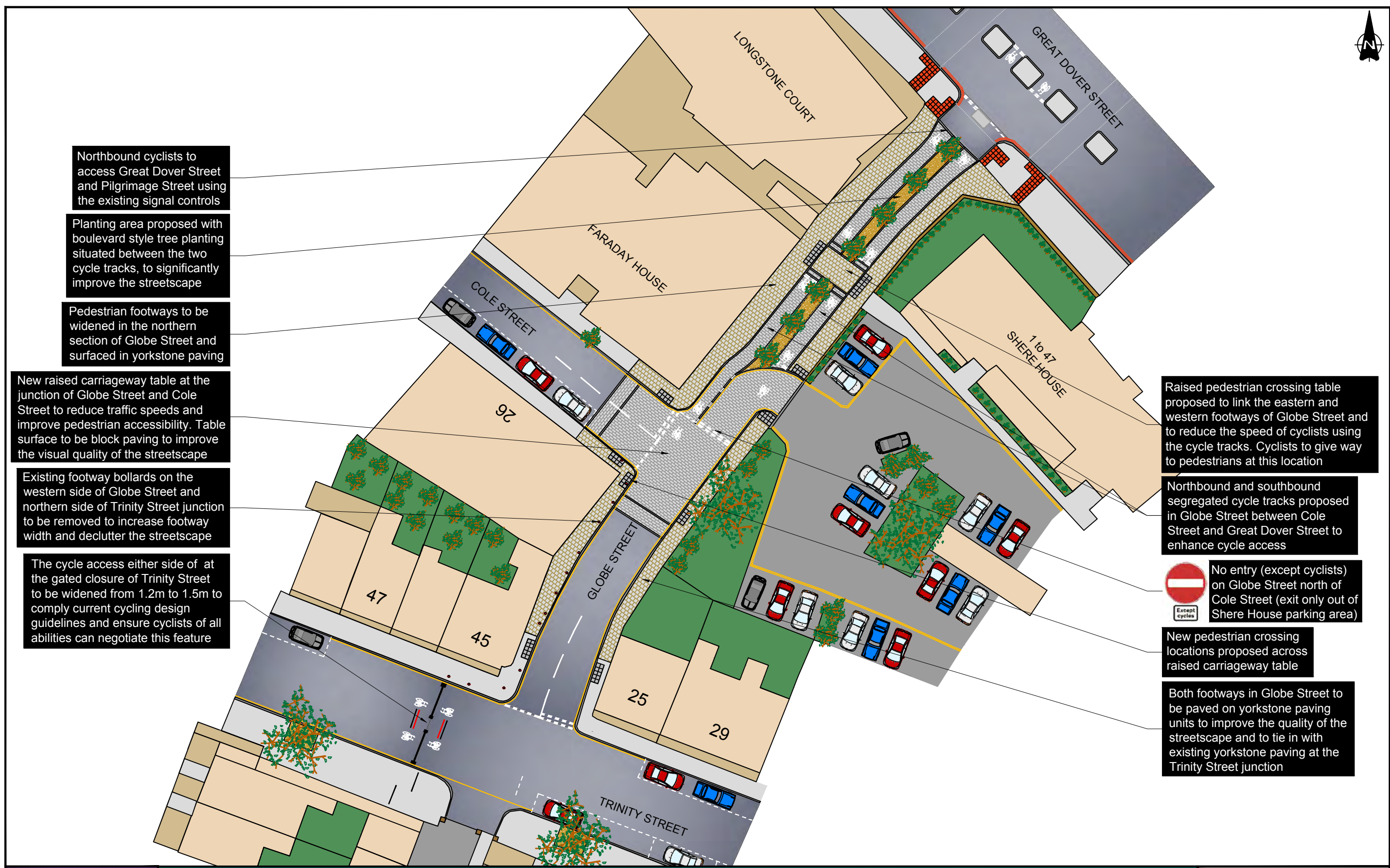
Southwark Council is holding a consultation to receive residents' and key stakeholders' comments regarding proposals relating to the borough's section of the Quietway (Greenwich to Waterloo) cycle route. This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey station in the east and traversing local roads to The Cut in the west. The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

Background

Quietways are a network of bike routes for less confident cyclists using low-traffic back streets. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.

What are the proposed changes?

- The section of Globe Street between Cole Street and Great Dover Street is to become a new public space for pedestrians and cyclist, with the carriageway removed and area paved to create new footways, tree planting and segregated cycle tracks.
- No entry (except cyclists) on Globe Street north of Cole Street (exit only out of Shere House parking area).
- New raised carriageway table at the junction of Globe Street and Cole Street to reduce traffic speeds and improve pedestrian accessibility. Table surface to be block paving to improve the visual quality of the streetscape
- Both footways in Globe Street to be paved on yorkstone paving units to improve the quality of the streetscape and to tie in with existing yorkstone paving at the Trinity Street junction
- Existing footway bollards on the western side of Globe Street and northern side of Trinity Street to be removed to increase footway width and remove obstruction for pedestrians.
- Existing carriageway chicanes at the gated closure of Trinity Street to be adjusted to increase the gap width from 1.2m to 1.5m in line with current cycling accessibility guidelines.



Northbound cyclists to access Great Dover Street and Pilgrimage Street using the existing signal controls

Planting area proposed with boulevard style tree planting situated between the two cycle tracks, to significantly improve the streetscape

Pedestrian footways to be widened in the northern section of Globe Street and surfaced in yorkstone paving


New raised carriageway table at the junction of Globe Street and Cole Street to reduce traffic speeds and improve pedestrian accessibility. Table surface to be block paving to improve the visual quality of the streetscape

Existing footway bollards on the western side of Globe Street and northern side of Trinity Street junction to be removed to increase footway width and declutter the streetscape

The cycle access either side of at the gated closure of Trinity Street to be widened from 1.2m to 1.5m to comply current cycling design guidelines and ensure cyclists of all abilities can negotiate this feature

Raised pedestrian crossing table proposed to link the eastern and western footways of Globe Street and to reduce the speed of cyclists using the cycle tracks. Cyclists to give way to pedestrians at this location

Northbound and southbound segregated cycle tracks proposed in Globe Street between Cole Street and Great Dover Street to enhance cycle access

 No entry (except cyclists) on Globe Street north of Cole Street (exit only out of Shere House parking area)

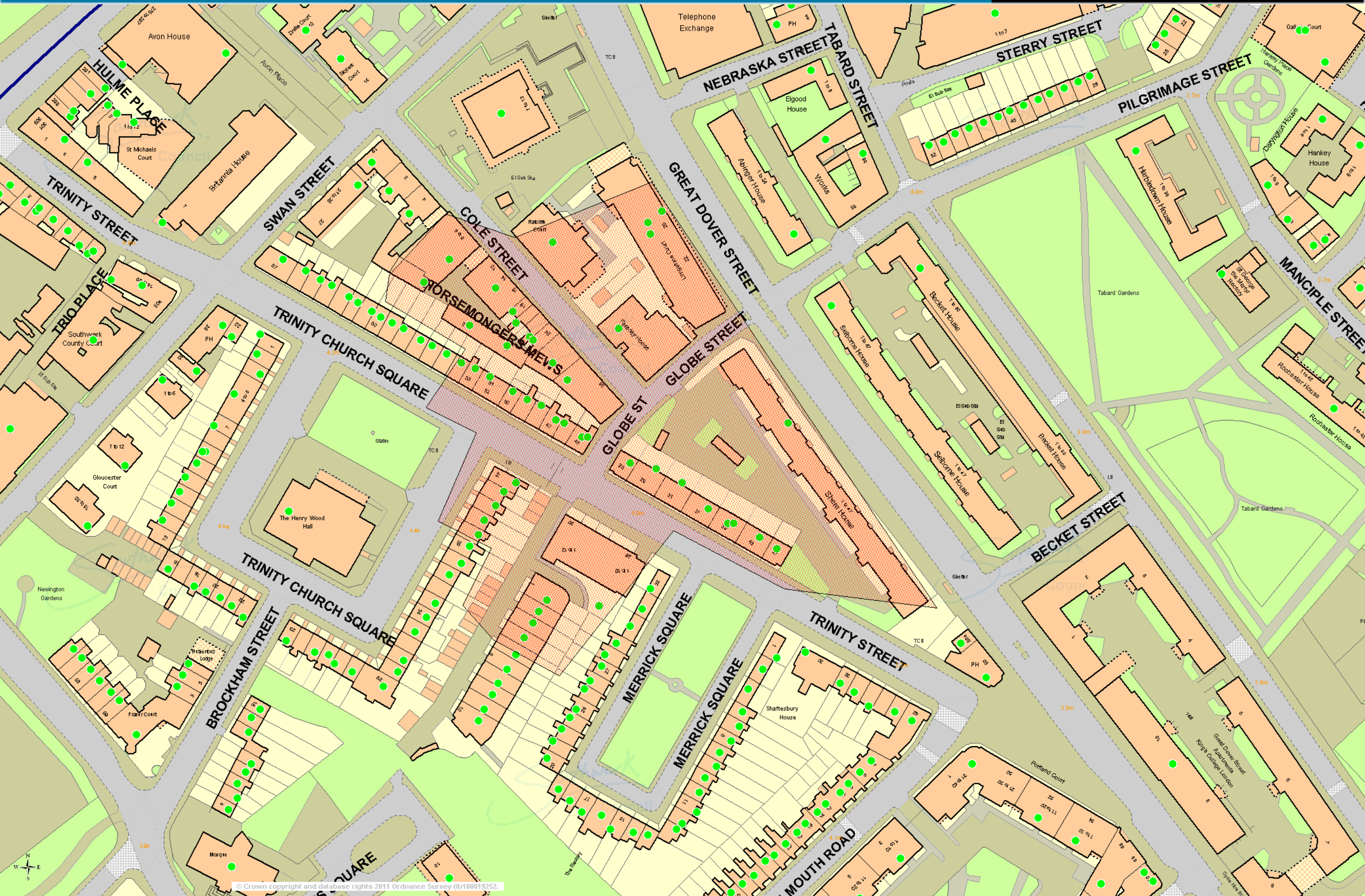
New pedestrian crossing locations proposed across raised carriageway table

Both footways in Globe Street to be paved on yorkstone paving units to improve the quality of the streetscape and to tie in with existing yorkstone paving at the Trinity Street junction

Appendix C: Location Plan and Extents of Consultation

Quietway Site J - Consultation Area

Date 30/7/2014



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Appendix D: List of Addresses within Distribution Area

ORGANISATION	SAO	PAO	STREET	TOWN	POSTCODE
	FLAT 5		28 COLE STREET	LONDON	SE1 4YH
		68	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
	FLAT 105		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
		18	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		79	RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LD
		103	RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LD
		1	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		89	RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LD
		93	RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LD
		98	RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LD
	FLAT 107		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
	FLAT 112		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
	FLAT 117		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
	FLAT 121		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
	FLAT 126		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
	FLAT 130		FARADAY HOUSE, 23	COLE STREET	LONDON SE1 4LE
		61	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
			51 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
		45A	TRINITY CHURCH SQUARE	LONDON SE1 4HT	
		12	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		102	RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LD
		35	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		48	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		64	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		46	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		53	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		74	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
EVERLUTION SOFTWARE LTD	STUDIO 2 AND 3	6-8	COLE STREET	LONDON SE1 4YH	
			22 COLE STREET	LONDON SE1 4YH	
	FLAT 1		44 TRINITY CHURCH SQUARE	LONDON SE1 4HY	
	FLAT 20	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
		39A	TRINITY STREET	LONDON SE1 4JA	
			42 TRINITY CHURCH SQUARE	LONDON SE1 4HY	
ECOLOGICAL CONSULTANCY LTD	STUDIO 5 AND 6	6-8	COLE STREET	LONDON SE1 4YH	
		29A	TRINITY STREET	LONDON SE1 4JA	
		31C	TRINITY STREET	LONDON SE1 4JA	
		39C	TRINITY STREET	LONDON SE1 4JA	
		47B	TRINITY STREET	LONDON SE1 4JA	
		45B	TRINITY CHURCH SQUARE	LONDON SE1 4HT	
		31	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		25	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		34	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		42	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
	FLAT 12	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 21	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 42	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 1		28 COLE STREET	LONDON SE1 4YH	
	FLAT 6		28 COLE STREET	LONDON SE1 4YH	
		28A	COLE STREET	LONDON SE1 4YH	
	STUDIO 10	6-8	COLE STREET	LONDON SE1 4YH	
	STUDIO 11	6-8	COLE STREET	LONDON SE1 4YH	
	STUDIO 12	6-8	COLE STREET	LONDON SE1 4YH	
	FLAT 1		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 2		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 3		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 4		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 5		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 6		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 7		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 8		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 9		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 10		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 11		30 TRINITY STREET	LONDON SE1 4FF	
	FLAT 12		30 TRINITY STREET	LONDON SE1 4FF	
			1 BEDFORD ROW	LONDON SE1 4GP	
			2 BEDFORD ROW	LONDON SE1 4GP	
			3 BEDFORD ROW	LONDON SE1 4GP	
			4 BEDFORD ROW	LONDON SE1 4GP	
	FLAT 38	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 33	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 23	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 28	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 31	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 46	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 7	SHERE HOUSE	GREAT DOVER STREET	LONDON SE1 4YQ	
	FLAT 2		52 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
	FLAT B		47 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
		75	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
		11	LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON SE1 4LB
INTEGRITY ARTIST MANAGEMENT LTD	STUDIO 7	6-8	COLE STREET	LONDON SE1 4YH	
			46 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
			48 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
			50 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
			53 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
		26A	COLE STREET	LONDON SE1 4YH	
			18 COLE STREET	LONDON SE1 4YH	
			20 COLE STREET	LONDON SE1 4YH	
			24 COLE STREET	LONDON SE1 4YH	
		39D	TRINITY STREET	LONDON SE1 4JA	
	FLAT 2		44 TRINITY CHURCH SQUARE	LONDON SE1 4HY	
	FLAT 3		52 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
	FLAT 5		49 TRINITY CHURCH SQUARE	LONDON SE1 4HT	
			1 HORSEMONGER MEWS	LONDON SE1 4GG	
			3 HORSEMONGER MEWS	LONDON SE1 4GG	
		31B	TRINITY STREET	LONDON SE1 4JA	
		37A	TRINITY STREET	LONDON SE1 4JA	
		37B	TRINITY STREET	LONDON SE1 4JA	
		37C	TRINITY STREET	LONDON SE1 4JA	
		39B	TRINITY STREET	LONDON SE1 4JA	
		43A	TRINITY STREET	LONDON SE1 4JA	

	43B	TRINITY STREET	LONDON	SE1 4JA
		40 TRINITY CHURCH SQUARE	LONDON	SE1 4HY
		41 TRINITY CHURCH SQUARE	LONDON	SE1 4HY
		43 TRINITY CHURCH SQUARE	LONDON	SE1 4HY
	25A	TRINITY STREET	LONDON	SE1 4JA
	43C	TRINITY STREET	LONDON	SE1 4JA
	47A	TRINITY STREET	LONDON	SE1 4JA
	47C	TRINITY STREET	LONDON	SE1 4JA
	25B	TRINITY STREET	LONDON	SE1 4JA
	25C	TRINITY STREET	LONDON	SE1 4JA
	29B	TRINITY STREET	LONDON	SE1 4JA
	29C	TRINITY STREET	LONDON	SE1 4JA
	31A	TRINITY STREET	LONDON	SE1 4JA
	GARDEN FLAT	39 TRINITY STREET	LONDON	SE1 4JA
DHA DESIGN SERVICES LTD	STUDIO 4	6-8 COLE STREET	LONDON	SE1 4YH
FORGE ARCHITECTS AND SURVEYORS LTD	STUDIO 8	6-8 COLE STREET	LONDON	SE1 4YH
EVERLUTION SOFTWARE LTD	STUDIO 9	6-8 COLE STREET	LONDON	SE1 4YH
		8 COLE STREET	LONDON	SE1 4YH
	82 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
FLAT 1		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 2		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 3		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 4		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 5		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 6		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 7		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 8		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 9		28 TRINITY STREET	LONDON	SE1 4FF
FLAT 10		28 TRINITY STREET	LONDON	SE1 4FF
	13 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	16 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	17 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	23 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	30 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	36 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	5 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	52 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
ADL DELIVER LTD	STUDIO 1	6-8 COLE STREET	LONDON	SE1 4YH
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	59 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	62 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	67 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
FLAT 104	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 106	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 108	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 109	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 110	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 111	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 113	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
	7 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	72 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	63 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	65 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
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FLAT 128	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 129	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 131	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
	24 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	26 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	28 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	99 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
	100 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
	101 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
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FLAT 116	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 118	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
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FLAT 120	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 122	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 132	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 133	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
	80 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
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	85 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
	21 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	22 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	84 RATCLIFFE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LD
	69 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
	70 LONGSTONE COURT, 22	GREAT DOVER STREET	LONDON	SE1 4LB
FLAT 123	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE
FLAT 124	FARADAY HOUSE, 23	COLE STREET	LONDON	SE1 4LE

London Borough of Southwark



Quietway Cycling Proposals

Site K – Borough High Street Junction and Great Suffolk Street

Public Consultation Summary

September 2014

London Borough of Southwark

Site K - Quietway Cycling Proposals Borough High Street Junction and Great Suffolk Street

Public Consultation Summary

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1.0 Introduction

1.1 Background

1.1.1 This document report has been produced by the London Borough of Southwark Public Realm Projects Group, to provide a summary of the consultation exercise for the Quietway Cycling Proposals for Site K at the Borough High Street / Trinity Street / Great Suffolk Street junction and along Great Suffolk Street to Toulmin Street. The measures are being drafted by the Public Realm Projects Team, with the project manager for this scheme being Chris Mascord (Senior Engineer).

1.1.2 The area under consideration is located within the SE1 district of Southwark (Cathedrals and Chaucer Ward) in the north of the borough. See figure 1 below.

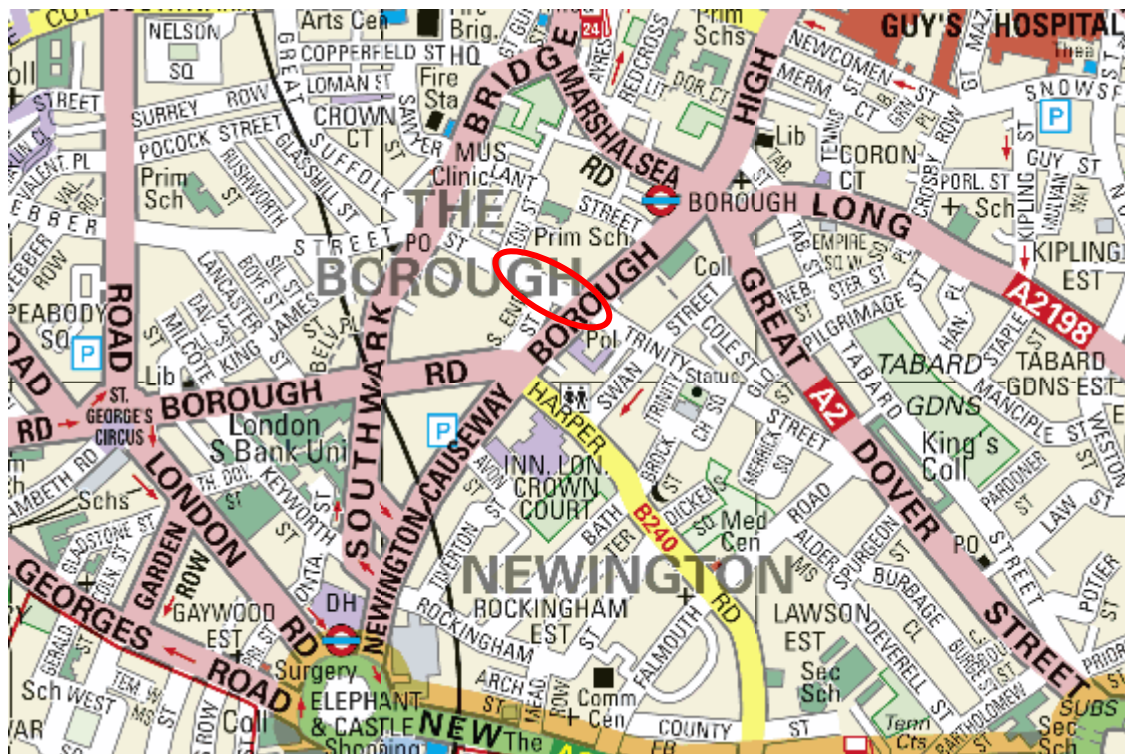


Figure 1: Location of proposed scheme

1.2 Project and Objectives

1.2.1 This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey Station in the east and traversing local roads to The Cut in the west (see figure 2). The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

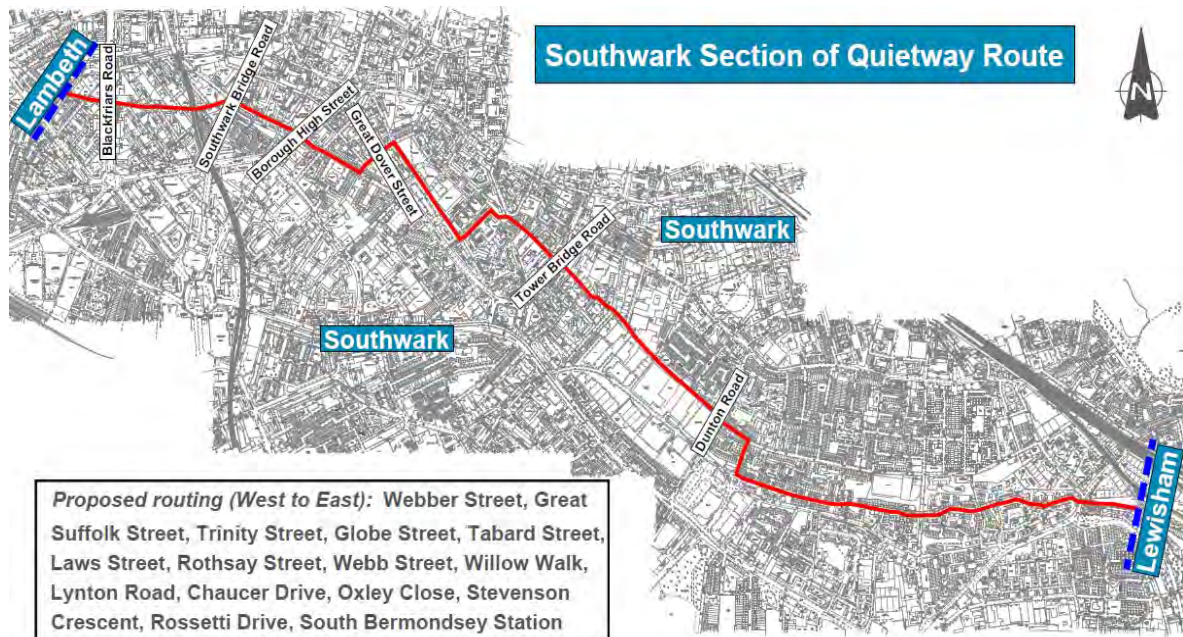


Figure 2: Southwark's section of Quietway Route

- 1.2.2 Quietways are a network of bike routes for less confident cyclists using residential streets with low traffic volumes. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.
- 1.2.3 The measures proposed in this consultation are part of the Council's ongoing commitment to make Southwark's streets safer and more accessible for all. The proposed measures will enhance the environment for all road users, reducing traffic speeds and improving pedestrian safety. Cycling proposals also have the added health benefits of improving the environment through reducing carbon emissions and getting more people onto bikes which in turn enhances their fitness and health.
- 1.2.4 The following measures were consulted upon to improve cycling and pedestrian accessibility and enhance the streetscape for Site K:
- Borough High Street junction to be significantly upgraded including an 'all green' pedestrian phase to improve safety, with automated countdown system so pedestrians know how much time they have left to cross the road.
 - Central pedestrian carriageway islands to be enlarged to provide a safer environment for pedestrian to wait to cross the road.
 - Subject to approval from the DfT and other governing bodies, the Great Suffolk Street and Trinity Street arms will have an early start cycle pre-signal so cyclists can traverse the junction and undertake turning movements before general traffic.

- Existing raised carriageway tables in Trinity Street and Great Suffolk Street are to be enlarged to provide cycle waiting areas at the signals. Both approaches will have semi segregated cycle lanes so that cyclists can have unobstructed access to the waiting areas.
- Great Suffolk Street to be resurfaced between Toulmin Street and Borough High Street to improve the ride quality for cyclists.
- Footways of Great Suffolk Street between Toulmin Street and Borough High Street be built out to inset the parking bays and narrow carriageway to reduce vehicle speeds and improve the quality of the streetscape.
- Footways on both sides repaved in new materials, with tree planting where possible to improve the streetscape.
- New carriageway table with footway buildouts proposed at the junction of Great Suffolk Street / Toulmin Street junction to reduce traffic speed and improve pedestrian accessibility.
- Kerbside parking bay layout to be modified with the removal of existing spaces in order to introduce the raised carriageway table at Toulmin Street and introduction of new, formal parking bays on the north side of Great Suffolk Street.

(See Appendix A – Initial Scheme Design)

1.3 Consultation Procedure

- 1.3.1 The views of the local community and those of statutory consultees have been sought, prior to the development of measures to a detailed design stage. Active community participation was encouraged through the use of a consultation document and questionnaire (see Appendix B – Consultation Documents).
- 1.3.2 The consultation document included a covering letter describing the proposals and a request for comments (including information to assist in translation and large print versions of the consultation document), preliminary design drawings (A3 size) and a questionnaire/comment form that could be sent to the Public Realm Projects Group with a pre-paid address reply.
- 1.3.3 The consultation document was delivered to a geographical area centred on the Borough High Street / Trinity Street junction and Great Suffolk Street between Borough High Street and Toulmin Street, using strategic roads and pedestrian desire lines as defined cut off points (See Appendix C – Location Plan and Extents of Consultation).
- 1.3.4 The distribution area was large enough to gain views from the wider community that may be considered to be affected by the proposed measures. A mailing list was established for the area by way of the Council's GIS database. In addition, the consultation documents and plans were supplied to the Council's established list of statutory consultees including London Buses, cycle groups and the Metropolitan Police. Please see Appendix D of list of addresses within the distribution area. Consultation documents were also delivered to the Southwark Housing Department and Grange Primary School.

- 1.3.5 The consultation documents were delivered by Royal Mail to 383 addresses detailed within the distribution list. The documents were delivered on the 5th August 2014, with a return deadline of the 5th September, allowing 4 weeks for the consultation period. However due to the summer holiday period, responses were accepted online until the 12th September 2014.
- 1.3.6 The proposals were also available to view online using consultation section of the council's website, with an e-form questionnaire provided in order to capture responses.

2.0 Consultation Responses

2.1 Response Rate and Distribution

- 2.1.1 A total of 34 responses were received during the consultation period (20 returned questionnaires and 14 online responses), equating to a 9% response rate.
- 2.1.2 Five responses were received from Statutory Consultees (Southwark Living Streets, Southwark Cyclists, Lambeth Cyclists, Road Haulage Association and Sustrans).

2.2 Questionnaire Analysis

- 2.2.1 The questionnaire element of the consultation document contained the following key questions and associated tick box options:

Q1. Are you a resident or business?

Q2. Do you support the proposals?

- 2.2.2 The following is a summary of replies received:

Question 1 - Are you a resident or business?

	Resident	Business
Replies	31	3
Total	91%	9%

Table 1: Returned questionnaire results for question 1

- 2.2.3 Table 1 indicates that the majority of responses received throughout the consultation period were from local residents, with only three businesses formally replying.

Question 2 – Do you support the proposals?

	Support	Opposed	No Opinion
Replies	20	11	3
Total	59%	32%	9%

Table 2: Returned questionnaire results for question 2

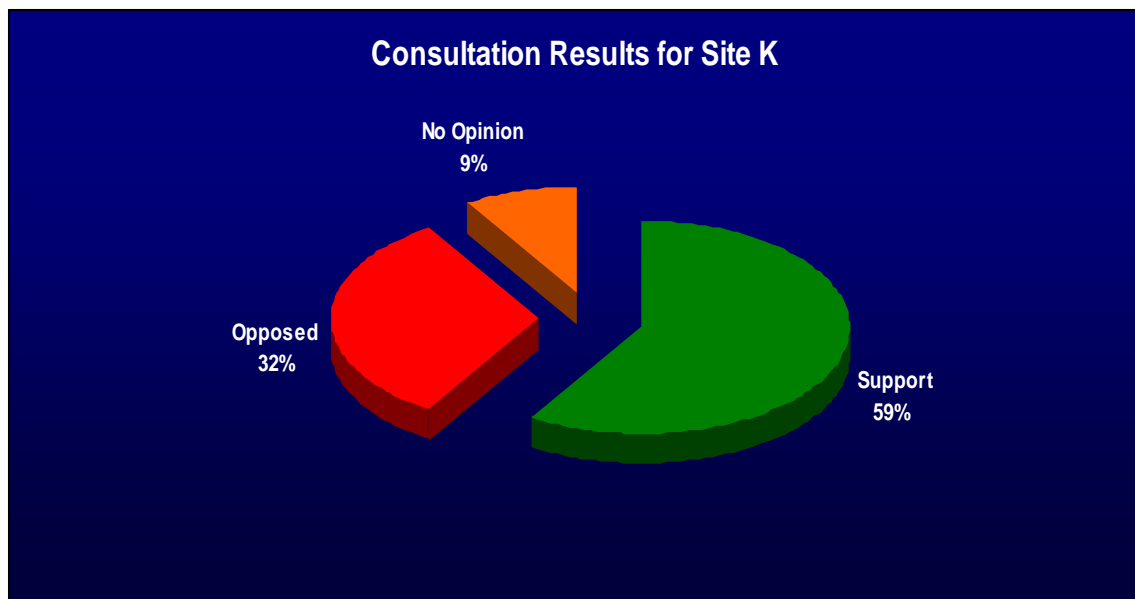


Figure 3: Graphical representation of consultation data for question 2

2.2.4 Table 2 and figure 2 indicate a majority of support for question 2, with 59% welcoming the proposed measures.

2.3 Additional Comments

2.3.1 The questionnaire element of the consultation document invited consultees to attach any additional comments they may have on the proposals when returning the reply-paid questionnaire. Consultees were also able to reply to the consultation online using the council's website.

2.3.2 The majority of respondents (59%) indicated support for the proposed measures, and discussed that Great Suffolk Street will look better and the works will benefit local residents and pedestrians.

2.3.3 A number of respondents commented that the improvements to the Borough High Street junction were great and pedestrian lights were badly needed at this location, as it is currently unsafe to cross the road.

- 2.3.4 A comment stated that the proposals are an excellent change in balance between vehicles, cyclists and pedestrians at the junction of Toulmin Street, which will be a lot safer and a more pleasant environment.
- 2.3.5 A respondent commented that the early start pre-signal sequence at the traffic lights is a good idea and the larger cycle waiting areas on top of the raised tables will help keep motorists back.
- 2.3.6 A comment was received stating that parking should be removed on Great Suffolk Street and put in side roads to help cyclists. *

* In response, parking provision is required in Great Suffolk Street to cater for both residents and businesses. Relocating the parking into the side roads is not a feasible option, as the carriageway width of the side roads are too narrow to accommodate kerbside parking. Other roads in the area already have kerbside parking bays as part of the local Controlled Parking Zone. Therefore the removal of parking in Great Suffolk Street cannot be offset locally, which would lead to vociferous objections from both residents and local businesses.

- 2.3.7 A suggestion was made to install more cycle parking in the area and highlighted that the redundant carriageway section on the north side of Great Suffolk Street that is being turned into footway would be a good option. *

* In response, the council will be identifying suitable locations along the Quietway route to install new footway cycle parking provision. There are a number of potential sites in Great Suffolk Street that have been identified, including the location highlighted by the respondent. In accordance with numerous studies concluding that on average, cyclists spend more money in local shops than other customers using alternative means of transport, the provision of additional cycle parking at the Great Suffolk Street Shopping Parade is an essential element to support local businesses, as well as helping to encourage short, local journeys to this location to be undertaken by bicycle.

- 2.3.8 A concern was raised that the commitment to plant new street trees is weak and there should be a firm agreement to plant trees in Great Suffolk Street. *

* In response, a key part of the Quietway ethos is to make routes more attractive to use for both cyclists and pedestrians. Street greening and tree planting are an integral initiative to meet this objective and the council hopes to install as many trees as practical along the Quietway route.

A number of proposed locations in Great Suffolk Street have been identified for potential street tree planting. However the locations are subject to further investigation to ascertain that the appropriate sub-surface formation can be achieved to ensure that newly planted trees will survive and flourish. If there are utility services directly under a proposed tree planting location, then it will not be feasible to plant the tree. This is primarily why the consultation plans refer to 'potential' tree planting locations.

- 2.3.9 A respondent recommended that all parking in the road should be removed and segregated cycle lanes installed. *

* In response, as outlined above, on-street parking is required to cater for local residents and businesses. As Great Suffolk Street has light traffic volumes and

no history of accidents involving cyclists between Borough High Street and Toulmin Street, there would be no justification to remove the parking to install segregated cycle lanes. Full segregating is discouraged on Quietway routes and should only be used on carriageways that have high vehicle volumes and speeds.

- 2.3.10 A comment was received stating that it would be preferred if the existing parking places remain and there is no need or benefit to build out the pavement. *

* In response, when reviewing the layout of the streetscape, it was evident that a number of existing parking bays on the southern side of Great Suffolk Street are located within the extents for the zig zag controlling lines of the zebra crossing at Toulmin Street. In accordance with statutory highway regulations, no parking can take place within the extents of zig zag prohibition markings. Therefore the bays are required to be removed. Leaving the bays in-situ renders them unenforceable and presents a potential safety issue with obstructing sightlines on approach to the zebra crossing location.

The northern side of the carriageway has suitable kerbside space to relocate the bays so that there is no net loss of parking.

Building out the footways adjacent to existing and proposed parking bays address potential conflict issues with cyclists riding adjacent to the kerblines than having to traverse out into the general traffic lane to cycle past the bays. The footway buildouts effectively inset the parking bays into the footway so that cyclists can take a consistent, linear line along the carriageway, taking appropriate positioning with general traffic flows. The footway buildouts also improve the streetscape by reducing the dominance of the carriageway and allowing opportunity to enhance the streetscape through the introduction of tree planting, cycle parking and seating, as well as increasing footway areas for pedestrians.

- 2.3.11 A comment was received that the designs were not ambitious enough and more segregation is required to make novice cyclists feel more comfortable about using the route. 'Looks like parking has been given priority over this'. *

* In response, as outlined above, segregation is not required along the Quietway routes and is not justified in Great Suffolk Street due to there being low traffic volumes and vehicle speeds, which are controlled using existing and proposed vertical deflection measures. Parking provision for local residents and businesses is essential along this section of the route and does not pose a threat to cyclists, as the footways are being extended to inset the parking bays (to match the successful scheme that was recently implemented in the eastern section of the street).

- 2.3.12 A request was made to resurface the section of Trinity Street between the Borough High Street junction and Trinity Church Square, as the road surface is poor quality. *

* In response, creating a smooth carriageway surface is essential to ensure a good ride quality for cyclists along the Quietway route. Officers will revisit this section of Trinity Street to assess the existing state of the carriageway and subject to funding, potentially include this work as part of the Site K proposals.

Trinity Church Square is due to be resurfaced in the current financial year, so officers will investigate if it is feasible that the proposed area to be resurfaced can be extended to include this short section of Trinity Street, with the works undertaken at the same time, which will minimise disruption to local residents and businesses.

2.3.13 Analysis of the additional comments from respondents that objected to the scheme highlighted the following concerns:

An objection was received stating that the changes to Great Suffolk Street over the last few years have adversely affected the parade of shops and that this could make things worse. *

* In response, the proposed measures will not have any adverse impact on local businesses or trade. There is no parking removal proposed adjacent to the shopping parade or traffic prohibition measures that will adversely affect access to Great Suffolk Street.

It can be argued that the improvements will potentially be beneficial to local businesses. Quietway routes will make cycling more attractive as an alternative mode of transport to undertake local journeys. As cycling numbers increase year on year, assisted by Quietway route implementation and with the provision of additional cycle parking in proximity to the shopping parade, the level of trade could potentially increase. Studies have concluded that people that cycle to local shopping destinations spend more money on average per visit than other shoppers that have arrived by foot or motor vehicle. It must also be noted that improvements to the streetscape will also make the street more attractive to pedestrians, which could increase existing levels of footfall and potential trade for local businesses.

A number of objections were received relating to taking away resident parking bays. 'It's hard enough to park in the area without taking away more spaces.' *

* In response, there is no net loss of parking associated with the scheme. All parking removed on the south side of Great Suffolk Street is being offset by introducing the same number of permanent holder bays on the north side of the carriageway.

A respondent objected to the scheme stating that cyclists have scant regard for pedestrians and complete contempt for pavement users. *

* In response, cyclists will be confined to the carriageway along Great Suffolk Street so there is minimal chance of conflict with pedestrians. Cyclists have to give way to pedestrians using controlled crossing facilities such as the zebra crossing a Toulmin Street and there are no sections of shared footway that would result in conflict.

A number of objections highlighted that Great Suffolk Street is a busy rat run and that if you were serious about cycling you would reduce traffic by either providing full segregation or removal of motor traffic through modal filtering. *

* In response, the proposed measures as part of this scheme will increase safety for cyclists by ensuring unobstructed access to signal junctions, advanced priority at signal junctions and curtailing traffic speeds through the introduction of further vertical deflection measures. As described earlier, the footways are also be built out which will not only improve the streetscape, but also remove the risk of cyclists conflicting with motor vehicles by moving out into the carriageway to traverse past parking bays.

The traffic volume using Great Suffolk Street is low compared to other roads in the area, which was demonstrated by traffic counts in morning and afternoon peak periods at the Borough High Street junction. The greatest risk to cyclists using this section of the route is potential conflict from motor vehicles turning across the path of cyclists traversing straight ahead at the Borough High Street junction. The traffic counts demonstrated that turning movements at this junction from both Great Suffolk Street and Trinity Street are minimal, with only 6 recorded over a 1 hour period from Trinity Street and 36 from Great Suffolk Street. Therefore the advanced cycle awaiting area in conjunction with early start pre-signal measures practically eliminate this risk, making the Great Suffolk Street routing of the Quietway route both attractive and safe for cyclists of all abilities.

Whilst closing Great Suffolk Street to through traffic would improve the Quietway route further, a comprehensive feasibility study would be required to ascertain the likelihood of traffic displacement onto other streets in the area. Due to the controversial nature of preventing through traffic and the potential access changes to residential and commercial properties, a separate consultation specifically on this proposal would be required. However, as outlined above, Great Suffolk Street traffic volumes and speeds are low and the risk of conflict with cyclists is minimal and therefore closing the road would not be justified. Such a proposal would also potentially lead to objections from local traders who would highlight that preventing through traffic would have a substantial impact on their trade.

A respondent objected stating that whilst the changes to the Borough High Street junction were good, there absolutely no changes to Great Suffolk Street that will benefit cyclists. *

* In response, there are numerous benefits for cyclists along Great Suffolk Street as part of the Quietway proposals for Site K. Traffic speeds will be reduced by introducing additional vertical deflection that will assist with enforcing the 20mph speed limit. As previously discussed, in-setting the parking bays by building out the footway will remove potential conflict by allowing cyclists to take a linear line along the carriageway without having to pull out around parking bays.

In addition the carriageway is being resurfaced between Borough High Street and Toulmin Street, which will greatly improve the ride quality for cyclists, as the current condition of the carriageway is poor.

An objection was received discussing that semi-segregated cycle lanes are useless and motorists will park in them. 'Either fully segregate or don't bother.' *

* In response, semi-segregated cycle lanes leading to advanced cycle waiting areas at signal junctions are being trialled as part of the Quietway proposals to assess their effectiveness. Installing armadillo lane delineators will make the cycle lane more prominent and should prevent encroachment of motor vehicles. The post monitoring of these measures will inform whether or not alternative designs should be considered. Armadillo delineators are also low cost and can be easily removed with minimal effort if they are deemed to be ineffectual in preventing vehicles from obstructing the cycle access lanes.

2.3.9 24% respondents did not submit a further comment.

2.4 Levels of Consensus

2.4.1 The following majority levels of agreement have been given in relation to the questions contained within the consultation document:

- 59% of respondents support the introduction of the Quietway cycle route proposals for Site I;
- 32% of respondents are opposed to the proposals; and
- 9% of respondents had no opinion.

2.5 Statutory Consultee Replies

2.5.1 Five statutory consultees provided a reply to the consultation.

- a) **The Road Haulage Association** replied in support and welcomed the retention of a section of single yellow line on the northern side of Great Suffolk Street for loading activities. However they noted that they were not in favour of the nearside cycle lanes on approach to junctions as cyclists will cut up the inside of traffic partially putting themselves in danger. *

* In response, the cycle feeder lanes leading to the advanced cycle waiting areas are proposed to ensure that cyclists can have unobstructed access the waiting area when the signals are red (which will be at least 70% of the time). The lanes will be semi-segregated which will reduce the likelihood of vehicle encroachment, allowing 1.5m of carriageway width for cyclists. When the signals are green, it is envisaged that cyclists will maintain their position on the middle of the general traffic lane and not use the cycle lane before entering the junction. In addition, as discussed previously, the number of vehicles turning left at this location is minimal and therefore the risk of left hook collisions with cyclists is minimal.

- b) **Southwark Living Streets** replied indicating strong support for the scheme and stated that the Borough High Street junction improvements were fantastic and the streetscape improvements to Great Suffolk Street will greatly improve the environment for not only cyclists but also for pedestrians.

- c) **Lambeth Cyclists** replied in support of the scheme and requested that the final design ensure the elephant footprint markings across Borough High Street line up with the centre of the lanes and not the nearside kerbs of the junction.
- d) **Southwark Cyclists** replied indicating support for the scheme. They highlighted traffic volume using both Trinity Street and Great Suffolk Street is low, with on-site observations concluding left turning traffic numbers are low, which coupled with that the proposed pre-signal and advanced cycle waiting area, will significantly reduce the left turn hook risk currently experienced by cyclists at the junction.
- e) **Sustrans** replied indicating support for the scheme but did not provide any specific questions or comments on the proposed measures.

2.5.2 No objections were received from Ward Members throughout the consultation period.

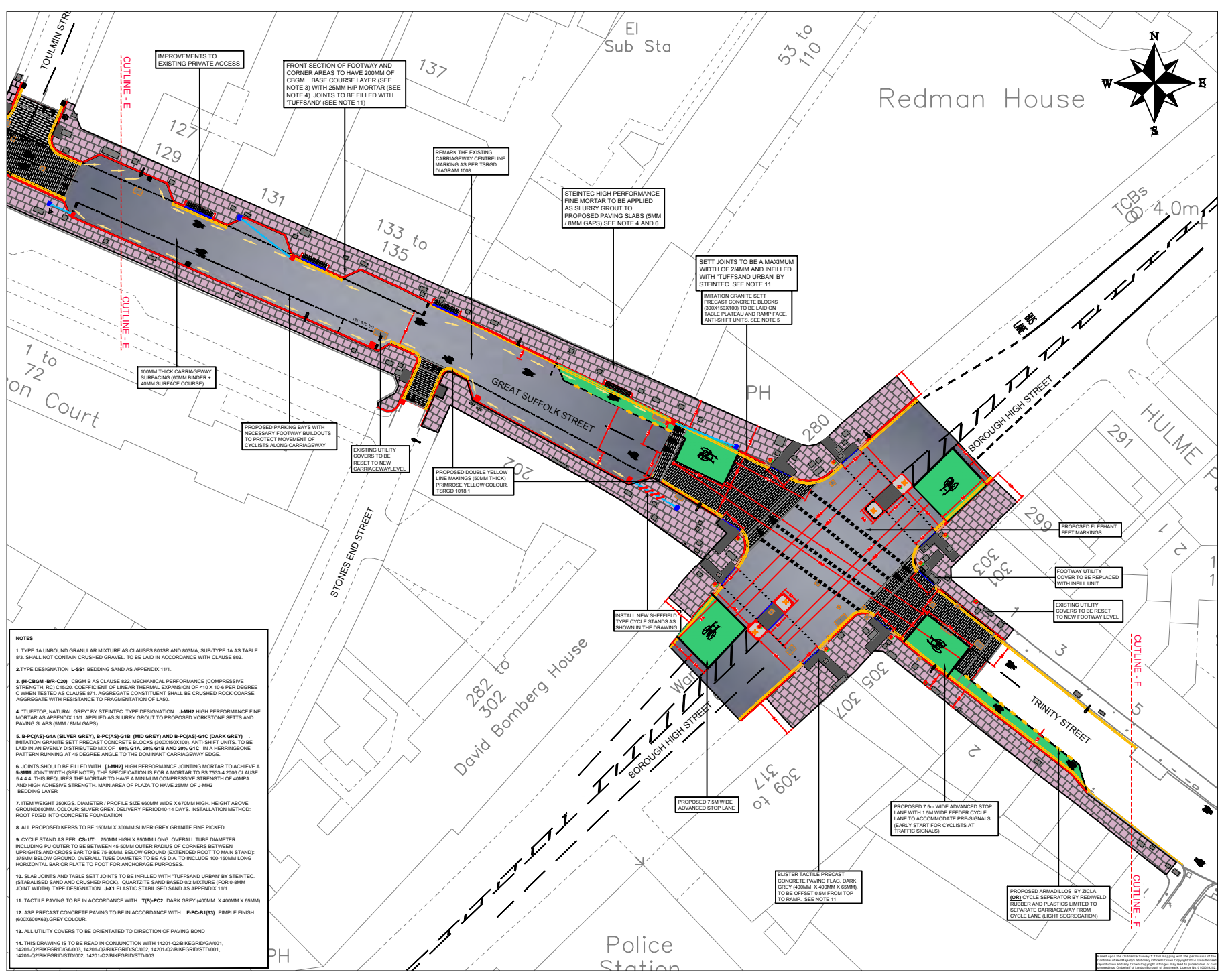
3.0 Recommendations

Due to the majority of respondents supporting the scheme and Southwark's on-going commitment to improve and promote cycling in the borough, it is recommended that the scheme proceed to implementation.

Appendices

Appendix A:	Initial Scheme Design
Appendix B:	Consultation Documents
Appendix C:	Location Plan and Extents of Consultation
Appendix D:	List of Addresses within the Distribution Area

Appendix A: Initial Scheme Design



LEGEND / NOTES.

- KEY:**
- CARRIAGEWAY RECONSTRUCTION, 40mm THICK TS15 PC21 PSV 68- LAY ON 50mm THICK SMA 1E EN 4995 OR WTR 2 TO CL307 OR CARRIAGEWAY REPAIR CONSTRUCTION.
 - FOOTWAY RECONSTRUCTION, PROVIDE AND LAY PCC PAVING FLAG 800 X 800 X 75mm FOR FOOTWAY CONSTRUCTION LAD ON 200mm THICK TYPE 1 SUBBASE AND 30mm THICK LOW FINES BEDDING SAND.
 - RAISED FEATURES, PROVIDE AND LAY ANTI-SHIFT PC BLOCKS 300 X 150 X 100mm FOR RAISE TABLE CONSTRUCTION LAD ON 200mm THICK C8/8M-C8 AND 30mm THICK LOW FINES BEDDING SAND.
 - SLURRY PAVING, PROVIDE AND LAY PCC SLURRY TACTILE PAVERS 400 X 400 X 75mm THICK (REF TB18/6/175) ON SPECIFIED FOOTWAY CONSTRUCTION LAD ON 200mm THICK C8/8M-C8 SUBBASE AND 30mm THICK TUFFBED LAYING MORTAR BY STEINTEC.
 - TUFFBED LAYING, SECTION OF FOOTWAY TO BE LAD ON 200mm THICK C8/8M-C8 SUBBASE AND 30mm THICK TUFFBED LAYING MORTAR BY STEINTEC.
 - CYCLE LANE, PROPOSED CYCLE LANE / ADVANCED STOP LINE (GREEN COLOUR IS FOR INDICATIVE PURPOSE ONLY).
 - EXISTING CYCLE SUPER HIGHWAY BY TRANSPORT FOR LONDON
 - ROADWORK, EXISTING IRONWORK COVER AND FRAME TO BE ADJUSTED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INSTALL FLAT KERB 150 X 300MM UNITS SILVER GREY.
 - STANDARD GRANITE KERBS, INSTALL NEW STANDARD GRANITE KERBS (200X100 X 300MM) LAD WITH 100MM UPSTAND ON ST4 INCLUDING FRONT KERB HALCHING TO BE 100MM BELOW CARRIAGEWAY. REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M(SHEET 1 OF 4) & TYPE R, TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
 - STANDARD GRANITE KERBS FLUSHED, INSTALL NEW STANDARD GRANITE KERBS (200X100 X 300MM) FLUSHED ON ST4 INCLUDING FRONT KERB HALCHING TO BE 100MM BELOW CARRIAGEWAY. REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M(SHEET 1 OF 4) & TYPE R, TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
 - STANDARD GRANITE TRANSITION KERBS, NEW STRAIGHT KERB TYPE T1S TRANSITION KERBS (200X100 X 300MM) TYPE K1S (SHEET 5 OF 4) REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M(SHEET 1 OF 4) & TYPE R, TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
 - STANDARD GRANITE TRANSITION KERBS, NEW STRAIGHT KERB TYPE T1S TRANSITION KERBS (200X100 X 300MM) TYPE K1S (SHEET 5 OF 4) REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M(SHEET 1 OF 4) & TYPE R, TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
 - NEW WHITE LINE ROAD MARKING.
 - NEW YELLOW LINE ROAD MARKING.
 - RED ROAD MARKINGS TO BE REINSTALLED.
 - NEW DRAIN PIPE, PROVIDE AND INSTALL SURFACE WATER DRAIN PIPE (GROUP 4 150mm INTERNAL DIAMETER) DEPTH OF INVERT=+20. AVERAGE DEPTH OF 700MM ON BED TYPE Z (DEPTH OF TROUGH ON BED TYPE Z).
 - NEW GULLY, PROVIDE AND INSTALL PRECAST CONCRETE TRAPPED GULLY WITH TYPE B COVER, GULLY DEPTH +0700MM OR BUILD CATCH PIT WHERE APPLICABLE.
 - EXISTING ROAD GULLY TO BE CONVERTED TO MANHOLE.
 - EXISTING ROAD GULLY TO BE RAISED AND ADJUSTED.
 - NEW CYCLE SEPARATOR OR ARMADILLOS.
 - NEW LAMP COLUMN LOCATION.
 - EXISTING LAMP COLUMN LOCATION.
 - NEW SHEFFIELD CYCLE STANDS.
 - NEW TREE PIT CONSTRUCTION (REFER TO STANDARD DRAWING DETAILS NO 14201-02/28/KEGR/STD/001).
 - EXISTING / PROPOSED LONGITUDINAL FALL ON CARRIAGEWAY.
 - EXISTING BELL BOLLARDS TO BE RELOCATED AS SHOWN IN THE DRAWING.
 - ACCESS IMPROVEMENTS, LIGHT GREY PCC BLOCKS B-PC10/8-81a (200 X 100 X 75) JOINTED WITH 24MM J-SS1 SHARP SAND ON 50MM THICK BSA A/C4 SUBBASE EN 12619/50. REFER TO STANDARD DRAWING DETAIL S NO 14201-02/28/KEGR/STD/002.
 - PROPOSED LOCATION FOR THE SIGN POSTS.

- NOTES**
1. TYPE 1A UNBOUND GRANULAR MIXTURE AS CLAUSES 801SR AND 803MA, SUB-TYPE 1A AS TABLE 803. SHALL NOT CONTAIN CRUSHED GRAVEL. TO BE LAD IN ACCORDANCE WITH CLAUSE 802.
 2. TYPE DESIGNATION L-SB1 BEDDING SAND AS APPENDIX 11/1.
 3. (M-C8M)-B/P-C29) C8M B AS CLAUSE B22. MECHANICAL PERFORMANCE (COMPRESSIVE STRENGTH, RD C15/20. COEFFICIENT OF LINEAR THERMAL EXPANSION OF +10 X 10-6 PER DEGREE C WHEN TESTED AS CLAUSE B71. AGGREGATE CONSTITUENT SHALL BE CRUSHED ROCK COARSE AGGREGATE WITH RESISTANCE TO FRAGMENTATION OF LA50.
 4. TUFFTOP, NATURAL GREY BY STEINTEC. TYPE DESIGNATION J-MH2 HIGH PERFORMANCE FINE MORTAR AS APPENDIX 11/1. APPLIED AS SLURRY GROUT TO PROPOSED YORKSTONE SETTS AND PAVING SLABS (5MM / 6MM GAPS).
 5. B-PC10/8-81a (SILVER GREY), B-PC10/8-81b (MID GREY) AND B-PC10/8-81c (DARK GREY) IMITATION GRANITE SETT PRECAST CONCRETE BLOCKS (300X100X100). ANTI-SHIFT UNITS. TO BE LAD IN AN EVENLY DISTRIBUTED MIX OF 60% G1A, 20% G1B AND 20% G1C. IN A HERRINGBONE PATTERN RUNNING AT 45 DEGREE ANGLE TO THE DOMINANT CARRIAGEWAY EDGE.
 6. JOINTS SHOULD BE FILLED WITH J-MH2 HIGH PERFORMANCE JOINTING MORTAR TO ACHIEVE A 5.444 JOINT WIDTH (SEE NOTE). THE SPECIFICATION IS FOR A MORTAR TO BS 7533-2:2008 CLAUSE 5.4.4.4. THIS REQUIRES THE MORTAR TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 40MPa AND HIGH ADHESIVE STRENGTH. MAIN AREA OF PLAZA TO HAVE 25MM OF J-MH2 BEDDING LAYER.
 7. ITEM WEIGHT 300KG. DIAMETER / PROFILE SIZE 600MM WIDE X 670MM HIGH. HEIGHT ABOVE GROUND/0.00M. COLOUR: SILVER GREY. DELIVERY PERIOD 10-14 DAYS. INSTALLATION METHOD: ROOT FIXED INTO CONCRETE FOUNDATION.
 8. ALL PROPOSED KERBS TO BE 150MM X 300MM SILVER GREY GRANITE FINE PICKED.
 9. CYCLE STAND AS PER CS-1/7. 750MM HIGH X 800MM LONG. OVERALL TUBE DIAMETER INCLUDING PU OUTER TO BE BETWEEN 45-50MM OUTER RADIUS OF CORNERS BETWEEN UPRIGHTS AND CROSS BAR TO BE 75.00MM. BELOW GROUND (EXTENDED ROOT TO MAIN STAND); 375MM BELOW GROUND. OVERALL TUBE DIAMETER TO BE AS D.A. TO INCLUDE 100-150MM LONG HORIZONTAL BAR OR PLATE TO FOOT FOR ANCHORAGE PURPOSES.
 10. SLAB JOINTS AND TABLE SETT JOINTS TO BE INFILLED WITH TUFFSAND URBAN BY STEINTEC. (STABILISED SAND AND CRUSHED ROCK). QUARTZITE SAND BASED 0/2 MIXTURE (FOR 0-8MM JOINT WIDTH). TYPE DESIGNATION J-MH2. ELASTIC STABILISED SAND AS APPENDIX 11/1.
 11. TACTILE PAVING TO BE IN ACCORDANCE WITH T10-PC2 DARK GREY (400MM X 400MM X 65MM).
 12. ASP PRECAST CONCRETE PAVING TO BE IN ACCORDANCE WITH F-PC-81(83). PIMPLE FINISH (0/20X0/20) GREY COLOUR.
 13. ALL UTILITY COVERS TO BE ORIENTATED TO DIRECTION OF PAVING BOND.
 14. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH 14201-02/28/KEGR/GA/001, 14201-02/28/KEGR/GA/003, 14201-02/28/KEGR/STD/002, 14201-02/28/KEGR/STD/001, 14201-02/28/KEGR/STD/002, 14201-02/28/KEGR/STD/003

Project		CENTRAL LONDON BIKE GRID Q2 - BLUE ROUTE	
Title		GENERAL ARRANGEMENT SECTION-6 BOROUGH HIGH STREET JUNCTION	
Contract No.	HW2013 NEC3	Drawn	CRH
Scale	1 : 250 @ A1	Purpose	CONSTRUCTION
Drawing No.	14201-Q2/BIKEGRID/GA/006	Checked	CRH
Date Drawn	JUNE 2014	Date Issued	JULY 2014

Appendix B: Consultation Documents

- New carriageway table with footway buildouts proposed at the junction of Great Suffolk Street / Toulmin Street junction to reduce traffic speed and improve pedestrian accessibility.
- Kerbside parking bay layout to be modified with the removal of existing spaces in order to introduce the raised carriageway table at Toulmin Street and introduction of new, formal parking bays on the north side of Great Suffolk Street.



Quietway
Greenwich to Waterloo

We want your views

It is important for all consultees to respond to the consultation. We would be grateful if you could take the time to review the proposals outlined in this document and provide a response using the pre-paid envelope and questionnaire provided by **Friday 5th September 2014**

Alternatively, you can view the proposals at www.southwark.gov.uk/consultations and complete the online questionnaire.

Site K – Borough High Street Junction and Great Suffolk Street

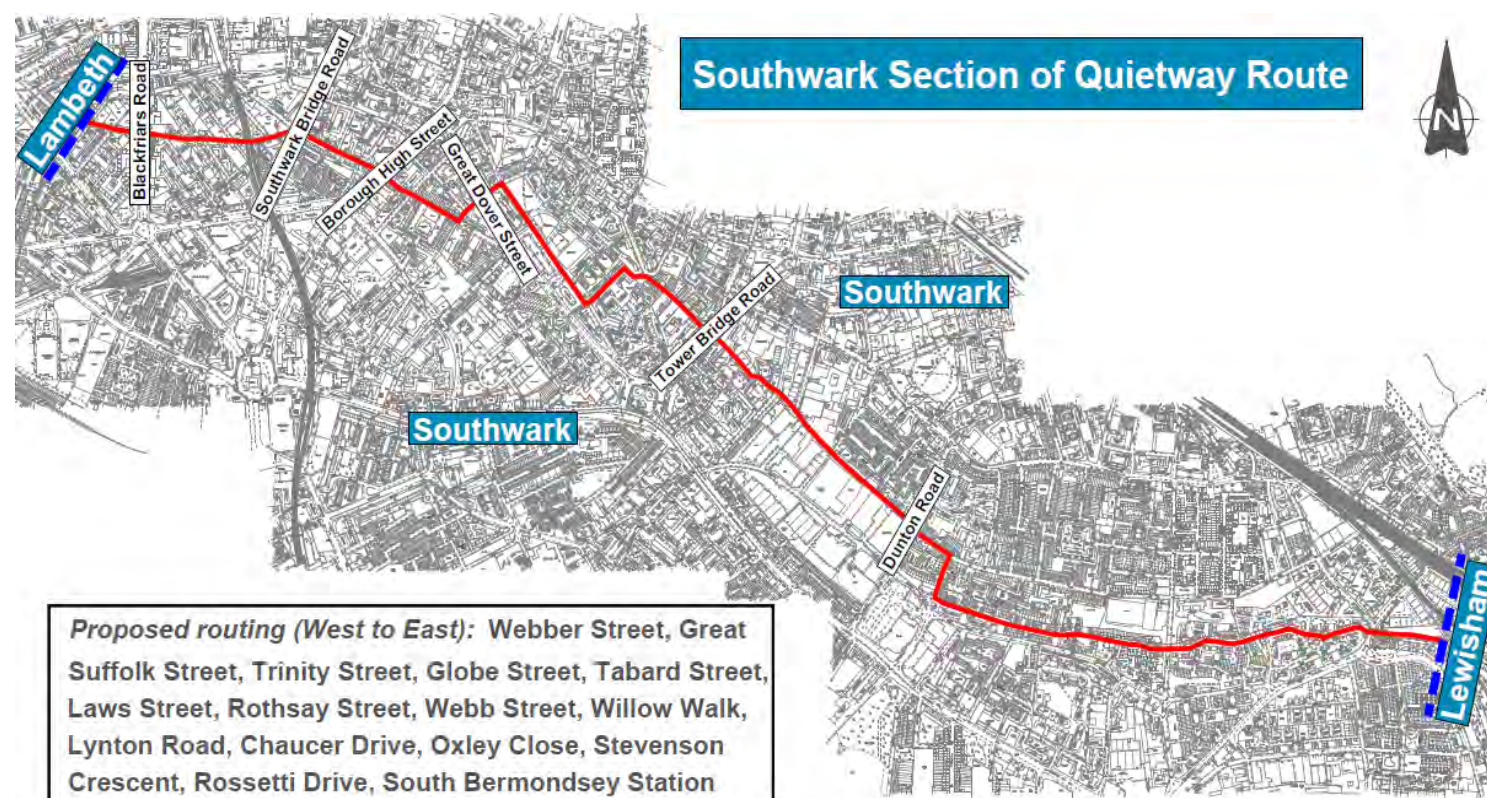
Pedestrian safety, Cycle Priority and Streetscape Improvements

What happens next?

The responses to the questionnaire will be analysed and taken into account in the final design of the proposed works. As you will appreciate Southwark Council receives many comments from consultations and therefore are unable to respond personally to specific issues raised. However all comments and suggestions will be taken into consideration before a decision is made. The consultation results and recommendations will be presented at a local community council meeting in September 2014.

Should you require any further information regarding the proposed scheme please do not hesitate to contact Chris Mascord at chris.mascord@southwark.gov.uk

Further information on other schemes along the route in Southwark can also be found at: www.southwark.gov.uk/consultations



Have your say

Southwark Council is holding a consultation to receive residents' and key stakeholders' comments regarding proposals relating to the borough's section of the Quietway (Greenwich to Waterloo) cycle route. This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey station in the east and traversing local roads to The Cut in the west. The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

Background

Quietways are a network of bike routes for less confident cyclists using low-traffic back streets. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.

What are the proposed changes?

- Borough High Street junction to be significantly upgraded including an 'all green' pedestrian phase to improve safety, with automated countdown system so pedestrians know how much time they have left to cross the road.
- Central pedestrian carriageway islands to be enlarged to provide a safer environment for pedestrian to wait to cross the road.
- Subject to approval from the DfT and other governing bodies, the Great Suffolk Street and Trinity Street arms will have an early start cycle pre-signal so cyclists can traverse the junction and undertake turning movements before general traffic.
- Existing raised carriageway tables in Trinity Street and Great Suffolk Street are to be enlarged to provide cycle waiting areas at the signals. Both approaches will have semi segregated cycle lanes so that cyclists can have unobstructed access to the waiting areas.
- Great Suffolk Street to be resurfaced between Toulmin Street to and Borough High Street to be resurfaced to improve the ride quality for cyclists.
- Footways of Great Suffolk Street between Toulmin Street to be built out to inset the parking bays and narrow carriageway to reduce vehicle speeds and improve the quality of the streetscape.
- Footways on both sides repaved in new materials, with tree planting where possible to improve the streetscape.



Redundant section of carriageway to be converted to footway to improve the streetscape

Proposed raised carriageway table at the junction of Great Suffolk Street and Toulmin Street (incorporating the existing zebra crossing facility), to reduce traffic speeds and improve pedestrian accessibility

New permit holder parking bays to be introduced on the northern side of Great Suffolk Street. Existing vehicle accesses into adjacent businesses retained

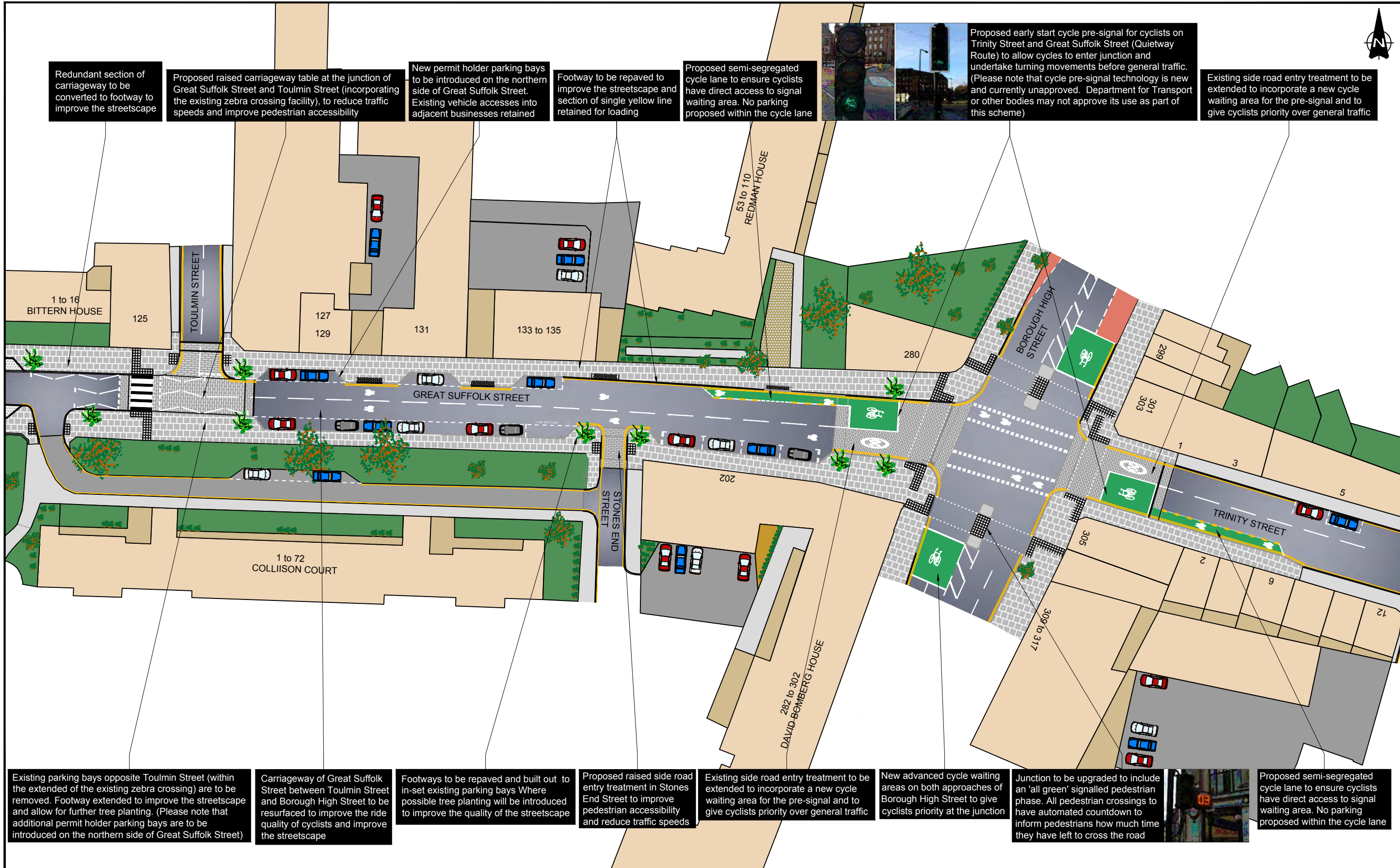
Footway to be repaved to improve the streetscape and section of single yellow line retained for loading

Proposed semi-segregated cycle lane to ensure cyclists have direct access to signal waiting area. No parking proposed within the cycle lane



Proposed early start cycle pre-signal for cyclists on Trinity Street and Great Suffolk Street (Quietway Route) to allow cycles to enter junction and undertake turning movements before general traffic. (Please note that cycle pre-signal technology is new and currently unapproved. Department for Transport or other bodies may not approve its use as part of this scheme)

Existing side road entry treatment to be extended to incorporate a new cycle waiting area for the pre-signal and to give cyclists priority over general traffic



Existing parking bays opposite Toulmin Street (within the extended of the existing zebra crossing) are to be removed. Footway extended to improve the streetscape and allow for further tree planting. (Please note that additional permit holder parking bays are to be introduced on the northern side of Great Suffolk Street)

Carriageway of Great Suffolk Street between Toulmin Street and Borough High Street to be resurfaced to improve the ride quality of cyclists and improve the streetscape

Footways to be repaved and built out to in-set existing parking bays Where possible tree planting will be introduced to improve the quality of the streetscape

Proposed raised side road entry treatment in Stones End Street to improve pedestrian accessibility and reduce traffic speeds

Existing side road entry treatment to be extended to incorporate a new cycle waiting area for the pre-signal and to give cyclists priority over general traffic

New advanced cycle waiting areas on both approaches of Borough High Street to give cyclists priority at the junction

Junction to be upgraded to include an 'all green' signalled pedestrian phase. All pedestrian crossings to have automated countdown to inform pedestrians how much time they have left to cross the road



Proposed semi-segregated cycle lane to ensure cyclists have direct access to signal waiting area. No parking proposed within the cycle lane



SITE K - GREAT SUFFOLK STREET / TRINITY STREET JUNCTION, STREETScape AND CYCLE PRIORITY IMPROVEMENTS



Appendix C: Location Plan and Extents of Consultation

Quietway Site K - Consultation Area

Date 30/7/2014



Appendix D: List of Addresses within Distribution Area

ORGANISATION	SAO	PAO	STREET	TOWN	POSTCODE	
SUCCESS ASSOCIATES LTD	FLAT 4	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH	
	FLAT 3	RUSE, 280	BOROUGH HIGH STREET	LONDON	SE1 1JS	
ST VINCENT DE PAUL SOCIETY	FIFTH FLOOR	8A	199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
	FLAT B	291-299	TRINITY STREET	LONDON	SE1 1DB	
	FLAT C		BOROUGH HIGH STREET	LONDON	SE1 1JG	
	FLAT 66		307 BOROUGH HIGH STREET	LONDON	SE1 1JH	
	FLAT 70		307 BOROUGH HIGH STREET	LONDON	SE1 1JH	
	FLAT 109	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	FLAT 56	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	UNIT A	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	UNIT D SECOND FLOOR	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
	FLAT B7A	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
CONNECTING LONDON LTD UNITED VISUAL ARTISTS LTD	FLAT C2A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT C4B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT 2	199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED		
	FLAT B5A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT A4A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT B1	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT B3B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT D1	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT D5	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT E6	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT 14	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
	FLAT 19	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
	FLAT 23	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
	FLAT 12	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH	
	FLAT 16	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH	
	FLAT 7	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH	
	FLAT 2	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
		172A	GREAT SUFFOLK STREET	LONDON	SE1 1PE	
	FLAT 49	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FLAT 55	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FAIRBRIDGE LTD COBELLI'S			5 TRINITY STREET	LONDON	SE1 1DB
				299 BOROUGH HIGH STREET	LONDON	SE1 1JG
		FLAT 26	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
		FLAT 83	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	COLIN G BANFIELD & LESLIE J STEVENS TERRY CAFE		DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
				8 TRINITY STREET	LONDON	SE1 1DB
				158 GREAT SUFFOLK STREET	LONDON	SE1 1PE
				2 HULME PLACE	LONDON	SE1 1HX
				24 SUDREY STREET	LONDON	SE1 1PF
				8 TRINITY STREET	LONDON	SE1 1DB
THE CORPORATION OF TRINITY HOUSE	THIRD FLOOR FLAT					
	FLAT 8	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY	
		14-16	TRINITY STREET	LONDON	SE1 1DB	
	GROUND FLOOR	170A	GREAT SUFFOLK STREET	LONDON	SE1 1PE	
SMITH YEATMAN LTD		176A	GREAT SUFFOLK STREET	LONDON	SE1 1PE	
			103 GREAT SUFFOLK STREET	LONDON	SE1 1PQ	
	FLAT 11	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY	
	FLAT 10		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY	
	FLAT 71	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FLAT 4	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
	FLAT 11	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH	
	FLAT 1		18 TRINITY STREET	LONDON	SE1 1DB	
	FLAT 5		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY	
	FLAT 10	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
THE MINISTRY OF FUN LTD CO NO: 032323211	FLAT 14	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
	UNIT 4	SUFFOLK HOUSE, 127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
	FLAT 76	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	FLAT 80	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	FLAT 86	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	FLAT 90	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	FLAT 95	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
	FLAT 3	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY	
	FLAT 2		305 BOROUGH HIGH STREET	LONDON	SE1 1JH	
	FLAT 3		18 TRINITY STREET	LONDON	SE1 1DB	
	FLAT 35	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FLAT 39	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FLAT 44	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FIRST FLOOR	133-135	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
	FLAT 7	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
	FLAT 12	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
	FLAT 54	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FLAT 59	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	FLAT 64	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA	
	PRISONERS FAMILY & FRIENDS SERVICE TRUSTEES A DUNLOP A MILLB			50 SWAN STREET	LONDON	SE1 1DF
FLAT 19		COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
FLAT 23		COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
FLAT 28		COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
FLAT 32		COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
FLAT 7		COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ	
FLAT 102		REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW	
			107 GREAT SUFFOLK STREET	LONDON	SE1 1PQ	
			305 BOROUGH HIGH STREET	LONDON	SE1 1JH	
		DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
NIKI NEWS	FLAT 4	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT D2	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT C3B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT C1B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT B6A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	FLAT B2A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ	
	REAR OF	156A-176B	GREAT SUFFOLK STREET	LONDON	SE1 1PE	
	UNIT 1	SUFFOLK HOUSE, 127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
	UNIT 7	SUFFOLK HOUSE, 127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
	UNIT 3	SUFFOLK HOUSE, 127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP	
CORPORATION OF LONDON COMMUNITY SERVICES DEPARTMENT THE MINISTRY OF FUN LTD JOHN KEIL INVESTMENTS LTD SIMON VINALL PHOTOGRAPHY LIMITED	UNIT 2	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	
	LIVING ACCOMMODATION		125 GREAT SUFFOLK STREET	LONDON	SE1 1PQ	
	FLAT 4		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
	FLAT 5		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
	FLAT 9	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
	UNIT 3A	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ	

	UNIT 4A	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 4B	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 5A	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 2A	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	SECOND FLOOR FLAT		4 TRINITY STREET	LONDON	SE1 1DB
	FLAT 1		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT A5A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT A2B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
DRYCLEANERS		12A	TRINITY STREET	LONDON	SE1 1DB
		297	BOROUGH HIGH STREET	LONDON	SE1 1JG
		156B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		158B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		125	GREAT SUFFOLK STREET	LONDON	SE1 1PQ
		156A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		172B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		176B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
PRISONERS FAMILIES & FRIENDS SERVICE		20	TRINITY STREET	LONDON	SE1 1DB
RAE & CO SOLICITORS		2	TRINITY STREET	LONDON	SE1 1DB
G WORRAL & SON LTD		174	GREAT SUFFOLK STREET	LONDON	SE1 1PE
THE SALON,		160	GREAT SUFFOLK STREET	LONDON	SE1 1PE
BELLA PIZZA C-T		6	TRINITY STREET	LONDON	SE1 1DB
RIPPING IMAGE LTD		131	GREAT SUFFOLK STREET	LONDON	SE1 1PP
			GREAT SUFFOLK STREET	LONDON	SE1 1PE
		162A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		162B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		164-166	GREAT SUFFOLK STREET	LONDON	SE1 1PE
DENISE FLORIST		168	GREAT SUFFOLK STREET	LONDON	SE1 1PE
			GREAT SUFFOLK STREET	LONDON	SE1 1PE
THE SOL FACTORY LTD		170B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
			GREAT SUFFOLK STREET	LONDON	SE1 1PE
		172	GREAT SUFFOLK STREET	LONDON	SE1 1PE
			GREAT SUFFOLK STREET	LONDON	SE1 1PE
		174A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		174B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
ERIC'S GREEN GROCONS		176	GREAT SUFFOLK STREET	LONDON	SE1 1PE
SCANPLUS LTD		133-135	GREAT SUFFOLK STREET	LONDON	SE1 1PP
OLLEY'S FISH & CHIPS		101	GREAT SUFFOLK STREET	LONDON	SE1 1PQ
GIGGLING-SAUSAGE		105	GREAT SUFFOLK STREET	LONDON	SE1 1PQ
IL FURNAR		109	GREAT SUFFOLK STREET	LONDON	SE1 1PQ
ALFA SUPPORT SERVICES		302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
		12	TRINITY STREET	LONDON	SE1 1DB
		1-3	TRINITY STREET	LONDON	SE1 1DB
		21	SUDREY STREET	LONDON	SE1 1PF
		25	SUDREY STREET	LONDON	SE1 1PF
		26	SUDREY STREET	LONDON	SE1 1PF
		27	SUDREY STREET	LONDON	SE1 1PF
		28	SUDREY STREET	LONDON	SE1 1PF
		160A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		160B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		301-303	BOROUGH HIGH STREET	LONDON	SE1 1JH
ARROW THERAPEUTICS LTD		BRITANNIA HOUSE, 7	TRINITY STREET	LONDON	SE1 1DB
		10	TRINITY STREET	LONDON	SE1 1DB
		307	BOROUGH HIGH STREET	LONDON	SE1 1JH
		1	HULME PLACE	LONDON	SE1 1HX
		158A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		2	TRINITY STREET	LONDON	SE1 1DB
		2	TRINITY STREET	LONDON	SE1 1DB
	FIRST FLOOR FLAT				
	SECOND FLOOR AND THIRD FLOOR FLAT				
	FLAT A3B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT E3	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT E4	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT E5	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B5B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B6B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B7B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C1A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C2B4	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C3A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C4A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C5A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C5B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
		162	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		170	GREAT SUFFOLK STREET	LONDON	SE1 1PE
TOTE BOOKMAKERS		12B	TRINITY STREET	LONDON	SE1 1DB
	FLAT C6A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT C6B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
COURT SERVICE AGENCY		CROWN COURT	SWAN STREET	LONDON	SE1 1DF
	FLAT A2A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT A3A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT 6	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 18	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 20	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 21	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 1	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 3	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 4	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 5	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 22	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 24	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 25	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 1	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	GROUND FLOOR AND FIRST FLOOR FLAT		197 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
	SECOND FLOOR AND THIRD FLOOR FLAT		197 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
	UNIT 3	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 4	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 5	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 8	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 9	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 10	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 1		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
	FIRST FLOOR FLAT		4 TRINITY STREET	LONDON	SE1 1DB
	FOURTH FLOOR	291-299	BOROUGH HIGH STREET	LONDON	SE1 1JG
	FLAT D3	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT D4	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	UNIT 1A	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 6		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED

PATEL DODHIA & CO.	BASEMENT AND GROUND FLOOR		4 TRINITY STREET	LONDON	SE1 1DB
	FLAT E2	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
UNITED VISUAL ARTISTS LTD	UNIT B	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
LEES ASSOCIATES LTD	UNIT C	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
	FLAT 12		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 1		204 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 2		204 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 100	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 101	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 92	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 93	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 94	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 96	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 97	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 98	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 99	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 62	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 7		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 8		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 9		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 11		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 2		18 TRINITY STREET	LONDON	SE1 1DB
	FLAT 59	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 60	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 61	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 53	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 56	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 57	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 58	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 60	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 61	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 62	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 63	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 65	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 66	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 67	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 68	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 69	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 71	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 73	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 74	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 75	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 77	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 78	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 79	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 81	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 82	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 84	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 85	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 87	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 88	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 89	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 91	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 4		18 TRINITY STREET	LONDON	SE1 1DB
	FLAT 7	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	FLAT 8	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	FLAT 63	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 64	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 65	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 67	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 13		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 1	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 13	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 38	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 40	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 41	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 42	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 43	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 45	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 46	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 3		305 BOROUGH HIGH STREET	LONDON	SE1 1JH
	FLAT A5B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT A6A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT A6B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B2B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B3A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B4A	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
	FLAT B4B	DAVID BOMBERG HOUSE, 282-302	BOROUGH HIGH STREET	LONDON	SE1 1JJ
LEE FITZGERALD ARCHITECTS LTD	BASEMENT AND GROUND FLOOR		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
	FLAT 105	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 106	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 107	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 108	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 110	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 53	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 54	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 55	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 57	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 58	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 14	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 15	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 2	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 13	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 15	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 16	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 17	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT 3	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 5	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 6	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 8	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 9	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH

	FLAT 68	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 69	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 70	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 7	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
	FLAT 9	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
	FLAT 12	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
	FLAT 72	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 1	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 10	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 20	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 21	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 22	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 24	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 25	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 27	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 11	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 12	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 13	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 15	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 16	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 17	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 18	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 2	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 29	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 3	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 30	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 31	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 4	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 5	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 6	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 8	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 9	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1NZ
	FLAT 33	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 34	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 36	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 37	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
ATELIER KONINCK QBFZ LTD	FLAT 1		305 BOROUGH HIGH STREET	LONDON	SE1 1JH
	UNIT 8	SUFFOLK HOUSE, 127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
	FLAT 1	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
	FLAT 6	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
ABERDEEN SEA PRODUCTS LTD	FLAT 72	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	UNIT 2	SUFFOLK HOUSE, 127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
	FLAT 104	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 103	REDMAN HOUSE	LANT STREET	LONDON	SE1 1QW
	FLAT 50	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 51	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 52	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 47	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 48	COLLINSON COURT	GREAT SUFFOLK STREET	LONDON	SE1 1PA
	FLAT 3		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 4		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 6		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 2		202 GREAT SUFFOLK STREET	LONDON	SE1 1NY
	FLAT 1	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	FLAT 2	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	FLAT 3	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	FLAT 5	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	FLAT 6	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
RED LETTER DESIGN	THIRD FLOOR	133-135	GREAT SUFFOLK STREET	LONDON	SE1 1PP
	FLAT 4	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
	FLAT 2		6 TRINITY STREET	LONDON	SE1 1DB
WILLIAM HILL	GROUND FLOOR	293-295	BOROUGH HIGH STREET	LONDON	SE1 1JG
DR FARYDON MIRZAI	BASEMENT AND GROUND FLOOR	305-307	BOROUGH HIGH STREET	LONDON	SE1 1JH
	FLAT 3		6 TRINITY STREET	LONDON	SE1 1DB
	FLAT 5	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
	FLAT 1		6 TRINITY STREET	LONDON	SE1 1DB
	FLAT 10	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
THE CORPORATION OF TRINITY HOUSE	FIRST FLOOR AND SECOND FLOOR	14-16	TRINITY STREET	LONDON	SE1 1DB
	SECOND FLOOR FLAT		8 TRINITY STREET	LONDON	SE1 1DB
	FLAT 2	ST MICHAELS COURT, 3	HULME PLACE	LONDON	SE1 1HY
MERCATOR LONDON LTD IN LIQUIDATION	UNIT 5	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
MERCATOR LONDON LTD	UNIT 6	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
ARC ELECTRICAL	UNIT 9	127-129	GREAT SUFFOLK STREET	LONDON	SE1 1PP
	FLAT A		307 BOROUGH HIGH STREET	LONDON	SE1 1JH
	LIVING ACCOMMODATION	RUSE, 280	BOROUGH HIGH STREET	LONDON	SE1 1JS
	FLAT 11	SIGNAL HOUSE, 137	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	UNIT 1B	SIGNAL HOUSE, 137A	GREAT SUFFOLK STREET	LONDON	SE1 1PZ
	FLAT D		307 BOROUGH HIGH STREET	LONDON	SE1 1JH
			156 GREAT SUFFOLK STREET	LONDON	SE1 1PE
			282 BOROUGH HIGH STREET	LONDON	SE1 1JJ
			304 BOROUGH HIGH STREET	LONDON	SE1 1JJ

London Borough of Southwark



Quietway Cycling Proposals

Site L –Great Suffolk Street / Southwark Bridge Road Junction

Public Consultation Summary

September 2014

London Borough of Southwark

Site L - Quietway Cycling Proposals Great Suffolk Street / Southwark Bridge Road Junction

Public Consultation Summary

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1.0 Introduction

1.1 Background

1.1.1 This document report has been produced by the London Borough of Southwark Public Realm Projects Group, to provide a summary of the consultation exercise for the Quietway Cycling Proposals for Site L at the Great Suffolk Street / Southwark Bridge Road junction. The measures are being drafted by the Public Realm Projects Team, with the project manager for this scheme being Chris Mascord (Senior Engineer).

1.1.2 The area under consideration is located within the SE1 district of Southwark (Cathedrals Ward) in the north of the borough. See figure 1 below.

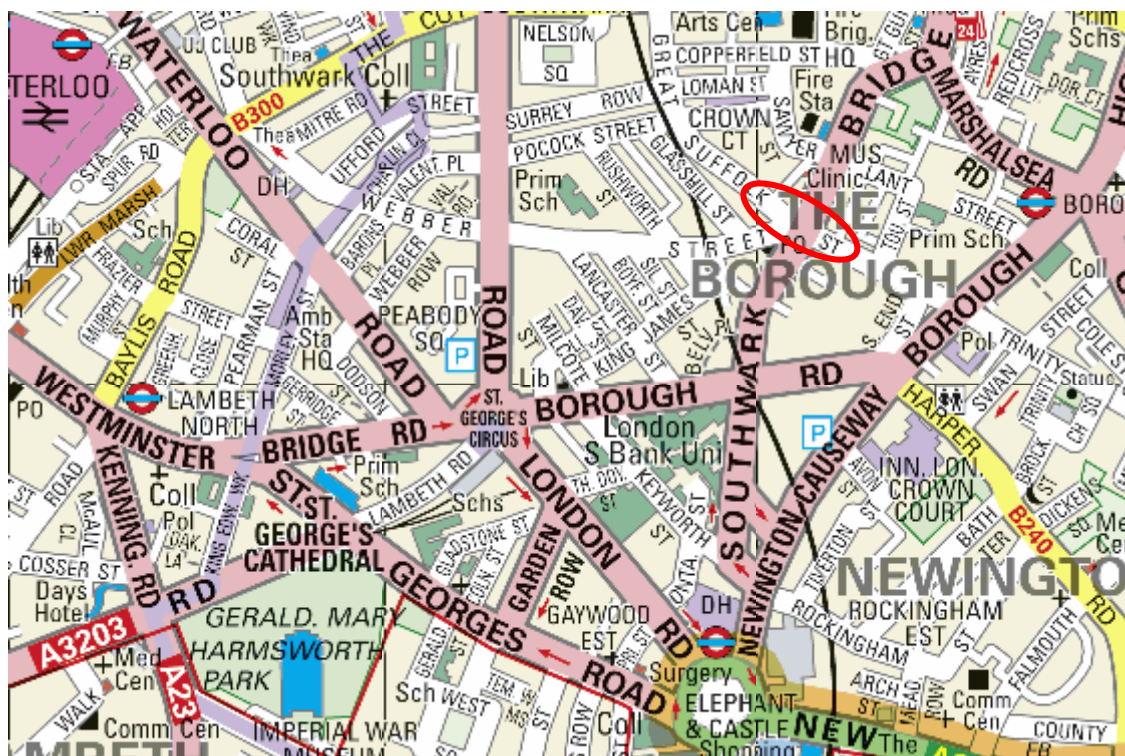


Figure 1: Location of proposed scheme

1.2 Project and Objectives

1.2.1 This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey Station in the east and traversing local roads to The Cut in the west (see figure 2). The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

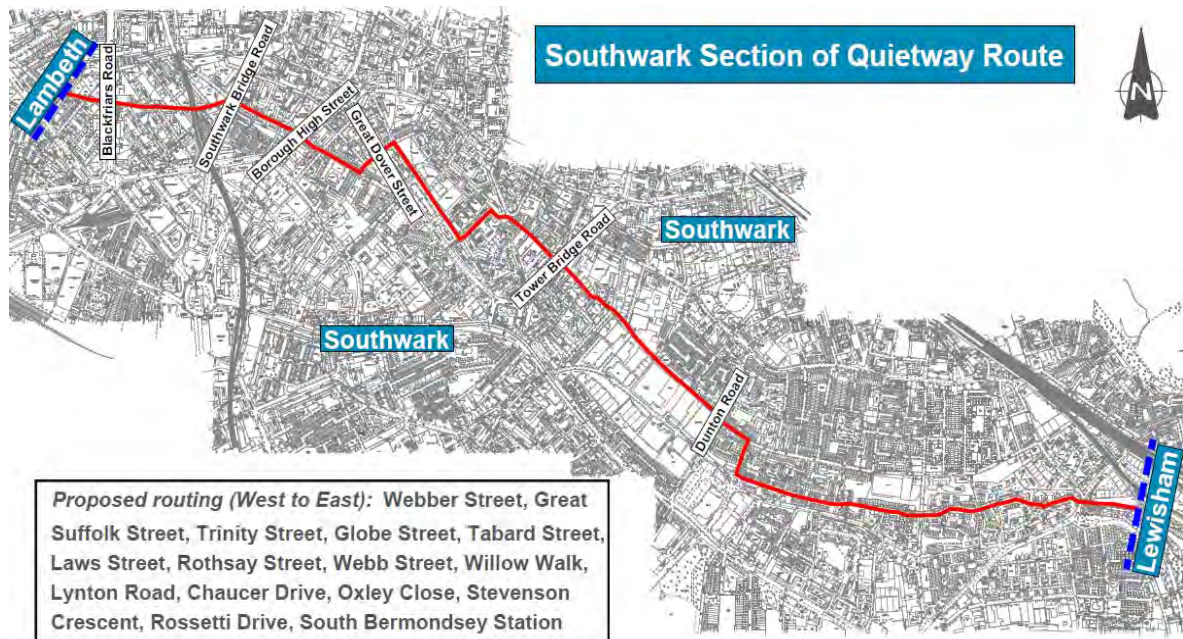


Figure 2: Southwark's section of Quietway Route

- 1.2.2 Quietways are a network of bike routes for less confident cyclists using residential streets with low traffic volumes. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.
- 1.2.3 The measures proposed in this consultation are part of the Council's ongoing commitment to make Southwark's streets safer and more accessible for all. The proposed measures will enhance the environment for all road users, reducing traffic speeds and improving pedestrian safety. Cycling proposals also have the added health benefits of improving the environment through reducing carbon emissions and getting more people onto bikes which in turn enhances their fitness and health.
- 1.2.4 The following measures were consulted upon to improve cycle safety and accessibility for Site L:
- Subject to approval from the DfT and other governing bodies, the Great Suffolk Street arms of the Southwark Bridge Road junction will have an early start cycle pre-signal so cyclists can traverse the junction and undertake turning movements before general traffic.
 - Existing raised carriageway table on Great Suffolk Street on the eastern side of the junction is to be extended to provide a cycle waiting area, ahead of general traffic at the signals.

- The western arm of the junction will have semi segregated cycle lane so that cyclists can have unobstructed access to the waiting areas on approach to Southwark Bridge Road.
- Keep Clear marking proposed on Great Suffolk Street adjacent to Webber Street to allow cyclists exiting Webber Street to access the cycle lane and advanced waiting area on approach to Southwark Bridge Road.
- The carriageway of Webber Street is to be resurfaced and existing sets of speed cushions to be replaced with sinusoidal humps to improve the ride quality for cyclists.

(See Appendix A – Initial Scheme Design)

1.3 Consultation Procedure

- 1.3.1 The views of the local community and those of statutory consultees have been sought, prior to the development of measures to a detailed design stage. Active community participation was encouraged through the use of a consultation document and questionnaire (see Appendix B – Consultation Documents).
- 1.3.2 The consultation document included a covering letter describing the proposals and a request for comments (including information to assist in translation and large print versions of the consultation document), preliminary design drawings (A3 size) and a questionnaire/comment form that could be sent to the Public Realm Projects Group with a pre-paid address reply.
- 1.3.3 The consultation document was delivered to a geographical area centred on the Southwark Bridge Road / Great Suffolk Street junction, using strategic roads and pedestrian desire lines as defined cut off points (See Appendix C – Location Plan and Extents of Consultation).
- 1.3.4 The distribution area was large enough to gain views from the wider community that may be considered to be affected by the proposed measures. A mailing list was established for the area by way of the Council's GIS database. In addition, the consultation documents and plans were supplied to the Council's established list of statutory consultees including London Buses, cycle groups and the Metropolitan Police. Please see Appendix D of list of addresses within the distribution area. Consultation documents were also delivered to the Southwark Housing Department and Grange Primary School.
- 1.3.5 The consultation documents were delivered by Royal Mail to 343 addresses detailed within the distribution list. The documents were delivered on the 5th August 2014, with a return deadline of the 5th September, allowing 4 weeks for the consultation period. However due to the summer holiday period, responses were accepted online until the 12th September 2014.
- 1.3.6 The proposals were also available to view online using consultation section of the council's website, with an e-form questionnaire provided in order to capture responses.

2.0 Consultation Responses

2.1 Response Rate and Distribution

2.1.1 A total of 38 responses were received during the consultation period (26 returned questionnaires and 12 online responses), equating to a 11% response rate.

2.1.2 Four responses were received from Statutory Consultees (Southwark Living Streets, Southwark Cyclists, Lambeth Cyclists and Sustrans).

2.2 Questionnaire Analysis

2.2.1 The questionnaire element of the consultation document contained the following key questions and associated tick box options:

Q1. Are you a resident or business?

Q2. Do you support the proposals?

2.2.2 The following is a summary of replies received:

Question 1 - Are you a resident or business?

	Resident	Business
Replies	36	2
Total	95%	5%

Table 1: Returned questionnaire results for question 1

2.2.3 Table 1 indicates that the majority of responses received throughout the consultation period were from local residents, with only three businesses formally replying.

Question 2 – Do you support the proposals?

	Support	Opposed	No Opinion
Replies	27	11	0
Total	71%	29%	%

Table 2: Returned questionnaire results for question 2



Figure 3: Graphical representation of consultation data for question 2

2.2.4 Table 2 and figure 2 indicate a majority of support for question 2, with 71% welcoming the proposed measures.

2.3 Additional Comments

2.3.1 The questionnaire element of the consultation document invited consultees to attach any additional comments they may have on the proposals when returning the reply-paid questionnaire. Consultees were also able to reply to the consultation online using the council's website.

2.3.2 The majority of respondents (71%) indicated support for the proposed measures, and discussed that the junction improvements will be a big benefit to cyclists and the plans at the Southwark Bridge Road / Great Suffolk Street junction are well thought out.

2.3.3 A number of respondents commented that the improvements were good and will make the environment safer for cyclists and make it easier to exit Webber Street.

2.3.4 A comment from a local business highlighted that over half their staff cycle to work and the proposed improvements are very much appreciated.

2.3.5 A number of respondents commented that they were regular cyclists and welcomed the new, safer route between Southwark and Lambeth through the provision of new infrastructure specifically targeted at signalised road junctions where the majority of accidents occur.

2.3.6 A respondent commented that the early start pre-signal sequence at the traffic lights is a good idea and will help reduce left turn conflict with cars.

2.3.7 A comment was received stating that the proposed semi segregated cycle lane on the western side of Great Suffolk Street won't work as there is not enough carriageway width. *

* In response, traffic movements have been modelled at the junction to ensure that turning manoeuvres can take place without conflict. In order to create enough carriageway width to accommodate the semi-segregated cycle lane, the southern kerblines of Great Suffolk Street between Southwark Bridge Road and Webber Street is being cut back. This kerblines alteration can take place without compromising footway width for pedestrians or result in extensive changes to the existing layout of the adjacent public space.

Semi-segregated cycle lanes leading to advanced cycle waiting areas at signal junctions are being trialled as part of the Quietway proposals to assess their effectiveness. Armadillo lane delineators are also low cost and can be easily removed with minimal effort if they are deemed to be ineffectual.

- 2.3.8 A suggestion was made to divert more traffic away from the junction, particularly Webber Street and the western arm of Great Suffolk Street, as there are always accidents here and conflict with large vehicles. *

* In response, whilst closing or diverting traffic from Great Suffolk Street and Webber Street would potentially improve the Quietway route further, a comprehensive feasibility study would be required to ascertain the likelihood of traffic displacement onto other streets in the area. Due to the controversial nature of preventing through traffic and the potential access changes to residential and commercial properties, a separate consultation specifically on this proposal would be required. However it is noted that Great Suffolk Street and Webber Street have low traffic volumes and speeds and therefore the risk of conflict with cyclists is minimal. Such a proposal would also potentially lead to objections from local traders who would highlight that preventing through traffic would have a substantial impact on their trade.

- 2.3.9 A request was made for more speed controls (speed bumps) in Sturge Street, as vehicles travel fast along there on approach to Great Suffolk Street making this junction dangerous to cross. *

* In response, Sturge Street is not located on the Quietway route and therefore Quietway funding can not be utilised to introduce traffic calming measures for this road. If there is a speeding issue in Sturge Street and local support for the introduction of traffic calming, a deputation can be made to the local Community Council forum in order for formal discussion and investigation to take place to see if it is feasible to make changes to the highway layout to curtail traffic speeds.

- 2.3.10 A request was made to improve the existing zebra crossing on Southwark Bridge Road adjacent to Lant Street, as so many motorists and cyclists do not observe this crossing and stop for pedestrians. *

* In response, whilst this zebra crossing is located on the Cycle Super Highway Network, this location falls outside the scope of the Quietway route proposals. Therefore upgrading the zebra crossing on Southwark Bridge Road adjacent to Lant Street is unable to be considered as part of the current proposals. However the existing Cycle Super Highway is currently being reviewed by Transport for London and the issues raised by the respondent will be disseminated to the project review team for them to investigate the issue further.

2.3.11 A request was made for more cycle parking at the shops (eastern side of the junction) to improve potential trade. *

* In response, the council will be identifying suitable locations along the Quietway Route to install new cycle parking provision. There are a number of potential sites in Great Suffolk Street that have been identified, adjacent to the shopping parade on the eastern arm of the junction. In accordance with numerous studies concluding that cyclists spend on average more money in local shops than other customers using alternative means of transport, the provision of additional cycle parking at the Great Suffolk Street Shopping Parade is an essential element to support local businesses, as well as helping to encourage short, local journeys to this location to be undertaken by bicycle

2.3.12 Analysis of the additional comments from respondents that objected to the scheme highlighted the following concerns:

A number of objections were received stating the proposals will not prevent cyclists taking a shortcut over the pavement to access Webber Street. *

* In response, cyclists recognise the safest route for cyclists to cross the junction is to use the existing pelican crossing and traverse across the public square to access Webber Street, which potentially results in conflict with pedestrians using this space. This practice is currently illegal and is not to be encouraged.

The proposed measures at the Southwark Bridge Road / Great Suffolk Street junction will improve safety and accessibility for cyclists. The advanced cycle waiting areas, cycle pre-signal priority and semi-segregated cycle lane on the western arm of Great Suffolk Street will make the junction considerably more cycle friendly, minimising existing left hook risks from motor vehicles and allowing cyclists to take a priority position in front of motor traffic on both arms of Great Suffolk Street.

In addition, the keep clear marking adjacent to the exit lane from Webber Street will ensure that cyclists can safely access the semi-segregated cycle lane leading to the advanced cycle waiting area.

The above measures will significantly reduce the risk of cyclists continuing to traverse across the signalised pedestrian crossing and public square and should give cyclists more confidence when crossing the junction.

A number of respondents objected on the grounds of parking loss being unacceptable and introduction of double yellow lines*

* In response, there is no parking loss associated with these proposals and all existing short stay and permit holder bays in Great Suffolk Street are to be retained. Additional kerbside parking controls are only proposed adjacent to the junction or pedestrian crossing points to improve safety and ensure sightlines are maintained. Vehicles should not be parked within 10m of a road junction due to the adverse effect on sightlines and constraints on carriageway width. Therefore the availability of kerbside parking for local residents and businesses is not compromised as part of these proposals.

A respondent objected on the grounds that it will be impossible for vehicle to pick up and drop off on the kerb outside no. 118 Southwark Bridge Road. The proposals to introduce the semi-segregated cycle lane will turn this building into an island. *

* In response, apart from the proposed section of double yellow line parking controls extending 10m from the pedestrian crossing location on the northern side of Southwark Bridge Road, there is an existing single yellow line retained for the majority of the frontage adjacent to no. 118 Southwark Bridge Road. The single yellow line parking restriction allows for loading activity to service this building.

Double yellow line parking restrictions are required within the proposed semi-segregated cycle lane to ensure it is free from obstruction. This area would be inappropriate for loading due to its proximity to the signal junction and being opposite the Webber Street junction. Vehicles parked on this kerbline would block forward sightlines of approaching traffic to the primary signal heads, forcing vehicles onto the opposing carriageway on approach to the junction and would obstruct vehicles turning out of Webber Street into Great Suffolk Street.

A respondent objected stating that there were enough measures already for cyclists, cycle usage in this area is low and they make crossing the road hazardous. *

* In response, the majority of collisions involving cycles take place at signalled junctions. Therefore it essential that safety improvements and new technology is introduced to not only reduce the number and severity of accidents, but encourage more people to cycle as a primary mode of transport.

Recent cycle counts along Great Suffolk Street have shown that over 200 cycles traverse this section of the Quietway route every hour in peak morning and afternoon periods. These numbers are expected to rise year on year as cycling levels in the borough increase and significant improvements take place to safety and accessibility for cyclists.

There is no evidence to suggest that cyclists pose a safety risk for pedestrians crossing the carriageway. Cyclists are subject to the same traffic regulations that motor vehicles adhere to on the carriageway. The Great Suffolk Street / Southwark Bridge Road junction is fully signalised with an 'all green' pedestrian phase so there is minimal risk of conflict between pedestrians and other road users.

A number of respondents objected highlighting that the work is a waste of time and money and the reality is that London is not designed for cycling. *

* In response, the proposed measures along the Quietway route align with the council's emerging cycling strategy and the Mayor's Vision for Cycling. The Mayor has commitment to invest total of £913m over the next 10 years in cycling safety and infrastructure development to significantly increase the modal share in cycling as a safe, healthy and sustainable form of transport in London.

The London boroughs will play a central role in delivering this vision, by helping to develop, fund and deliver better and safer routes for cycling.

The council welcomes significant investment from Transport for London to take forward the Quietway programme that will not only significantly upgrade a number of existing cycle routes, but also make considerable improvements to the streetscape for the benefits of all road users and local residents.

The road network in London has been previously developed around the movement of goods and services. However with cycling numbers increasing throughout London, improvements to cycling infrastructure, dangerous junctions and reallocation of road space needs to take place not only to improve cycling safety but also embrace a change in attitude and culture to cycling being a viable and suitable mode of transport available to people of all abilities; and the introduction of Quietway routes is an integral part of this initiative and

A number of objections were received stating that cyclists currently bypass the traffic lights by using the pavement and pedestrian crossing resulting in collisions and near misses with pedestrians. *

* In response, as outlined above, the proposed improvements to the Great Suffolk Street / Southwark Bridge Road junction will significantly reduce the likelihood that cyclists will use the signalised pedestrian crossing and traverse across the public square to bypass the junction. The proposed cycle access and priority improvements will significantly reduce existing conflict risk at the junction between cyclists and other road users.

A number of objections highlighted that Great Suffolk Street and Webber Street have too much through traffic and are too busy to be a Quietway route. Either provide full segregation or prevent through traffic with modal filtering. *

* In response, segregation is not required along the Quietway routes and is not justified in Great Suffolk Street or Webber Street due there being low traffic volumes and vehicle speeds, which are controlled using vertical deflection measures. Parking provision for local residents and businesses is essential along this section of the route and does not pose a threat to cyclists.

The traffic volumes using Great Suffolk Street and Webber are low compared to other roads in the area, which was demonstrated by traffic counts in morning and afternoon peak periods. The greatest risk to cyclists using this section of the route is potential conflict from motor vehicles turning across the path of cyclists traversing straight ahead at the Southwark Bridge Road junction. The advanced cycle awaiting area in conjunction with early start pre-signal measures practically eliminate this risk, making the Great Suffolk Street routing of the Quietway route both attractive and safe for cyclists of all abilities. The measures adjacent to Webber Street will also benefit cycle access and to ensure that they can safely access the advanced cycle waiting area on the eastbound approach of the Great Suffolk Street / Southwark Bridge Road junction.

As discussed previously, whilst closing Great Suffolk Street or Webber Street to through traffic would improve the Quietway route further, a comprehensive feasibility study would be required to ascertain the likelihood of traffic displacement onto other streets in the area. Due to the controversial nature of preventing through traffic and the potential access changes to residential and commercial properties, a separate consultation specifically on this proposal

would be required. However, as outlined above, traffic volumes and speeds are low and the risk of conflict with cyclists is minimal and therefore closing the roads would not be justified. Such a proposal would also potentially lead to objections from local traders and residents who would highlight that preventing through traffic would have a substantial impact on their trade and access to their properties.

2.3.9 18% respondents did not submit a further comment.

2.4 Levels of Consensus

2.4.1 The following majority levels of agreement have been given in relation to the questions contained within the consultation document:

- 71% of respondents support the introduction of the Quietway cycle route proposals for Site L;
- 29% of respondents are opposed to the proposals.

2.5 Statutory Consultee Replies

2.5.1 Five statutory consultees provided a reply to the consultation.

- a) **Southwark Living Streets** replied indicating strong support for the scheme and stated that the Southwark Bridge Road / Great Suffolk Street junction improvements were excellent and keep clear marking at Webber Street was a good idea to help cyclists emerge from Webber Street.
- b) **Lambeth Cyclists** replied in support of the scheme and that the final design ensures the 'elephant footprint' markings across Southwark Bridge Road line up with the centre of the lanes and not the nearside kerbs of the junction. This will further reduce left hook risk.

* In response, the consultation drawings is only indicative and as part of the scheme detailed design process, the markings will be placed appropriately in the carriageway to maximise the safety benefit for cyclists using this junction.

- c) **Southwark Cyclists** replied indicating support for the scheme and provided detailed results of their junction analysis that concluded that there were no major risks to cyclists at the junction and that the proposed measures will improve the existing situation. A suggestion was made to ban the left turn at the junction from the western arm into Great Suffolk Street into Southwark Bridge Road, as this would totally remove the risk of left hook collisions. *

* In response, due to the measures proposed, including an early start cycle phase and semi-segregated access lane leading to the larger advanced cycle waiting area, the risk of left hook conflict between cyclists and motor vehicles is significantly reduced. Banning the left turn here would require a separate consultation and further area wide analysis to ascertain the potential displacement of traffic onto neighbouring roads. As part of the post implementation monitoring of the scheme, the operation of the junction will be closely analysed to ensure that the left turn collision risk to cyclists has been mitigated through the introduction of the measures outlined above.

- d) **Sustrans** replied indicating support for the scheme but did not provide any specific questions or comments on the proposed measures.

2.5.2 No objections were received from Ward Members throughout the consultation period.

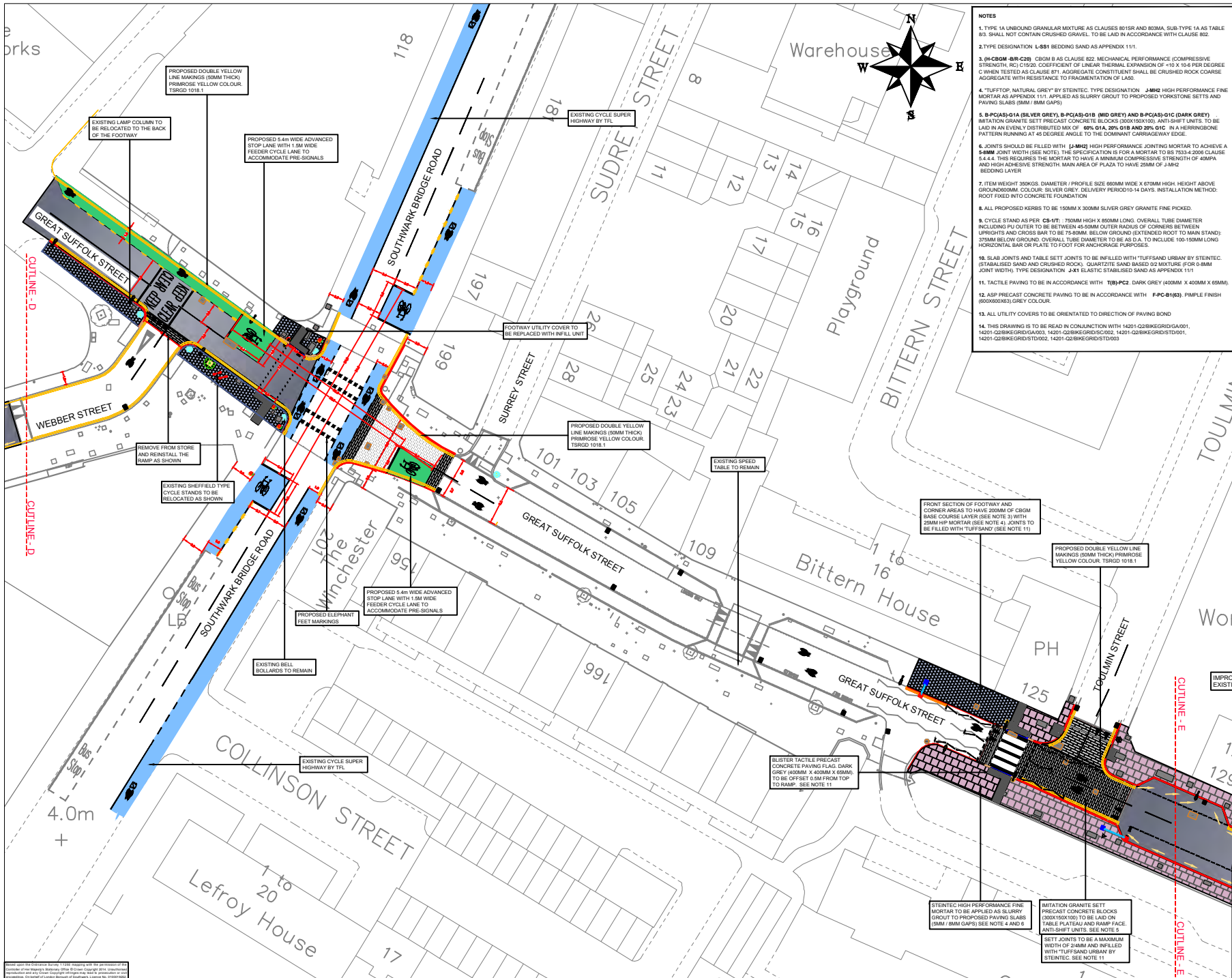
3.0 Recommendations

Due to the majority of respondents supporting the scheme and Southwark's on-going commitment to improve and promote cycling in the borough, it is recommended that the scheme proceed to implementation.

Appendices

Appendix A:	Initial Scheme Design
Appendix B:	Consultation Documents
Appendix C:	Location Plan and Extents of Consultation
Appendix D:	List of Addresses within the Distribution Area

Appendix A: Initial Scheme Design



- NOTES**
1. TYPE 1A UNBOUND GRANULAR MIXTURE AS CLAUSES B01SR AND B03MA, SUB-TYPE 1A AS TABLE B.3. SHALL NOT CONTAIN CRUSHED GRAVEL. TO LAID IN ACCORDANCE WITH CLAUSE B02.
 2. TYPE DESIGNATION L-S&T BEDDING SAND AS APPENDIX 11/1.
 3. (H-CBGM-BR-C20) CBGM AS CLAUSE B2. MECHANICAL PERFORMANCE (COMPRESSIVE STRENGTH), RC11620. COEFFICIENT OF LINEAR THERMAL EXPANSION OF $+10 \times 10^{-6}$ PER DEGREE C WHEN TESTED AS CLAUSE B71. AGGREGATE CONSTITUENT SHALL BE CRUSHED ROCK COMBIE AGGREGATE WITH RESISTANCE TO FRAGMENTATION OF LA50.
 4. "TUFFTOP", NATURAL GREY "BY" STENITEC. TYPE DESIGNATION J-MH2 HIGH PERFORMANCE FINE MORTAR AS APPENDIX 11/1. APPLIED AS SLURRY GROUT TO PROPOSED YORKSTONE SETTS AND PAVING SLABS (5MM / 8MM GAPS).
 5. (B-PCAS)-G1A (SILVER GREY), (B-PCAS)-G1B (MID GREY) AND (B-PCAS)-G1C (DARK GREY) IMITATION GRANITE SETT PRECAST CONCRETE BLOCKS (300X100X100). ANTI-SHIFT UNITS TO BE LAID IN AN EVENLY DISTRIBUTED MIX OF 60% G1A, 20% G1B AND 20% G1C IN A HERRINGBONE PATTERN RUNNING AT 45 DEGREE ANGLE TO THE DOMINANT CARRIAGEWAY EDGE.
 6. JOINTS SHOULD BE FILLED WITH (J-MH2) HIGH PERFORMANCE JOINTING MORTAR TO ACHIEVE A 5.0MM JOINT WIDTH (SEE NOTE). THE SPECIFICATION IS FOR A MORTAR TO BS 7533-2:2006 CLAUSE 5.4.4. THIS REQUIRES THE MORTAR TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 40MPa AND HIGH ADHESIVE STRENGTH. MAIN AREA OF PLAZA TO HAVE 25MM OF J-MH2 BEDDING LAYER.
 7. ITEM WEIGHT 300KGS, DIAMETER / PROFILE SIZE 600MM WIDE X 270MM HIGH, HEIGHT ABOVE GROUND/900MM COLOUR: SILVER GREY, DELIVERY PER/610/14 DAYS, INSTALLATION METHOD: ROOT FIXED INTO CONCRETE FOUNDATION.
 8. ALL PROPOSED KERBS TO BE 150MM X 300MM SILVER GREY GRANITE FINE PICKED.
 9. CYCLE STAND AS PER CS-1/7: 750MM HIGH X 500MM LONG, OVERALL TUBE DIAMETER INCLUDING PU OUTER TO BE BETWEEN 45-50MM OUTER RADIUS OF CORNERS BETWEEN UPRIGHTS AND CROSS BAR TO BE 75.00MM. BELOW GROUND (EXTENDED ROOT TO MAIN STAND): 375MM BELOW GROUND, OVERALL TUBE DIAMETER TO BE AS D.A. TO INCLUDE 100-150MM LONG HORIZONTAL BAR OR PLATE TO FOOT FOR ANCHORAGE PURPOSES.
 10. SLAB JOINTS AND TABLE SETT JOINTS TO BE FILLED WITH "TUFFSAND USRBAY" BY STENITEC (STABILISED SAND AND CRUSHED ROCK, QUARTZITE SAND BASED 0/2 MIXTURE) FOR 0-8MM JOINT WIDTH; TYPE DESIGNATION J-A1 ELASTIC STABILISED SAND AS APPENDIX 11/1.
 11. TACTILE PAVING TO BE IN ACCORDANCE WITH (TB)-PC2, DARK GREY (400MM X 400MM X 65MM).
 12. ASP PRECAST CONCRETE PAVING TO BE IN ACCORDANCE WITH (F-PC)-B1(63), PIMPLE FINISH (600X600X60); GREY COLOUR.
 13. ALL UTILITY COVERS TO BE ORIENTATED TO DIRECTION OF PAVING BOND.
 14. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH 14201-G2/8K/EG/GR/D/001, 14201-G2/8K/EG/GR/D/002, 14201-G2/8K/EG/GR/D/003, 14201-G2/8K/EG/GR/D/004, 14201-G2/8K/EG/GR/D/005, 14201-G2/8K/EG/GR/D/006, 14201-G2/8K/EG/GR/D/007, 14201-G2/8K/EG/GR/D/008, 14201-G2/8K/EG/GR/D/009, 14201-G2/8K/EG/GR/D/010, 14201-G2/8K/EG/GR/D/011, 14201-G2/8K/EG/GR/D/012, 14201-G2/8K/EG/GR/D/013

LEGEND / NOTES.

KEY:

- CARRIAGEWAY RECONSTRUCTION: 40mm THICK TSCS B110 PAV. 68+ LAD ON 50mm THICK 5MM 14 FINE 3480 des WITH 2 TO CLAST OR CARRIAGEWAY REPAIR CONSTRUCTION.
- FOOTWAY RECONSTRUCTION: PROVIDE AND LAY PCC PAVING FLAG 400 x 400 x 65MM FOR FOOTWAY CONSTRUCTION LAD ON 200mm THICK TYPE 1 SUBBASE AND THICK LOW FINES BEDDING SAND.
- RAISED FEATURES: PROVIDE AND LAY ANTI-SHIFT PC BLOCKS 300 x 150 x 70MM FOR RAISE TABLE CONSTRUCTION LAD ON 350mm THICK CBGM-C20 AND 30mm THICK LOW FINES BEDDING SAND.
- BUSTER PAVING: PROVIDE AND LAY PCC BUSTER TACTILE PAVERS 400 x 400 x 75mm THICK (REF TB-IN/5/75) ON SPECIFIED FOOTWAY CONSTRUCTION LAD ON 200mm THICK CBGM-C20 SUBBASE AND 30mm THICK "TUFFEED" LAYING MORTAR BY STENITEC.
- TUFFEED LAYING: SECTION OF FOOTWAY TO BE LAID ON 200mm THICK CBGM-C20 SUBBASE AND 30mm THICK "TUFFEED" LAYING MORTAR BY STENITEC.
- CYCLE LANE: PROPOSED CYCLE LANE / ADVANCED STOP LINE (GREEN COLOUR) FOR INDICATIVE PURPOSE ONLY.
- EXISTING CYCLE SUPER HIGHWAY BY TRANSPORT FOR LONDON.
- ROADWORK: EXISTING IRONWORK COVER AND FRAME TO BE RELOCATED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INSTALL FLAT KERB 150 x 300MM - UNITS SILVER GREY.
- STANDARD GRANITE KERBS: INSTALL NEW STANDARD GRANITE KERBS (750MM X 300MM) LAD WITH 100MM UPSTAND ON STA INCLUDING FRONT KERB HAUNCHING TO BE 100MM BELOW CARRIAGEWAY - REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE T1 (SHEET 1 OF 4) TYPE R + TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE KERBS FINISHED: INSTALL NEW STANDARD GRANITE KERBS (750MM X 300MM) LAD WITH 100MM UPSTAND ON STA INCLUDING FRONT KERB HAUNCHING TO BE 100MM BELOW CARRIAGEWAY - REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE T1 (SHEET 1 OF 4) TYPE R + TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE TO TRANSITION KERBS: NEW STRAIGHT KERB TYPE T1 (SHEET 1 OF 4) FOR FURTHER DETAILS.
- REFER TO DRAWING LBS/C10/KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE T1 (SHEET 1 OF 4) TYPE R + TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- NEW WHITE LINE ROAD MARKING.
- NEW YELLOW LINE ROAD MARKING.
- RED ROAD MARKINGS TO BE REINSTALLED.
- NEW DRAIN PIPE: PROVIDE AND INSTALL SURFACE WATER DRAIN PIPE GROUP P125mm INTERNAL DIAMETER DEPTH OF INVERT+200; AVERAGE DEPTH OF 700MM ON BED TYPE Z OF 750MM ON BED TYPE Z.
- NEW GULLY: PROVIDE AND INSTALL PRECAST CONCRETE TRAPPED GULLY WITH TYPE D COVER, GULLY DEPTH \approx 700mm OR BUILD CATCH PIT WHERE SPECIFIED.
- EXISTING ROAD GULLY TO BE CONVERTED TO MANHOLE.
- EXISTING ROAD GULLY TO BE RAISED AND ADJUSTED.
- NEW CYCLE SEPARATOR OR ARMADILLOS.
- NEW TRAFFIC SIGNAL POST LOCATION (PRIMARY/SECONDARY).
- NEW LAMP COLUMN LOCATION.
- EXISTING LAMP COLUMN LOCATION.
- NEW SHEFFIELD CYCLE STANDS.
- NEW TREE PIT CONSTRUCTION (REFER TO STANDARD DRAWING DETAILS NO 14201-G2/8K/EG/GR/D/001).
- PROPOSED LONGITUDINAL FALL ON CARRIAGEWAY.
- EXISTING BELL BOLLARDS TO BE RELOCATED AS SHOWN IN THE DRAWING.
- NEW FOOTWAY CONSTRUCTION: INSTALL NEW 600X600, 75MM THICK PCC CONCRETE PAVING FLAGS FOR FOOTWAY CONSTRUCTION LAD ON 200mm THICK TYPE 1 SUBBASE AND THICK LOW FINES BEDDING SAND.
- PROPOSED LOCATION FOR THE SIGN POSTS.
- FULL CARRIAGEWAY CONSTRUCTION: 40mm THICK TSCS B110 PAV. 68+ LAD ON 50mm THICK 5MM 14 FINE 3480 des WITH 2 TO CLAST OVER 100mm THICKNESS OF 5% EMER BINDER COURSE (AC 20 EMEZ B20 1500 DES) AS APPENDIX 7/1 TO BE LAID ON RAISE, 200MM THICK CBGM-B (H-CBGM-BR-C20) AS CLAUSE B22. MECHANICAL PERFORMANCE (COMPRESSIVE STRENGTH) RC11620.

Revision	Date	Amendment	Drawn	Design	Checked	Approved

PUBLIC REALM PROJECTS TEAM 150 TOOLEY STREET SET P-5LX

Project
CENTRAL LONDON BIKE GRID
Q2 - BLUE ROUTE

Title
GENERAL ARRANGEMENT
SECTION - 5
GREAT SUFFOLK STREET JUNCTION

Contract No.
HW2013 NEC3

Scale
1 : 250 @ A1

Drawing No.
14201-CENTRAL GRID/GA/005

Date Issued
JULY 2014

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Appendix B: Consultation Documents

We want your views

It is important for all consultees to respond to the consultation. We would be grateful if you could take the time to review the proposals outlined in this document and provide a response using the pre-paid envelope and questionnaire provided by **Friday 5th September 2014**

Alternatively, you can view the proposals at www.southwark.gov.uk/consultations and complete the online questionnaire.

Your views are essential for us to understand your requirements for the proposal and form a fundamental part of the scheme development process, whether you use public transport, cycle, walk or drive a private vehicle.



Quietway
Greenwich to Waterloo

Site L –Great Suffolk Street / Southwark Bridge Road Junction

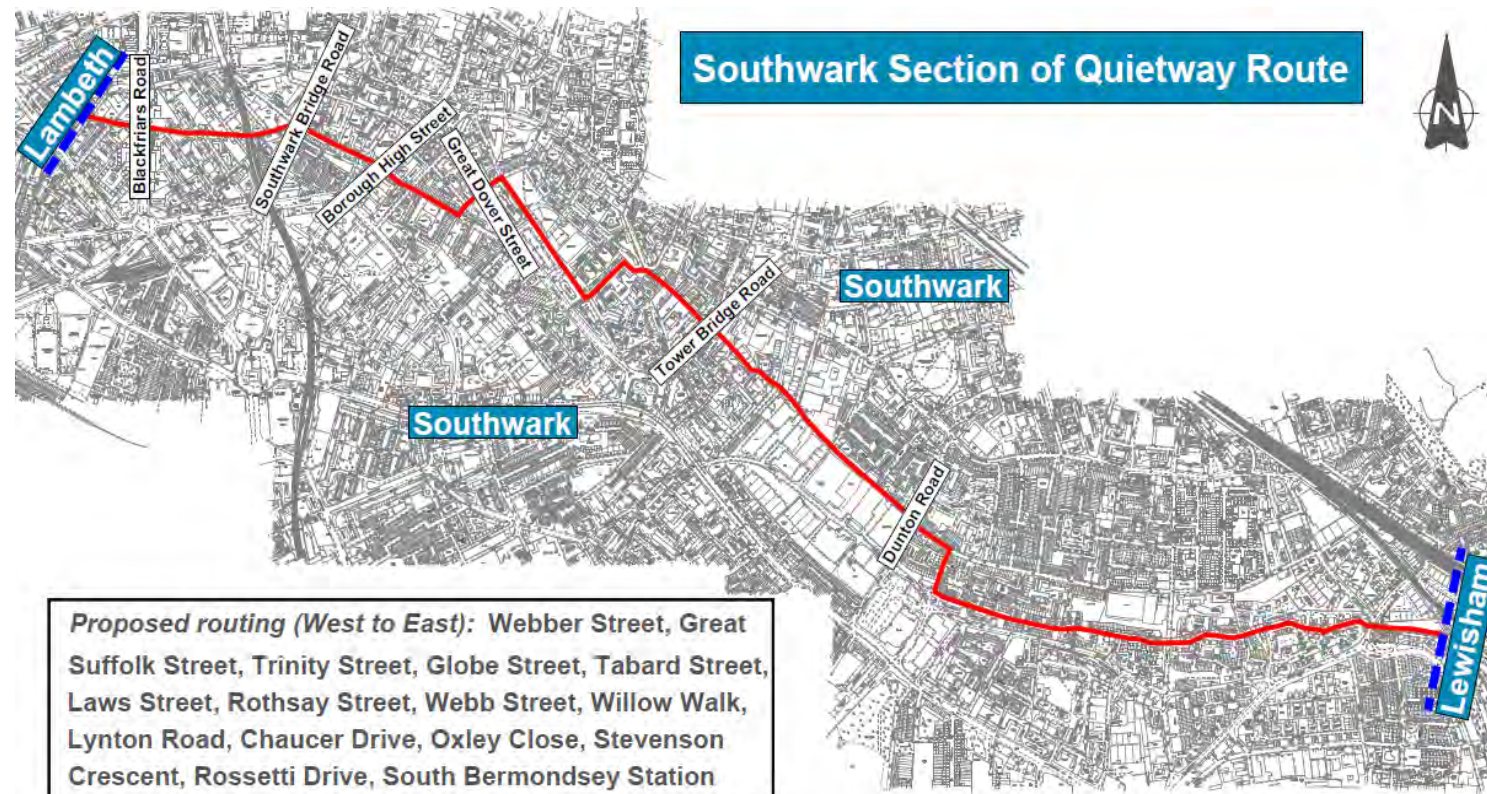
Cycle Priority Improvements

What happens next?

The responses to the questionnaire will be analysed and taken into account in the final design of the proposed works. As you will appreciate Southwark Council receives many comments from consultations and therefore are unable to respond personally to specific issues raised. However all comments and suggestions will be taken into consideration before a decision is made. The consultation results and recommendations will be presented at a local community council meeting in September 2014.

Should you require any further information regarding the proposed scheme please do not hesitate to contact Chris Mascord at chris.mascord@southwark.gov.uk

Further information on other schemes along the route in Southwark can also be found at: www.southwark.gov.uk/consultations



Have your say

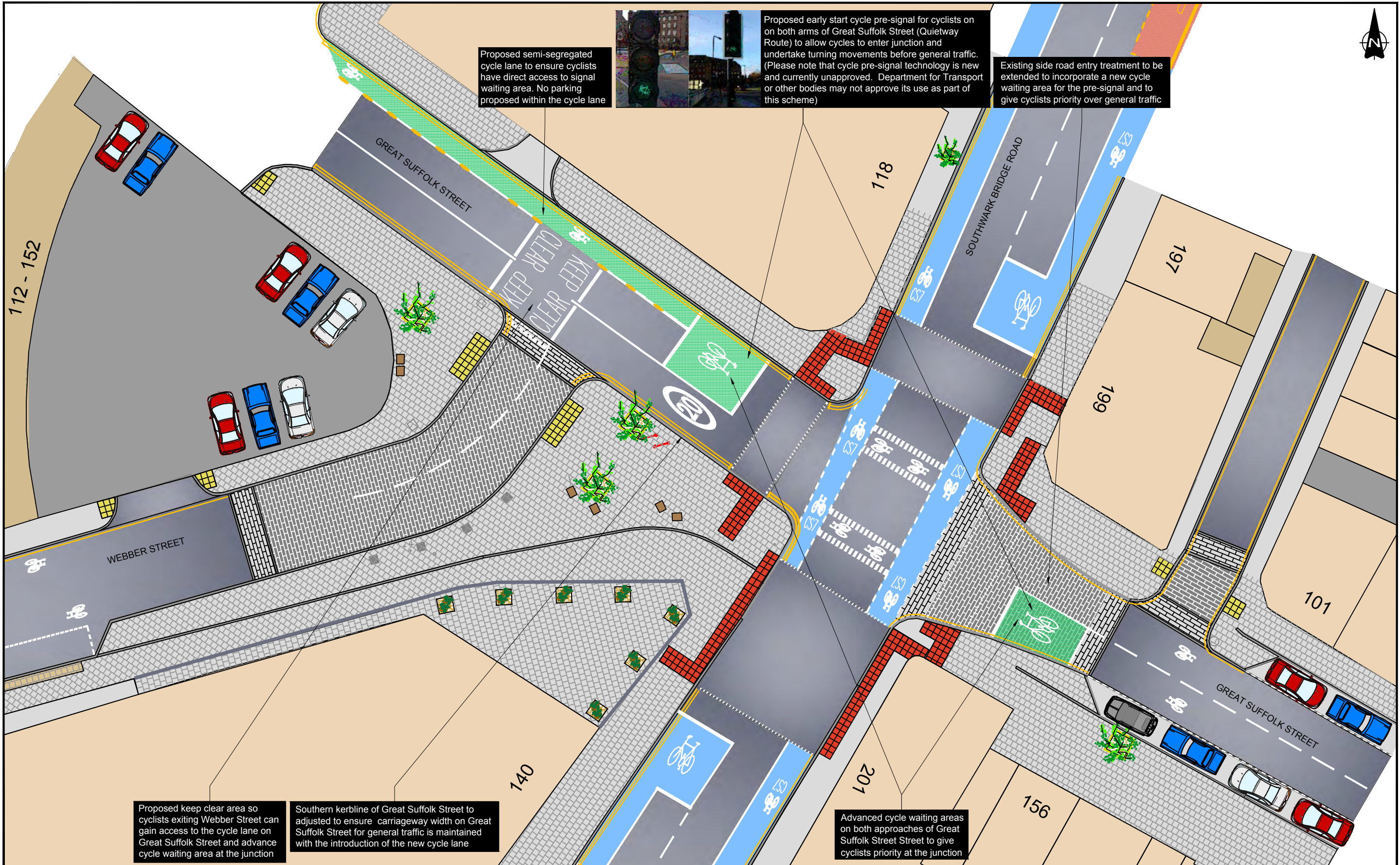
Southwark Council is holding a consultation to receive residents' and key stakeholders' comments regarding proposals relating to the borough's section of the Quietway (Greenwich to Waterloo) cycle route. This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey station in the east and traversing local roads to The Cut in the west. The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

Background

Quietways are a network of bike routes for less confident cyclists using low-traffic back streets. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.

What are the proposed changes?

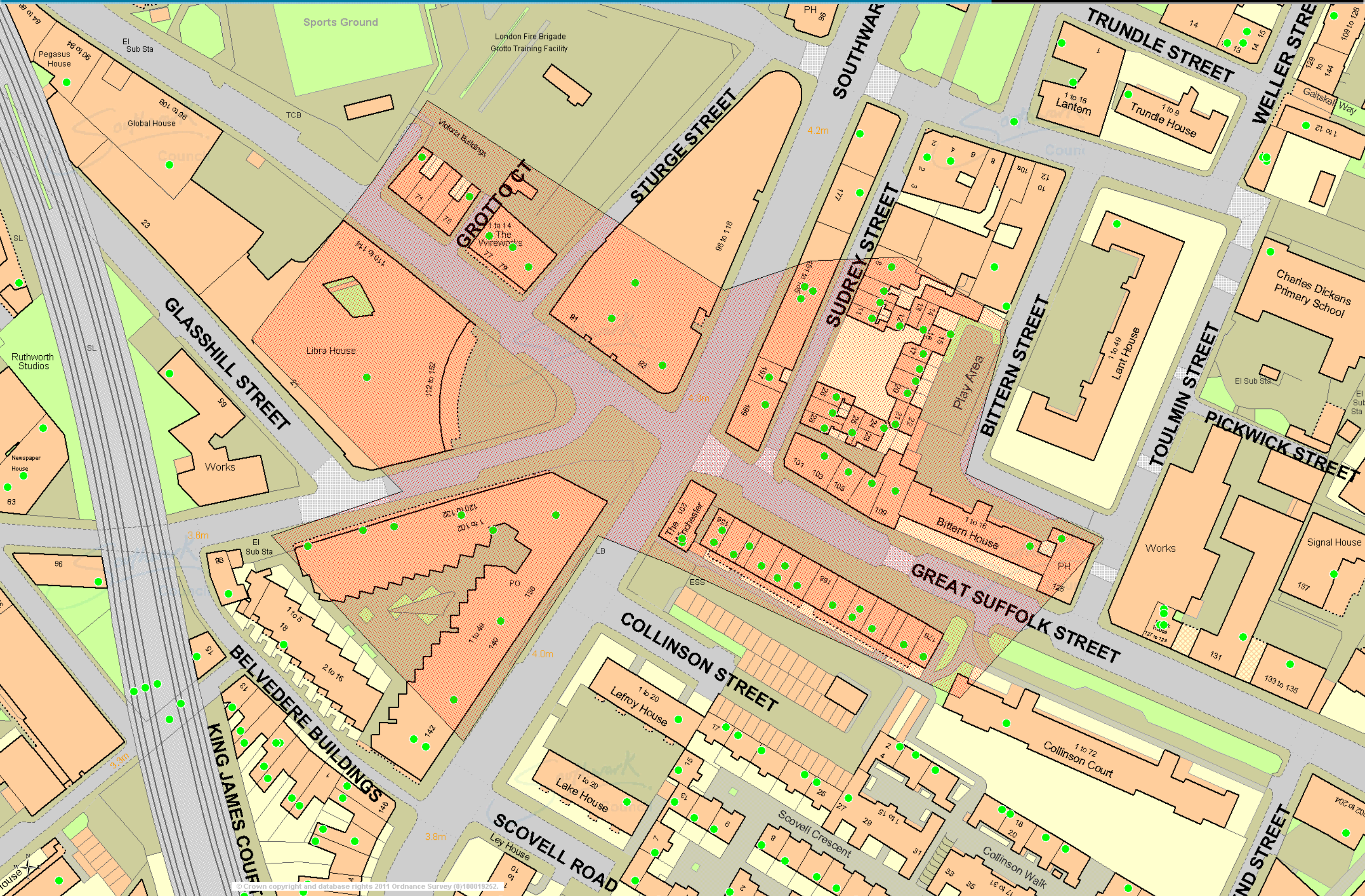
- Subject to approval from the DfT and other governing bodies, the Great Suffolk Street arms of the Southwark Bridge Road junction will have an early start cycle pre-signal so cyclists can traverse the junction and undertake turning movements before general traffic.
- Existing raised carriageway table on Great Suffolk Street on the eastern side of the junction is to be extended to provide a cycle waiting area, ahead of general traffic at the signals.
- The western arm of the junction will have semi segregated cycle lane so that cyclists can have unobstructed access to the waiting areas on approach to Southwark Bridge Road.
- Keep Clear marking proposed on Great Suffolk Street adjacent to Webber Street to allow cyclists exiting Webber Street to access the cycle lane and advanced waiting area on approach to Southwark Bridge Road.
- The carriageway of Webber Street is to be resurfaced and existing sets of speed cushions to be replaced with sinusoidal humps to improve the ride quality for cyclists.



Appendix C: Location Plan and Extents of Consultation

Quietway Site L - Consultation Area

Date 31/7/2014



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Appendix D: List of Addresses within Distribution Area

ORGANISATION	SAO	PAO	STREET	TOWN	POSTCODE
	FLAT 6		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 11		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 79		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 30		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 35		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 39		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 1		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 11	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 21		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 95		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 100		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 5		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
	FLAT 10		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
	FLAT 26		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 16		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
	FLAT 20		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
	FLAT 25		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
JOHNSON KING LTD	FLAT 4	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
	UNIT 3		93 GREAT SUFFOLK STREET	LONDON	SE1 0BX
	FLAT 16		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT B	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU
	FLAT B		183 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
NIKI NEWS			107 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
	FLAT 4		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
	FLAT 5		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
		12 WIREWORKS COURT, 79	GREAT SUFFOLK STREET	LONDON	SE1 0BU
	FLAT 65		130 WEBBER STREET	LONDON	SE1 0JP
			156 GREAT SUFFOLK STREET	LONDON	SE1 1PE
	FLAT 312		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 401		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 408		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 69		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 75		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 506		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 103		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 108		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 114		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 412		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 12	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 16	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 7	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 508		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 216		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 306		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	APARTMENT 1	EMILY COURT, 8	SUDREY STREET	LONDON	SE1 1PF
	FLAT 46		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 51		130 WEBBER STREET	LONDON	SE1 0JN
	FLAT 56		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 60		130 WEBBER STREET	LONDON	SE1 0JP
		7 WIREWORKS COURT, 79	GREAT SUFFOLK STREET	LONDON	SE1 0BU
	FLAT 2		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
	LIVING ACCOMMODATION		125 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
	FLAT 9	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
		162B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		164-166	GREAT SUFFOLK STREET	LONDON	SE1 1PE
DENISE FLORIST			168 GREAT SUFFOLK STREET	LONDON	SE1 1PE
		170B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
THE SOL FACTORY LTD			172 GREAT SUFFOLK STREET	LONDON	SE1 1PE
		174A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		174B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
ERIC'S GREEN GROCONS			176 GREAT SUFFOLK STREET	LONDON	SE1 1PE
OLLEY'S FISH & CHIPS			101 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
GIGGLING-SAUSAGE			105 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
IL FURNAR			109 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
		156B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		158B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
			14 SUDREY STREET	LONDON	SE1 1PF
			125 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
G WORRAL & SON LTD			174 GREAT SUFFOLK STREET	LONDON	SE1 1PE
THE SALON,			160 GREAT SUFFOLK STREET	LONDON	SE1 1PE
		156A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		172B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		176B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		162A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
	FLAT 85		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 89		130 WEBBER STREET	LONDON	SE1 0JP
MACGREGOR JONES			83 GREAT SUFFOLK STREET	LONDON	SE1 0BU
			77 GREAT SUFFOLK STREET	LONDON	SE1 0BU
			10 SUDREY STREET	LONDON	SE1 1PF
			11 SUDREY STREET	LONDON	SE1 1PF
			12 SUDREY STREET	LONDON	SE1 1PF
		160A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		160B	GREAT SUFFOLK STREET	LONDON	SE1 1PE
			15 SUDREY STREET	LONDON	SE1 1PF
			18 SUDREY STREET	LONDON	SE1 1PF
			25 SUDREY STREET	LONDON	SE1 1PF
			26 SUDREY STREET	LONDON	SE1 1PF
			27 SUDREY STREET	LONDON	SE1 1PF
			28 SUDREY STREET	LONDON	SE1 1PF
			120 WEBBER STREET	LONDON	SE1 0QL
			122 WEBBER STREET	LONDON	SE1 0QL
			124 WEBBER STREET	LONDON	SE1 0QL
ROYAL NATIONAL LIFEBOAT INSTITUTION			136 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DG
SUNSTAR GROUP LTD			138 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DG
INTERACT			19 SUDREY STREET	LONDON	SE1 1PF
			21 SUDREY STREET	LONDON	SE1 1PF

FLAT 83		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 84		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 411		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
FLAT 1		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 2		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 3		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 4		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 68		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 71		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 72		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 73		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 44		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 45		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 28		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 29		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 31		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 32		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 33		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 34		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 36		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 101		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 102		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 25		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 40		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 41		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 42		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 43		130 WEBBER STREET	LONDON	SE1 0JN
UNIT 4		93 GREAT SUFFOLK STREET	LONDON	SE1 0BX
UNIT 5		93 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 80		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 81		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 82		130 WEBBER STREET	LONDON	SE1 0JP
APARTMENT 2	EMILY COURT, 8	SUDREY STREET	LONDON	SE1 1PF
APARTMENT 3	EMILY COURT, 8	SUDREY STREET	LONDON	SE1 1PF
APARTMENT 4	EMILY COURT, 8	SUDREY STREET	LONDON	SE1 1PF
FLAT 503		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
FLAT 7	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
FLAT 8	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT
UNIT 2		93 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 13	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	2 WIREWORKS COURT, 79	GREAT SUFFOLK STREET	LONDON	SE1 0BU
	3 WIREWORKS COURT, 79	GREAT SUFFOLK STREET	LONDON	SE1 0BU
FLAT 24		130 WEBBER STREET	LONDON	SE1 0JN
APARTMENT 5	EMILY COURT, 8	SUDREY STREET	LONDON	SE1 1PF
FLAT 4		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 5		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 7		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 8		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 10		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 12		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 13		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 14		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 15		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 17		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 18		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 19		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 20		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 22		130 WEBBER STREET	LONDON	SE1 0JN
GROUND FLOOR AND FIRST FLOOR FLAT		197 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
SECOND FLOOR AND THIRD FLOOR FLAT		197 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
FLAT 47		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 48		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 49		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 50		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 52		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 53		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 54		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 55		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 57		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 58		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 59		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 61		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 62		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 63		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 64		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 66		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 67		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 1		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
FLAT 37		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 38		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 12		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 13		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 14		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 15		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 17		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 18		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 19		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 21		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 22		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 23		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 24		91 GREAT SUFFOLK STREET	LONDON	SE1 0BX
FLAT 1	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
FLAT 10	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
FLAT 2		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 3		130 WEBBER STREET	LONDON	SE1 0JN
FLAT 94		130 WEBBER STREET	LONDON	SE1 0JP
FLAT 96		130 WEBBER STREET	LONDON	SE1 0JP

SINCLAIR JOHNSTON & PARTNERS LTD
HAT-TRICK DESIGN CONSULTANTS LTD

BOURNE BUSINESS CONSULTING LLP

FLAT 97		130 WEBBER STREET	LONDON	SE1 0JP	
FLAT 98		130 WEBBER STREET	LONDON	SE1 0JP	
FLAT 99		130 WEBBER STREET	LONDON	SE1 0JP	
FLAT 1	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
FLAT 2	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
FLAT 3	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
FLAT 5	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
FLAT 6	WINCHESTER HOUSE, 201	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0DT	
FLAT I	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT J	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT K	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT L	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT A	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT A		181 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
FLAT A		183 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
FLAT F	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT B		181 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
FLAT C	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT C		181 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
		128 WEBBER STREET	LONDON	SE1 0QL	
FLAT G	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT E		181 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
FLAT D		181 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED	
FLAT 70		130 WEBBER STREET	LONDON	SE1 0JP	
FLAT H	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU	
FLAT		114 GREAT SUFFOLK STREET	LONDON	SE1 0NS	
CORPORATION OF LONDON COMMUNITY SERVICES DEPARTMENT	REAR OF 156A-176B	GREAT SUFFOLK STREET	LONDON	SE1 1PE	
	FLAT 9	130 WEBBER STREET	LONDON	SE1 0JN	
	FLAT E	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU
	FLAT 4	BITTERN HOUSE LANT ESTATE	BITTERN STREET	LONDON	SE1 1PH
	FLAT 3		199 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
METROPOLITAN SUPPORT TRUST		181-195	SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
		10 WIREWORKS COURT, 79	GREAT SUFFOLK STREET	LONDON	SE1 0BU
	FLAT 405		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT D	VICTORIA BUILDINGS	GREAT SUFFOLK STREET	LONDON	SE1 0BU
		172A	24 SUDREY STREET	LONDON	SE1 1PF
			GREAT SUFFOLK STREET	LONDON	SE1 1PE
			20 SUDREY STREET	LONDON	SE1 1PF
			9 SUDREY STREET	LONDON	SE1 1PF
	FLAT C		183 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0ED
			132 WEBBER STREET	LONDON	SE1 0QL
			17 SUDREY STREET	LONDON	SE1 1PF
		170A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
		176A	GREAT SUFFOLK STREET	LONDON	SE1 1PE
SMITH YEATMAN LTD			103 GREAT SUFFOLK STREET	LONDON	SE1 1PQ
TERRY CAFE			158 GREAT SUFFOLK STREET	LONDON	SE1 1PE
	FLAT 93		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 301		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 8		118 SOUTHWARK BRIDGE ROAD	LONDON	SE1 0BQ
	FLAT 74		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 78		130 WEBBER STREET	LONDON	SE1 0JP
	FLAT 23		130 WEBBER STREET	LONDON	SE1 0JN

London Borough of Southwark



Quietway Cycling Proposals

Site M – Webber Street and Blackfriars Road Junction

Public Consultation Summary

September 2014

London Borough of Southwark

Site M - Quietway Cycling Proposals Webber Street and Blackfriars Road Junction

Public Consultation Summary

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1.0 Introduction

1.1 Background

1.1.1 This document report has been produced by the London Borough of Southwark Public Realm Projects Group, to provide a summary of the consultation exercise for the Quietway Cycling Proposals for Site M on Webber Street and Blackfriars Road junction. The measures are being drafted by the Public Realm Projects Team, with the project manager for this scheme being Chris Mascord (Senior Engineer).

1.1.2 The area under consideration is located within the SE1 district of Southwark (Cathedrals Ward) in the north of the borough. See figure 1 below.

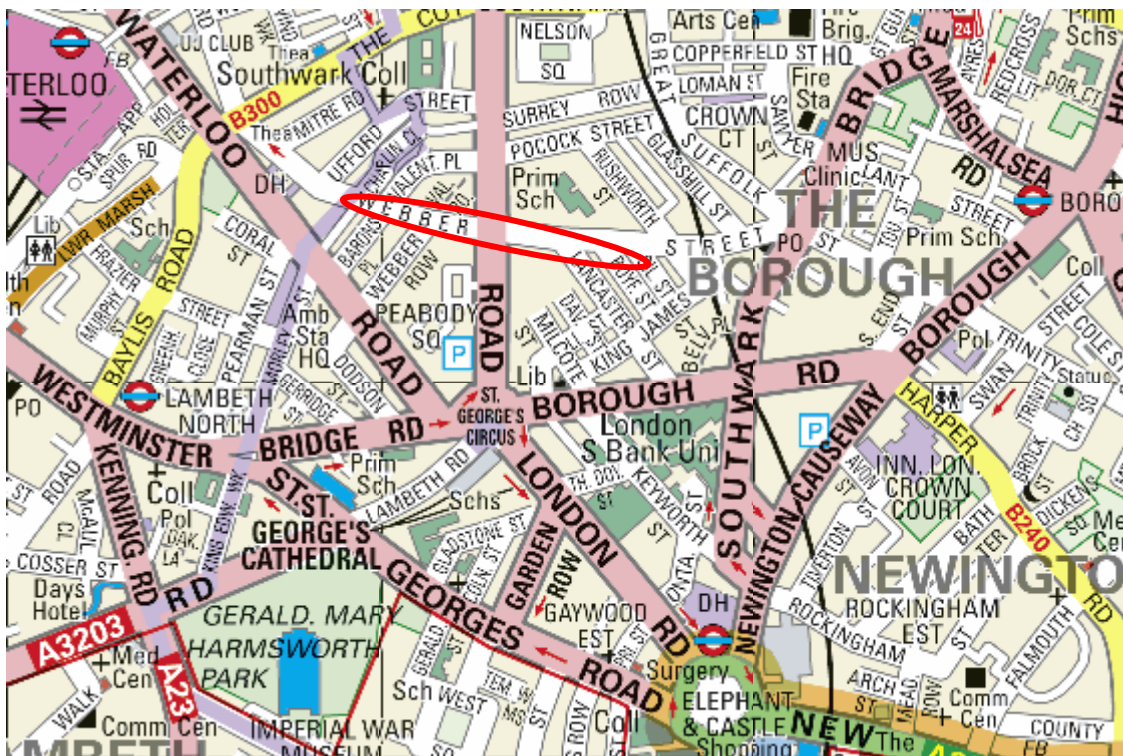


Figure 1: Location of proposed scheme

1.2 Project and Objectives

1.2.1 This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey Station in the east and traversing local roads to The Cut in the west (see figure 2). The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

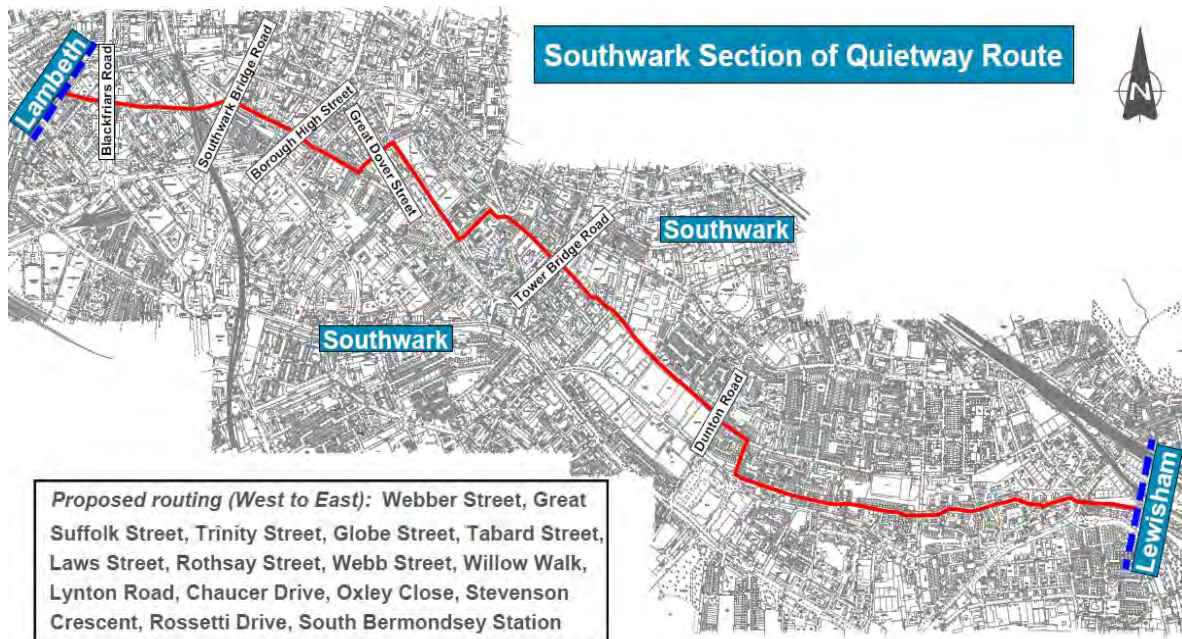


Figure 2: Southwark's section of Quietway Route

- 1.2.2 Quietways are a network of bike routes for less confident cyclists using residential streets with low traffic volumes. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.
- 1.2.3 The measures proposed in this consultation are part of the Council's ongoing commitment to make Southwark's streets safer and more accessible for all. The proposed measures will enhance the environment for all road users, reducing traffic speeds and improving pedestrian safety. Cycling proposals also have the added health benefits of improving the environment through reducing carbon emissions and getting more people onto bikes which in turn enhances their fitness and health.
- 1.2.4 The following measures were consulted upon to improve cycle safety and accessibility and enhance the streetscape for Site M:
- Subject to approval from the DfT and other governing bodies, the Webber Street arms of the Blackfriars Road junction will have an early start cycle pre-signal so cyclists can traverse the junction and undertake turning movements before general traffic. These measures will be implemented in conjunction with Transport for London's Blackfriars Road Cycle Super Highway and streetscape improvements works in Blackfriars Road that will be consulted upon separately.

- Both approaches of Webber Street will have semi segregated cycle lanes so that cyclists can have unobstructed access to the waiting areas on approach to Blackfriars Road.
- Improved advance cycle waiting areas to be provided on both approaches of Webber Street for cyclists to take position ahead to general traffic at the junction.
- Cycle waiting areas and cycle lanes on both approaches of Webber Street to be incorporated onto new raised carriageway tables to reduce traffic speeds and improve safety.
- The carriageway of Webber Street is to be resurfaced and existing sets of speed cushions to be replaced with sinusoidal humps to improve the ride quality for cyclists.
- The streetscape of Webber Street to the west of Blackfriars Road up to the borough boundary with Lambeth will have significant improvements with footway buildouts, additional parking bays and tree planting. Existing carriageway tables at Webber Row and Gray Street will also be improved with new materials to enhance the streetscape.
- A new side road entry table with footway buildouts is proposed in Lancaster Street at the junction of Webber Street to improve pedestrian accessibility and reduce vehicle speeds.
- Existing raised carriageway table and zebra crossing between Silex Street and Boyfield Street to be extended to include Boyfield Street. The larger table will reduce traffic speeds and improve pedestrian accessibility.
- Proposed raised carriageway table in Silex Street at the junction with Webber Street to reduce traffic speeds and improve pedestrian accessibility (one parking space to be removed).

(See Appendix A – Initial Scheme Design)

1.3 Consultation Procedure

- 1.3.1 The views of the local community and those of statutory consultees have been sought, prior to the development of measures to a detailed design stage. Active community participation was encouraged through the use of a consultation document and questionnaire (see Appendix B – Consultation Documents).
- 1.3.2 The consultation document included a covering letter describing the proposals and a request for comments (including information to assist in translation and large print versions of the consultation document), preliminary design drawings (A3 size) and a questionnaire/comment form that could be sent to the Public Realm Projects Group with a pre-paid address reply.
- 1.3.3 The consultation document was delivered to a geographical area centred on Webber Street between Silex Street in the east and the borough boundary with Lambeth in the west, using strategic roads and pedestrian desire lines as defined cut off points (See Appendix C – Location Plan and Extents of Consultation).

- 1.3.4 The distribution area was large enough to gain views from the wider community that may be considered to be affected by the proposed measures. A mailing list was established for the area by way of the Council's GIS database. In addition, the consultation documents and plans were supplied to the Council's established list of statutory consultees including London Buses, cycle groups and the Metropolitan Police. Please see Appendix D of list of addresses within the distribution area. Consultation documents were also delivered to the Southwark Housing Department and Grange Primary School.
- 1.3.5 The consultation documents were delivered by Royal Mail to 866 addresses detailed within the distribution list. The documents were delivered on the 5th August 2014, with a return deadline of the 5th September, allowing 4 weeks for the consultation period. However due to the summer holiday period, responses were accepted online until the 12th September 2014.
- 1.3.6 The proposals were also available to view online using consultation section of the council's website, with an e-form questionnaire provided in order to capture responses.

2.0 Consultation Responses

2.1 Response Rate and Distribution

- 2.1.1 A total of 52 responses were received during the consultation period (48 returned questionnaires and 4 online responses), equating to a 6% response rate.
- 2.1.2 Four responses were received from Statutory Consultees (Southwark Living Streets, Southwark Cyclists, Lambeth Cyclists and Sustrans).

2.2 Questionnaire Analysis

- 2.2.1 The questionnaire element of the consultation document contained the following key questions and associated tick box options:

Q1. Are you a resident or business?

Q2. Do you support the proposals?

- 2.2.2 The following is a summary of replies received:

Question 1 - Are you a resident or business?

	Resident	Business
Replies	49	3
Total	94%	6%

Table 1: Returned questionnaire results for question 1

2.2.3 Table 1 indicates that the majority of responses received throughout the consultation period were from local residents, with only three businesses formally replying.

Question 2 – Do you support the proposals?

	Support	Opposed	No Opinion
Replies	42	10	0
Total	81%	19%	0%

Table 2: Returned questionnaire results for question 2

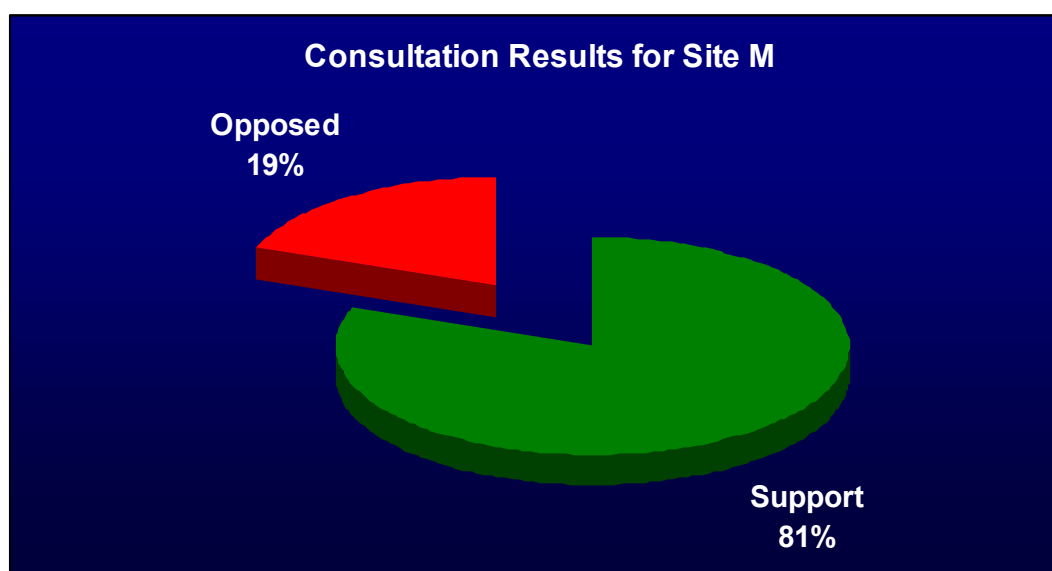


Figure 3: Graphical representation of consultation data for question 2

2.2.4 Table 2 and figure 2 indicate a majority of support for question 2, with 81% welcoming the proposed measures.

2.3 Additional Comments

2.3.1 The questionnaire element of the consultation document invited consultees to attach any additional comments they may have on the proposals when returning the reply-paid questionnaire. Consultees were also able to reply to the consultation online using the council's website.

2.3.2 The majority of respondents (81%) indicated support for the proposed measures, discussed that the improvements are a great idea and many commented that the tree planting and new paving would really make Webber Street look good and can't be implemented soon enough.

2.3.3 A number of respondents commented that they like the proposal to resurface Webber Street, as the surface is currently poor and bumpy.

- 2.3.4 A comment from a local business highlighted that the scheme would be very helpful for their staff the cycle to work.
- 2.3.5 A respondent commented that the plans will improve the local cycling route and help maintain a good flow of traffic.
- 2.3.6 A number of respondents commented that the early start pre-signal sequence at the traffic lights is a good idea that will improve safety for cyclists and will help reduce left turn conflict with cars.
- 2.3.7 A respondent commented that the improvements to the paving along Webber Street will be of great benefit to elderly residents in the area, as they are currently uneven and in some places dangerous.
- 2.3.8 A comment was received stating that if trees are to be planted, then appropriate maintenance is essential, as many of the trees planted in the area recently have died. *

* In response, all new trees will be planted in appropriately sized tree pits and the species will be carefully selected by the council's street tree team. All tree pits will have appropriate nutrient soil to ensure that the new trees can grow and flourish. The tree pits will also have root guards so that no damage will occur to surrounding footways as the trees mature. All trees planted in Webber Street will have a five year maintenance contract with a third party supplier who will be responsible for ensuring the trees receive enough water and nutrients to successfully establish themselves in the streetscape. Following the completion of this maintenance period, the care of the trees will revert back to the council's street care team.

- 2.3.9 A request was made to remove speed bumps as they are annoying for cyclists and cars alike. Cyclists prefer flat roads. *

* In response, although flat carriageway surfaces are ideal for cycling, it is essential to keep vehicle speeds low along Quietway routes to minimise the risk of conflict with cyclists and other road users. Whilst all streets in the area are 20mph, vertical deflection measures are effective to ensure motor vehicles comply with the speed limit. Removing these measures may result in increased vehicle speeds which would adversely affect the safety of vulnerable road users such as pedestrians and cyclists.

Where possible, existing sets of speed cushions are being replaced with sinusoidal road humps that can easily be traversed by cyclists and still effective in reducing motor vehicle speeds. This in conjunction with resurfacing the carriageway will provide much better ride quality for cyclists.

- 2.3.10 A respondent raised an issue that planning permission has been granted for the demolition of 27-31 Webber Street and to be aware that more HGV's will access the area throughout the development stage, which may endanger cyclists. *

* In response, the developer is required to provide a full traffic management plan and risk assessment to mitigate risks to road users throughout the construction period. The plans will be assessed by the council's network management team to

ensure that construction traffic will not adversely impact on the operation of Webber Street.

- 2.3.11 A comment was received stating that businesses on the northern side of Webber Street to the west of Blackfriars Road will need loading. *

* In response, no comments were received from these businesses requesting further loading provision in Webber Street. However as part of the scheme, a 12m parking bay is proposed adjacent to these businesses. It is likely that the bay will operate as a loading bay during daytime periods to cater for loading activities and revert to unrestricted resident parking at night.

- 2.3.12 A number of comments highlighted that residents have lost many parking places in the area which is causing great difficulty and it must be a priority to keep as much parking as possible. *

* In response, there is no net loss of parking proposed as part of the Webber Street proposals. There is one existing parking space being removed in Silex Street to introduce a raised table that will improve safety and pedestrian accessibility at the junction with Webber Street, but three additional spaces being introduced on the southern side of Webber Street to the west of the Blackfriars Road junction. All other existing resident permits bays on public highway are to be retained.

- 2.3.13 A request was made to ensure that the traffic lights at the Webber Street / Blackfriars Road junction are properly regulated to allow for traffic movement without traffic queues building on either side of Webber Street. *

* In response, the proposals are being fully modelled to ascertain the most efficient operational phasing of the traffic signals to ensure that oversaturation of junction is minimised. The signals will be linked to other signal junctions in the area which will enable real time changes to cycle times if traffic builds up on certain arms of the junction at any given period. This will minimise the likelihood of delays to traffic, especially in peak periods.

- 2.3.14 A request was made for a contra-flow cycle lane down the entire length of Webber Street. *

* In response, a contra-flow cycle lane can only be introduced on one way streets. Webber Street caters for two way traffic and therefore is not suitable for a contra-flow cycle.

- 2.3.15 A request was made to resurface all side roads leading off Webber Street as they are currently in very poor condition and have not been resurfaced for at least 14 years. *

* In response, Quietway funding can only be utilised to improve conditions for cycling and the adjacent streetscape on the prescribed route. Therefore the council is unable to consider the request to resurface the carriageway of adjacent side roads as part of the current proposals. However these comments will be passed to the council's Highway Maintenance Team in order to be considered as part of future non-principal road renewal programmes.

2.3.16 Analysis of the additional comments from respondents that objected to the scheme highlighted the following concerns:

An objection was received stating the proposals are making the road narrower and installing cycle lanes, instead of keeping the road width as it is. *

* In response, the proposed footway buildouts are the same width as existing parking bays and therefore the proposals do not result in narrowing adjacent running lane widths. The proposed semi-segregated cycle lane is positioned along the existing kerbline where there currently is ample to accommodate both a waiting vehicle and an unobstructed eastbound approach lane. Therefore the cycle lane does not compromise the width of the eastbound general traffic lane on approach to the Blackfriars Road junction. The cycle access lane on the western approach of Webber Street is being introduced by utilising a section of footway and therefore does not impact on existing carriageway width. It must be noted that the width of the existing footway to the rear of the cycle lane is still adequate for the level of pedestrian footfall at this location.

Building out the footways the width of the existing and proposed parking bays addresses potential conflict issues with cyclists riding adjacent to the kerbline then having to traverse out into the general traffic lane to cycle past parking bays. The footway buildouts effectively inset the parking bays into the footway so that cyclists can take a consistent, linear line along the carriageway, taking appropriate positioning with general traffic flows. The footway buildouts also improve the streetscape, by reducing the dominance of the carriageway and allowing opportunity to improve the streetscape through tree planting, cycle parking and seating, as well as increasing footway areas for pedestrians.

A respondent objected on the grounds that the council is pandering to cyclists who pay no road tax or congestion charge. They should not be given special treatment at our cost. *

* In response, cycling numbers are increasing year on year and it is essential to ensure that appropriate safety and accessibility measures are implemented on public highway to cater for this growing, sustainable mode of transport.

Cyclists are vulnerable road users and like pedestrians, require measures that allow safe passage along the carriageway and reduce the risk of conflict with other road users. With the population in London increasing faster than the rate of expansion of both rail and road networks, catering for more sustainable modes of transport, such as cycling is critical to keep London moving, reducing congestion on an already saturated network and minimising carbon emissions which has a quantifiable benefit for all.

The funding for the project has been ring fenced by the Mayor of London to be spent specifically on cycling and does not impact on council revenue or quality of service delivery.

Numerous respondents objected commenting the majority of the works were unnecessary and that there are more important things the council should be spending money on than a bike route. *

* In response, the proposed measures along the Quietway route align with the council's emerging cycling strategy and the Mayor's Vision for Cycling. The Mayor has commitment to invest total of £913m over the next 10 years in cycling safety and infrastructure development to significantly increase the modal share in cycling as a safe, healthy and sustainable form of transport in London.

The London boroughs will play a central role in delivering this vision, by helping to develop, fund and deliver better and safer routes for cycling.

The council welcomes significant investment from Transport for London to take forward the Quietway programme that will not only significantly upgrade a number of existing cycle routes, but also make considerable improvements to the streetscape for the benefits of all road users and local residents.

As outlined above, the project is being externally funded by the Mayor of London and not the council. The council is unable to spend the funding on any other measures or improvements that are not directly linked to or part of the proposed Quietway cycle route.

A number of respondents objected stating they do not see a problem with the Blackfriars Road junction and that they do not favour losing residential parking spaces which will result in problems, especially at the weekends. *

* In response, the Blackfriars Road junction has high north / south traffic volumes and is daunting for cyclists to traverse this is especially pertinent for new, less confident cyclists who may be put off using junction due to fear of conflict with other road users. The majority of serious collisions involving cyclists take place at signal junctions and with cycling numbers in London increasing and the provision of the Quietway programme encouraging cyclists to traverse this particular route, it is essential that all existing signal junctions on the route are upgraded to improve safety and reduce the risk of collisions.

The greatest risk to cyclists using this junction is potential conflict from motor vehicles turning left across the path of cyclists traversing straight ahead. Therefore the advanced cycle awaiting areas on carriageway tables in conjunction with early start pre-signal measures practically eliminate this risk, making the Webber Street / Blackfriars Road junction routing both attractive and safe to cyclists of all abilities.

Due to narrow carriageway width of Webber Street, cyclists currently struggle to access the existing advanced cycle awaiting area on the eastern arm of the junction. Provision of the semi-segregated cycle lane will address this issue and provide unobstructed access for cyclists to the advanced cycle waiting area.

As outlined earlier, there is no net loss of resident parking bays in Webber Street as a result of these proposals. Therefore the kerbside parking availability is not compromised.

An objection was received highlighting that cyclists ignore red lights and crossing the Blackfriars Road junction as a pedestrian is like dicing with death. Also the semi-segregated cycle lane will cause a lot of problems. *

* In response, there is no evidence to suggest that cyclists pose a safety risk for pedestrians crossing the carriageway at this location. Cyclists are subject to the same traffic regulations that motor vehicles adhere to on the carriageway. The Blackfriars Road / Webber Street junction is fully signalised with an 'all green' pedestrian phase so there is minimal risk of conflict between pedestrians and other road users.

It can also be argued that the proposed changes to the junction as part of the Quietway scheme and implementation of the Cycle Super Highway on Blackfriars Road will significantly upgrade the junction, giving cyclists priority and reducing the risk of left turn conflicts. This could reduce potential contraventions by cyclists at the junction who may currently position themselves on the far side of pedestrian crossings to reduce the risk from left turning traffic when a green signal is operational. Introducing the early start cycle phase and enlarged cycle waiting areas on both arms of Webber Street will potentially address this issue.

There is no evidence to suggest that the proposed semi-segregated cycle lanes will result in problems for other road users. The cycle lanes are an essential measure that will provide cyclists unobstructed access to the advanced cycle waiting areas past queuing traffic. As discussed earlier, the carriageway widths of the adjacent general traffic lanes are not compromised as a result of implementing the cycle lanes. In addition, the cycle lanes will also be reviewed as part of the scheme road safety audit to ensure that there are no adverse as a result of their implementation.

An objection was received stating too much traffic uses Webber Street for a Quietway route and the measures should go further by closing the street to through traffic. *

* In response, the traffic volumes using Webber Street is low compared to other roads in the area. The greatest risk to cyclists using this section of the route is potential conflict from motor vehicles turning across the path of cyclists traversing straight ahead at the Blackfriars Road junction. The introduction of advanced cycle awaiting areas in conjunction with early start pre-signal measures will eliminate this risk.

Whilst closing Webber Street to through traffic would improve the Quietway route further, a comprehensive feasibility study would be required to ascertain the likelihood of traffic displacement onto other residential streets in the area. Due to the controversial nature of preventing through traffic and the potential access changes to residential and commercial properties, a separate consultation specifically on this proposal would be required. However, as outlined above, Webber Street traffic volumes and speeds are low and the risk of conflict with cyclists is minimal.

2.3.9 18% respondents did not submit a further comment.

2.4 Levels of Consensus

2.4.1 The following majority levels of agreement have been given in relation to the questions contained within the consultation document:

- 81% of respondents support the introduction of the Quietway cycle route proposals for Site M;
- 19% of respondents are opposed to the proposals.

2.5 Statutory Consultee Replies

2.5.1 Four statutory consultees provided a reply to the consultation.

- a) **Southwark Living Streets** replied indicating strong support for the scheme and highlighted that the proposals had benefits built in for residents, cyclists and pedestrians. It was also mentioned that the semi-segregated cycle lanes leading to the advanced cycle waiting areas were excellent. New street trees and footway buildouts will really enhance the streetscape.
- b) **Lambeth Cyclists** replied in support of the scheme and that the final design ensures the elephant footprint markings across Blackfriars Road should lead to a safe riding position in the subsequent lane. This will further reduce left hook risk. It was also mentioned that the single yellow line on Webber Street between Webber Row and Gray Street should be made double with no loading due to narrow width. A suggestion was also made for extending the footway across the mouth of Friars Primary School car park entrance.

* In response, the consultation drawings is only indicative and as part of the scheme detailed design process, the markings will be placed appropriately in the carriageway to maximise the safety benefit for cyclists using this junction.

The council will investigate whether it is feasible to introduce double yellow line parking controls on the south side of Webber Street between Webber Row and Gray Street. Any proposed changes must be subject to the statutory consultation process and there would a risk that residents will object, as the single yellow line can potentially be parked on in evening periods outside the hours of the controlled parking zone operation.

The suggestion to extend the footway surfacing over the vehicular entrance in the adjacent primary school will be considered as part of the detailed design process.

- c) **Southwark Cyclists** replied indicating support for the scheme and indicated the biggest risk for cyclists using the route along Webber Street is the risk of from left turning motor vehicles traffic turning across the path of a cyclists going straight ahead. Site observations and verifying traffic counts concluded that at peak times approximately 12 vehicles turn left out of both arms of Webber Street and therefore there is not a major left turn demand at this junction. The proposed cycle early start signals will help to minimise this risk further and are a beneficial addition to this junction.

- d) **Sustrans** replied indicating support for the scheme but did not provide any specific questions or comments on the proposed measures.

2.5.2 No objections were received from Ward Members throughout the consultation period. A series of comments was received from Cllr Adele Morris who highlighted the following;

- There are cars frequently parked on the single yellow line on the south side of Webber Street to the west of Webber Row that narrow the road significantly.
- Vehicles also regularly park on single yellow lines between Boyfield Street and Blackfriars road which also constrains carriageway width.
- Across both sections there must no net loss of parking spaces which are very well used by residents.
- Noted the proposed Valentine Place development and need to coordinate both sets of works on site to minimise disruption and to avoid new footway paving or tree planting from being ruined.
- Highlighted the unsuccessful tree planting that took place on the southern side of Webber Street to the west of Blackfriars Road.

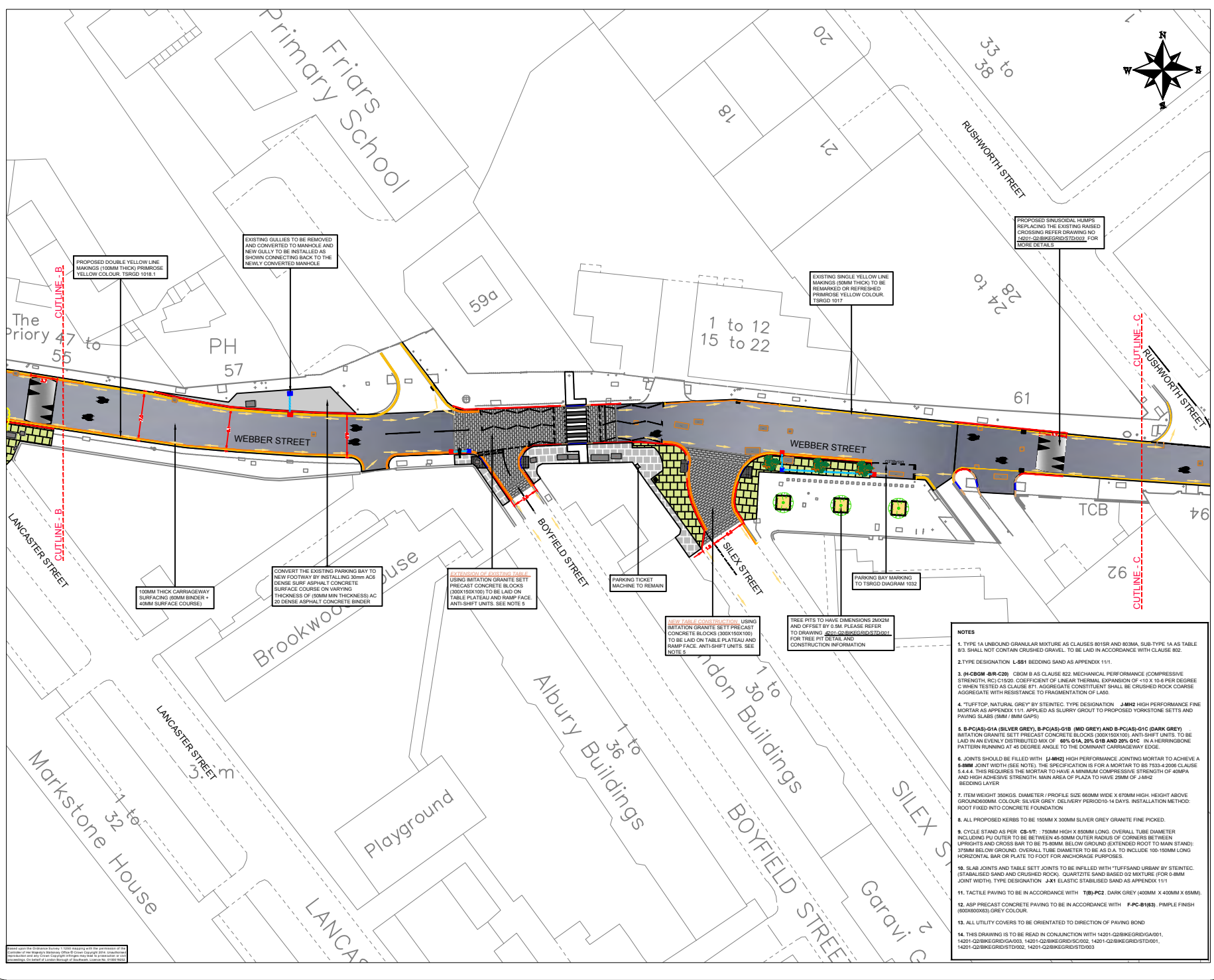
3.0 Recommendations

Due to the majority of respondents supporting the scheme and Southwark's on-going commitment to improve and promote cycling in the borough, it is recommended that the scheme proceed to implementation.

Appendices

Appendix A:	Initial Scheme Design
Appendix B:	Consultation Documents
Appendix C:	Location Plan and Extents of Consultation
Appendix D:	List of Addresses within the Distribution Area

Appendix A: Initial Scheme Design



LEGEND / NOTES.

KEY:

- CARRIAGEWAY RECONSTRUCTION:** 40mm THICK T8CS ST10 P8V 68- LAD CARBON THICK 30A T8CS 40mm THICK T8CS TO GULLY FOR RAISED TABLE, ROUND TOP HUMP OR CARRIAGEWAY REPAIR CONSTRUCTION.
- FOOTWAY RECONSTRUCTION HEAVY OVERSIL AERIAL:** PROVIDE LAY PCC FINISH T8CS 100 X 750mm HIGH TUBES FOR FOOTWAY CONSTRUCTION LAD ON 200mm THICK TYPE 1 SUBBASE.
- FOOTWAY RECONSTRUCTION LIGHT OVERSIL AERIAL:** PROVIDE LAY PCC FINISH T8CS 100 X 750mm HIGH TUBES FOR FOOTWAY CONSTRUCTION LAD ON 150mm THICK TYPE 1 SUBBASE.
- RAISED TABLE CONSTRUCTION:** PROVIDE AND LAY ANTI-SHIFT PCC BLOCKS 100 X 100mm PORTABLE TABLE CONSTRUCTION LAD ON 300mm THICK CBGM-C20 AND 30mm THICK LOW FINES BEDDING SAND.
- BLISTER PAVING:** PROVIDE AND LAY PCC BLISTER TACTILE PAVERS 400 X 400 X 25mm THICK (REF T8CS/ST10) ON SPECIFIED FOOTWAY CONSTRUCTION LAD ON 200mm THICK CBGM-C20 SUBBASE AND 30mm THICK TUFFED LAYING MORTAR BY STENDEC.
- IRONWORK:** EXISTING IRONWORK COVER AND FRAME TO BE ADJUSTED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INSTALL FLAT KERBS 150 X 300MM - UNITS SILVER GREY.
- STANDARD GRANITE KERBS:** INSTALL NEW STANDARD GRANITE KERBS (150MM X 300MM) LAD FLUSH ON ST4 INCLUDING FRONT KERBS HANGING TO BE 100MM BELOW CARRIAGEWAY - REFER TO DRAWING L83-D10 KERBS AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1(SHEET 1 OF 4) & TYPE R - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE KERBS FLUSHED:** INSTALL NEW STANDARD GRANITE KERBS (150MM X 300MM) LAD FLUSH ON ST4 INCLUDING FRONT KERBS HANGING TO BE 100MM BELOW CARRIAGEWAY - REFER TO DRAWING L83-D10 KERBS AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1(SHEET 1 OF 4) & TYPE R - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE TRANSITION KERBS:** NEW STRAIGHT KERB TYPE T1 (SHEET 3 OF 4) TRANSITION KERBS TYPE T1 (SHEET 3 OF 4) REFER TO DRAWING L83-D10 KERBS AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1(SHEET 1 OF 4) & TYPE R - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- NEW WHITE LINE ROAD MARKING:** EXISTING YELLOW LINE ROAD MARKING TO BE REMARKED / REFRESHED.
- NEW YELLOW LINE MARKING:** TO BE INSTALLED AS PART OF THE PROPOSED TRAFFIC REGULATION ORDER.
- RED ROAD MARKINGS:** TO BE REINSTALLED.
- DRAIN PIPE:** PROVIDE AND INSTALL SURFACE WATER DRAIN PIPE GROUP 1 - 75mm INTERNAL DIAMETER DEPTH OF INVERT 100mm. AVERAGE DEPTH OF 750mm ON BED TYPE 2. DEPTH OF 750mm ON BED TYPE 2.
- TRAPPED GULLY:** PROVIDE AND INSTALL PRECAST CONCRETE TRAPPED GULLY WITH TYPE D COVER. GULLY DEPTH = 750mm OR BUILD CATCH PITS REFER SPECIFIED.
- EXISTING ROAD GULLY:** TO BE RAISED AND ADJUSTED.
- NEW LAMP COLUMN LOCATION:** EXISTING LAMP COLUMN LOCATION.
- TREE PIT CONSTRUCTION:** NEW TREE PIT CONSTRUCTION (REFER TO STANDARD DRAWING DETAILS NO 14201-Q2BIKEGRIDSTD001).
- EXISTING / PROPOSED LONGITUDINAL FALL ON CARRIAGEWAY:** INSTALL NEW SINUSOIDAL HUMP: 40 THICK S3H P8V 68- AS APPENDIX T1 WITH B14 OR 14201-PC2. REGULATING INVERT COURSE BEH AS APPENDIX T1. (REFER TO STANDARD DRAWING DETAILS NO 14201-Q2BIKEGRIDSTD003).
- PROPOSED LOCATION FOR THE SIGN POSTS**

NOTE:
THIS DRAWING IS TO BE READ IN CONJUNCTION WITH:
14201-Q2BIKEGRIDSTD001
14201-Q2BIKEGRIDSTD002
14201-Q2BIKEGRIDSTD003
14201-Q2BIKEGRIDSTD004

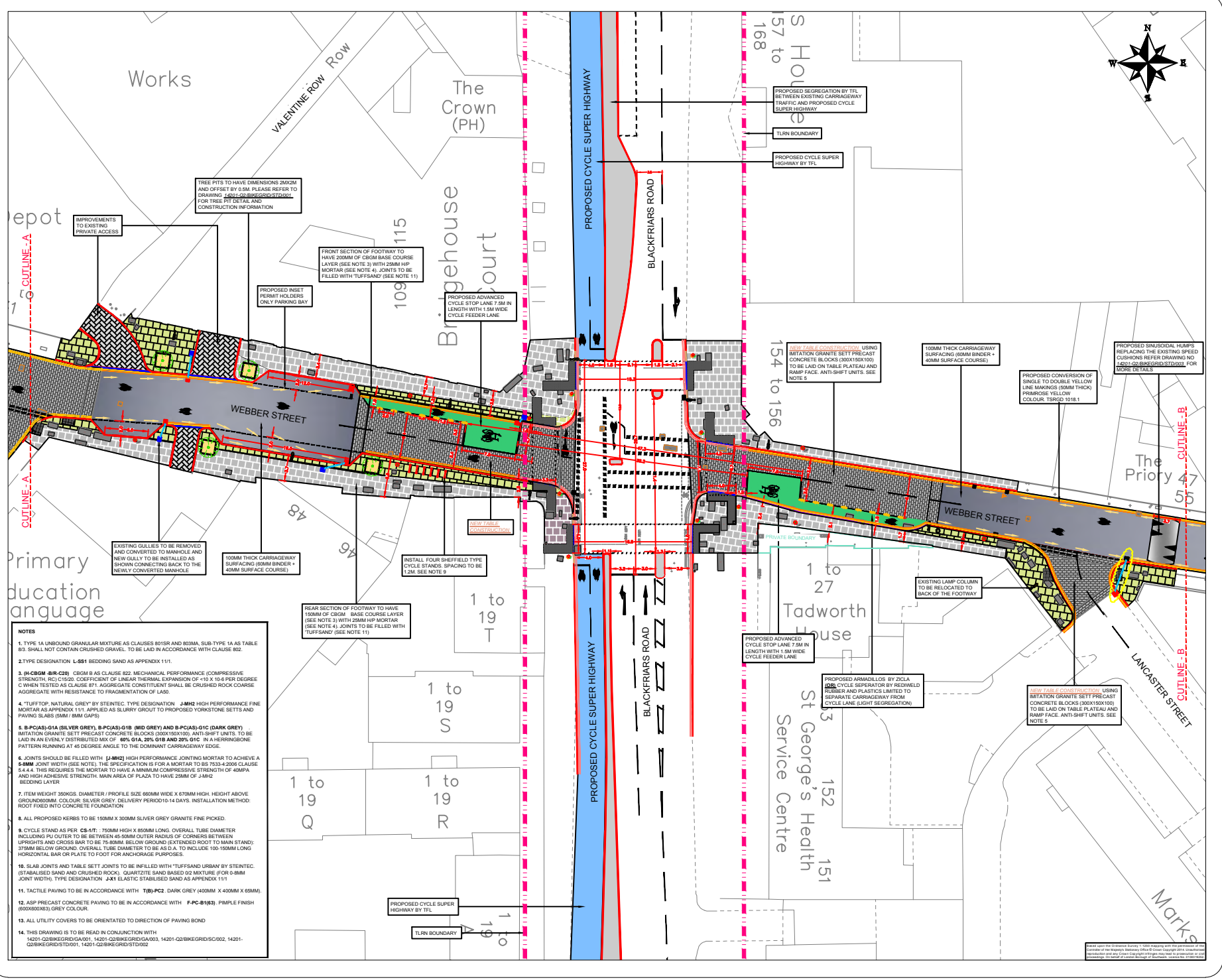
Revision	Date	Amendment	Drawn	Design	Checked	Approved

- ### NOTES
- TYPE 1A UNBOUND GRANULAR MIXTURE AS CLAUSES 8018 AND 803A. SUB-TYPE 1A IS TABLE B/3. SHALL NOT CONTAIN CRUSHED GRAVEL. TO BE LAID IN ACCORDANCE WITH CLAUSE 802.
 - TYPE DESIGNATION L-881 BEDDING SAND AS APPENDIX 11/1.
 - 14-CBGM-B-R-C20 CBGM B AS CLAUSE 822 MECHANICAL PERFORMANCE (COMPRESSIVE STRENGTH RCI C1920, COEFFICIENT OF LINEAR THERMAL EXPANSION OF -11.0 X 10⁻⁶ PER DEGREE C WHEN TESTED AS CLAUSE 87). AGGREGATE CONSTITUENT SHALL BE CRUSHED ROCK COURSE AGGREGATE WITH RESISTANCE TO FRAGMENTATION OF L450.
 - TUFF TOP, NATURAL GREY BY STENDEC. TYPE DESIGNATION J-MH2 HIGH PERFORMANCE FINE MORTAR AS APPENDIX 11/1. APPLIED AS SLURRY GROUT TO PROPOSED YORKSTONE SETTS AND PAVING SLABS (50mm / 50mm GAPS).
 - B-PC(A5)-G1A (SILVER GREY), B-PC(A5)-G1B (MD GREY) AND B-PC(A5)-G1C (DARK GREY) IMITATION GRANITE SETT PRECAST CONCRETE BLOCKS (300X150X100) ANTI-SHIFT UNITS. TO BE LAID IN AN EVENLY DISTRIBUTED MIX OF 60% G1A, 20% G1B AND 20% G1C. IN A HERRINGBONE PATTERN RUNNING AT 45 DEGREE ANGLE TO THE DOMINANT CARRIAGEWAY EDGE.
 - JOINTS SHOULD BE FILLED WITH J-MH2 HIGH PERFORMANCE JOINTING MORTAR TO ACHIEVE A 6-8MM JOINT WIDTH (SEE NOTE). THE SPECIFICATION IS FOR A MORTAR TO BS 7324-2:2006 CLAUSE 5.4.4. THIS REQUIRES THE MORTAR TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 40MPa AND HIGH ADHESIVE STRENGTH. MAIN AREA OF PLAZA TO HAVE 25MM OF J-MH2 BEDDING LAYER.
 - ITEM WEIGHT 35KGS. DIAMETER / PROFILE SIZE 600MM WIDE X 670MM HIGH HEIGHT ABOVE GROUND 600MM. COLOUR: SILVER GREY. DELIVERY PERIOD 10-14 DAYS. INSTALLATION METHOD: ROOT FIXED INTO CONCRETE FOUNDATION.
 - ALL PROPOSED KERBS TO BE 150MM X 300MM SILVER GREY GRANITE FINE PIPED.
 - CYCLE STAND AS PER CS-117: 750MM HIGH X 800MM LONG. OVERALL TUBE DIAMETER INCLUDING PU OUTER TO BE BETWEEN 45-50MM OUTER RADIUS OF CORNERS BETWEEN UPRIGHTS AND CROSS BAR TO BE 75-80MM BELOW GROUND (EXTENDED ROOT TO MAIN STAND); 370MM BELOW GROUND. OVERALL TUBE DIAMETER TO BE AS D.A. TO INCLUDE 100-150MM LONG HORIZONTAL BAR OR PLATE TO FOOT FOR ANCHORAGE PURPOSES.
 - SLAB JOINTS AND TABLE SETT JOINTS TO BE FILLED WITH TUFFSAND URBYAN BY STENDEC. (STABILISED SAND AND CRUSHED ROCK). QUARTZITE SAND BASED 02 MIXTURE (FOR 8-8MM JOINT WIDTH). TYPE DESIGNATION J-X1 ELASTIC STABILISED SAND AS APPENDIX 11/1.
 - TACTILE PAVING TO BE IN ACCORDANCE WITH T1(B)-PC2 DARK GREY (400MM X 400MM X 65MM) (600X600MM) GREY COLOUR.
 - ASP PRECAST CONCRETE PAVING TO BE IN ACCORDANCE WITH F-PC-B1(63). PIMPLE FINISH (600X600MM) GREY COLOUR.
 - ALL UTILITY COVERS TO BE ORIENTATED TO DIRECTION OF PAVING BOND.
 - THIS DRAWING IS TO BE READ IN CONJUNCTION WITH 14201-Q2BIKEGRIDSTD001, 14201-Q2BIKEGRIDSTD002, 14201-Q2BIKEGRIDSTD003, 14201-Q2BIKEGRIDSTD004, 14201-Q2BIKEGRIDSTD005, 14201-Q2BIKEGRIDSTD006.

PUBLIC REALM PROJECTS TEAM 160 TROLEY STREET SE1P 9LX

Project	CENTRAL LONDON BIKE GRID Q2 - BLUE ROUTE		
Title	GENERAL ARRANGEMENT SECTION-3 EAST OF BLACKFRAIRS ROAD JUNCTION		
Contract No.	HW2013 NEC3		
Scale	1:500 @ A1	Purpose	CONSTRUCTION
Drawing No.	14201-Q2BIKEGRID/GA003	Rev.	-
Date Drawn	JUNE 2014	Date Issue	JULY 2014

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LEGEND / NOTES.

KEY:

- CARRIAGEWAY RECONSTRUCTION:** 40mm THICK TS10 ST10 PEV 68-LAD ON 50MM THICK SMA 14 BIT 4098 GR WTR 2 TO CL387 FOR RAISED TABLE. ROUNDO TOP HUMPS ON CARRIAGEWAY REPAIR CONSTRUCTION.
- FOOTWAY RECONSTRUCTION HEAVY OVERSILT AREAS:** PROVIDE AND LAY PVC FINISH FLOOR 100 TO 105mm THICK FIBRE FOR FOOTWAY CONSTRUCTION LAID ON 200mm THICK TYPE 1 SUBBASE.
- FOOTWAY RECONSTRUCTION LIGHT OVERSILT AREAS:** PROVIDE AND LAY PVC FINISH FLOOR 100 TO 105mm THICK FIBRE FOR FOOTWAY CONSTRUCTION LAID ON 150mm THICK TYPE 1 SUBBASE.
- RAISED TABLE CONSTRUCTION:** PROVIDE AND LAY ANTI-SHIFT PC BLOCKS 300X150X100 FOR RAISED TABLE CONSTRUCTION LAID ON 350mm THICK CBGM-C20 AND 300mm THICK LOW FINES BEDDING SAND.
- TABLET PAVING:** PROVIDE AND LAY PVC BUTLER TACTILE PAVING 400X400X20mm THICK (REF T18-NSV(7)) ON SPECIFIED FOOTWAY CONSTRUCTION LAID ON 200mm THICK CBGM-C20 SUBBASE AND 300mm THICK TUFFSAND LAYING MORTAR BY STENTEC.
- ALL CYCLE LANE:** PROPOSED CYCLE LANE / ADVANCED STOP LINE (GREEN COLOUR IS FOR INDICATIVE PURPOSE ONLY)
- PROPOSED CYCLE SUPER HIGHWAY BY TRANSPORT FOR LONDON**
- IRONWORK:** EXISTING IRONWORK COVER AND FRAME TO BE ADJUSTED TO MATCH NEW LEVELS IN FOOTWAYS AND CARRIAGEWAYS. PROVIDE AND INSTALL FLAT REB 150 x 300mm UNITS SILVER GREY.
- STANDARD GRANITE KERBS:** INSTALL NEW STANDARD GRANITE KERBS (T80M X 300M) LAID WITH 100MM UPSTAND ON ST4 (INCLUDING FRONT KERB HALUNGS) TO BE 100MM BELOW CARRIAGEWAY. REFER TO DRAWING LBS-C10 KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1 (SHEET 1 OF 4) & TYPE R - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS.
- STANDARD GRANITE KERBS FLUSHED:** INSTALL NEW STANDARD GRANITE KERBS (T80M X 300M) LAID WITH 100MM UPSTAND ON ST4 (INCLUDING FRONT KERB HALUNGS) TO BE 100MM BELOW CARRIAGEWAY. REFER TO DRAWING LBS-C10 KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1 (SHEET 1 OF 4) & TYPE R - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS. NEW PRIMARY TRAFFIC SIGNAL POST LOCATION.
- STANDARD GRANITE TRANSITION KERBS:** NEW STRAIGHT KERB TYPE T18 (T80M X 300M) TRANSITION KERBS TYPE K16-119(S-D1). REFER TO DRAWING LBS-C10 KERB AND EDGE RESTRAINT FOUNDATION DETAIL TYPE M1 (SHEET 1 OF 4) & TYPE R - TYPE T1 (SHEET 3 OF 4) FOR FURTHER DETAILS. NEW WHITE LINE ROAD MARKING. NEW YELLOW LINE ROAD MARKING. RED ROAD MARKINGS TO BE REINSTALLED.
- DRAIN PIPE:** PROVIDE AND INSTALL SURFACE WATER DRAIN PIPE GROUP 6, 150mm INTERNAL DIAMETER. DEPTH OF INVERT+0.00. AVERAGE DEPTH OF 700MM ON BED TYPE 2. NEW WHITE LINE ROAD MARKING.
- TRAPPED GULLY:** PROVIDE AND INSTALL PRECAST CONCRETE TRAPPED GULLY WITH TYPE D COVER. GULLY DEPTH 420mm OR BUILD CATCH PIT WHERE SPECIFIED. EXISTING ROAD GULLY TO BE CONVERTED TO MANHOLE. EXISTING ROAD GULLY TO BE RAISED AND ADJUSTED. NEW CYCLE SEPARATOR OR ARMADILLOS. NEW TRAFFIC SIGNAL POST LOCATION (PRIMARY/SECONDARY). NEW LAMP COLUMN LOCATION. EXISTING LAMP COLUMN LOCATION. NEW SHEFFIELD CYCLE STANDS. TREE PIT CONSTRUCTION: NEW TREE PIT CONSTRUCTION (REFER TO STANDARD DRAWING DETAILS NO 14201-Q2BKEGRD(STD001)). EXISTING (PROPOSED) LONGITUDINAL FALL ON CARRIAGEWAY. EXISTING BELL BOLLARD TO BE RELOCATED AS SHOWN IN THE DRAWING. TURN BOUNDARY. INSTALL NEW SINUSOIDAL HUMPS: 45 THICK SMA PSV 68-AS APPENDIX 7/1 WITH BIT OR 14202 PCC. REGULATING BINDER COURSE SMA AS APPENDIX 7/1. REFER TO STANDARD DRAWING DETAILS NO 14201-Q2BKEGRD(STD003).
- NEW ACCESS IMPROVEMENTS:** LIGHT GREY PCC BLOCKS (REF TO T18-NSV(7)) FINISH FLOOR FINISHED WITH 20MM JESS SHARP SAND ON 50MM THICK BIFA ACTA DENSITE 610 100195 (REFER TO STANDARD DRAWING DETAILS NO 14201-Q2BKEGRD(STD003)).
- PROPOSED LOCATION FOR THE SIGN POSTS:**

NOTE:
THIS DRAWING IS TO BE READ IN CONJUNCTION WITH:
14201-Q2BKEGRD(STD002)
14201-Q2BKEGRD(STD001)
14201-Q2BKEGRD(STD003)
14201-Q2BKEGRD(STD003)

Revision	Date	Amendment	Drawn	Checked	Approved

PUBLIC REALM PROJECTS TEAM 150 TOLEY STREET SE1P 6LX

Project
CENTRAL LONDON BIKE GRID
Q2 - BLUE ROUTE

Title
GENERAL ARRANGEMENT
SECTION-2
BLACKFRIARS ROAD JUNCTION

Contract No.	HW2013 NEC3	Drawn	CH
Scale	1:500 @ A1	Checked	CH
Drawing No.	14201-Q2/BK/GRD/GA/002	Approved	CH
Date Drawn	JUNE 2014	Date Issued	JULY 2014

- NOTES**
- TYPE 1A UNBOUND GRANULAR MIXTURE AS CLAUSES 801R AND 803MA, SUB-TYPE 1A AS TABLE 80. SHALL NOT CONTAIN CRUSHED GRAVEL. TO BE LAID IN ACCORDANCE WITH CLAUSE 802.
 - TYPE DESIGNATION L-S11 BEDDING SAND AS APPENDIX 11/1.
 - (H-CBM) (R-C20) CBGM B AS CLAUSE 822. MECHANICAL PERFORMANCE (COMPRESSIVE STRENGTH), ρ_{10} (COEFFICIENT OF LINEAR THERMAL EXPANSION OF $+10 \times 10^{-6}$ PER DEGREE C WHEN TESTED AS CLAUSE 871. AGGREGATE CONSTITUTION SHALL BE CRUSHED ROCK COARSE AGGREGATE WITH RESISTANCE TO FRAGMENTATION OF L40).
 - TUFFTOP: NATURAL GREY BY STENTEC. TYPE DESIGNATION J-MHZ HIGH PERFORMANCE FINE MORTAR AS APPENDIX 11/1. APPLIED AS SILVER GROUT TO PROPOSED YORKSTONE SETTS AND PAVING SLABS (50MM / 80MM GAPS).
 - B-PC(A)-G1A (SILVER GREY), B-PC(A)-G1B (MID GREY) AND B-PC(A)-G1C (DARK GREY). IMITATION GRANITE SETT PRECAST CONCRETE BLOCKS (300X150X100). ANTI-SHIFT UNITS. TO BE LAID IN AN EVENLY DISTRIBUTED MIX OF 80% G1A, 20% G1B AND 20% G1C. IN A HERRINGBONE PATTERN RUNNING AT 45 DEGREE ANGLE TO THE DOMINANT CARRIAGEWAY EDGE.
 - JOINT WIDTH (SEE NOTE 1). SPECIFICATION FOR A MORTAR TO BS 7533-2:2008 CLAUSE 5.4.4.4. THIS REQUIRES THE MORTAR TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 40MPa AND HIGH ADHESIVE STRENGTH. MAIN AREA OF PLAZA TO HAVE 25MM OF J-MHZ BEDDING LAYER.
 - ITEM WEIGHT 350KGS. DIAMETER / PROFILE SIZE 600MM WIDE X 670MM HIGH. HEIGHT ABOVE GROUND 100MM. COLOUR: SILVER GREY. DELIVERY PERIOD 10-14 DAYS. INSTALLATION METHOD: ROOT FIXED INTO CONCRETE FOUNDATION.
 - ALL PROPOSED KERBS TO BE 150MM X 300MM SILVER GREY GRANITE FINE PICKED.
 - CYCLE STANDS AS PER CB-171. 750MM HIGH X 850MM LONG. OVERALL TUBE DIAMETER INCLUDING PLI OUTER TO BE BETWEEN 65-90MM OUTER RADII OF CORNERS BETWEEN UPRIGHTS AND CROSS BAR TO BE 75-90MM. BELOW GROUND (EXTENDED ROOT TO MAIN STAND); 375MM BELOW GROUND. OVERALL TUBE DIAMETER TO BE AS D.A. TO INCLUDE 100-150MM LONG HORIZONTAL BAR OR PLATE TO FOOT FOR ANCHORAGE PURPOSES.
 - SLAB JOINTS AND TABLE SETT JOINTS TO BE INFILLED WITH TUFFSAND URBAN BY STENTEC (STABILISED SAND AND CRUSHED ROCK). QUARTZITE SAND BASED 02 MIXTURE. FOR 0.8MM JOINT WIDTH. TYPE DESIGNATION L-A11 (GLASSIC STABILISED SAND AS APPENDIX 11/1).
 - TACTILE PAVING TO BE IN ACCORDANCE WITH T18-PC2. DARK GREY (400MM X 400MM X 65MM).
 - ASP PRECAST CONCRETE PAVING TO BE IN ACCORDANCE WITH F-PC-B(16). PAMPLE FINISH (02000003). GREY COLOUR.
 - ALL UTILITY COVERS TO BE ORIENTATED TO DIRECTION OF PAVING BOND.
 - THIS DRAWING IS TO BE READ IN CONJUNCTION WITH:
14201-Q2BKEGRD(GA/001), 14201-Q2BKEGRD(GA/003), 14201-Q2BKEGRD(SC/002), 14201-Q2BKEGRD(SC/001), 14201-Q2BKEGRD(SC/002)

Appendix B: Consultation Documents

- A new side road entry table with footway buildouts is proposed in Lancaster Street at the junction of Webber Street to improve pedestrian accessibility and reduce vehicle speeds.
- Existing raised carriageway table and zebra crossing between Silex Street and Boyfield Street to be extended to include Boyfield Street. The larger table will reduce traffic speeds and improve pedestrian accessibility.
- Proposed raised carriageway table in Silex Street at the junction with Webber Street to reduce traffic speeds and improve pedestrian accessibility (one parking space to be removed).

We want your views

It is important for all consultees to respond to the consultation. We would be grateful if you could take the time to review the proposals outlined in this document and provide a response using the pre-paid envelope and questionnaire provided by the **8th September 2014**.



Quietway
Greenwich to Waterloo

Site M – Webber Street and Blackfriars Road Junction

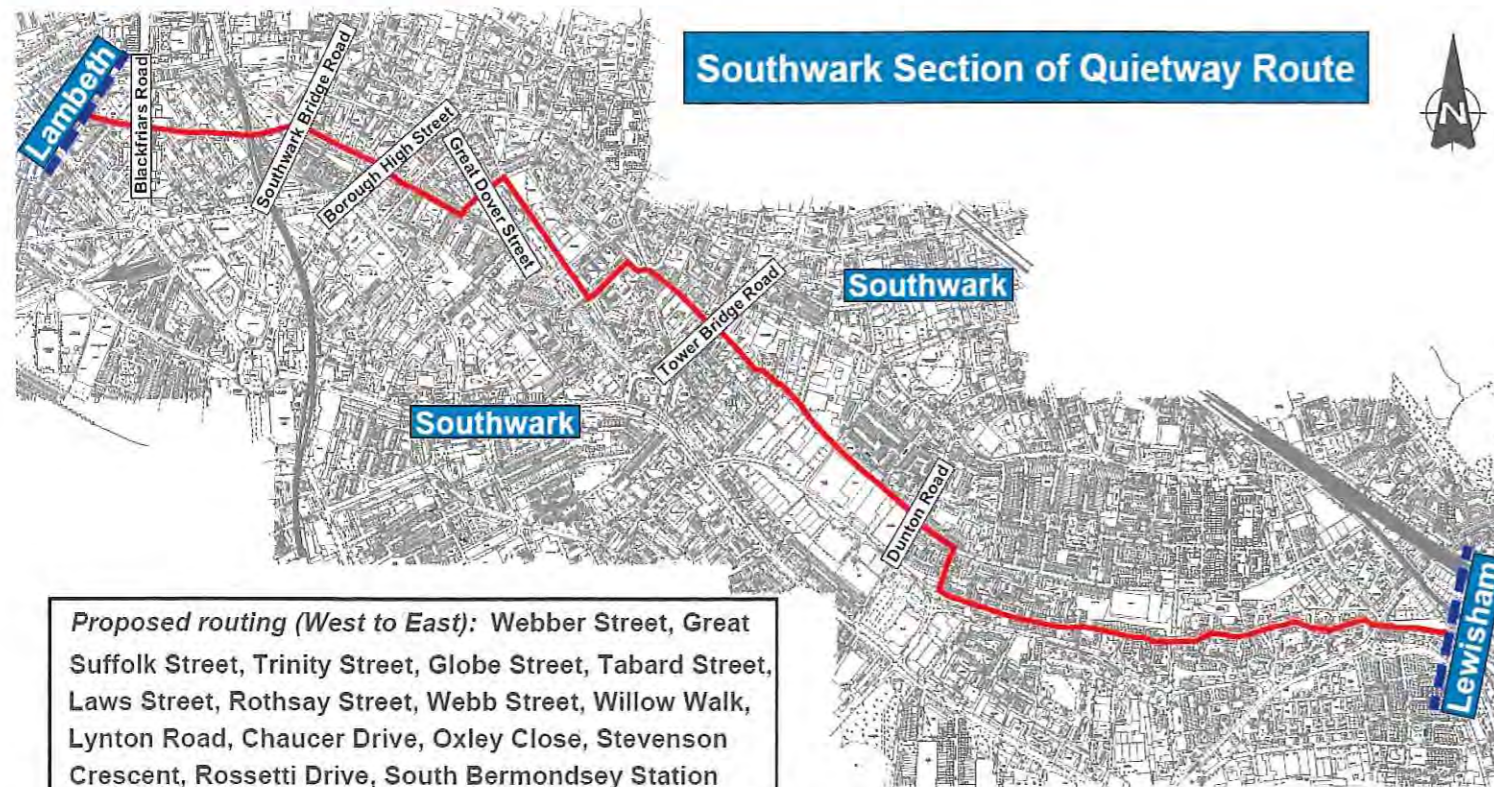
Cycle Priority Improvements and Streetscape Improvements

What happens next?

The responses to the questionnaire will be analysed and taken into account in the final design of the proposed works. As you will appreciate Southwark Council receives many comments from consultations and therefore are unable to respond personally to specific issues raised. However all comments and suggestions will be taken into consideration before a decision is made. The consultation results and recommendations will be presented at a local community council meeting in September 2014.

Should you require any further information regarding the proposed scheme please do not hesitate to contact Chris Mascord at chris.mascord@southwark.gov.uk

Further information on other schemes along the route in Southwark can also be found at: www.southwark.gov.uk/consultations



Have your say

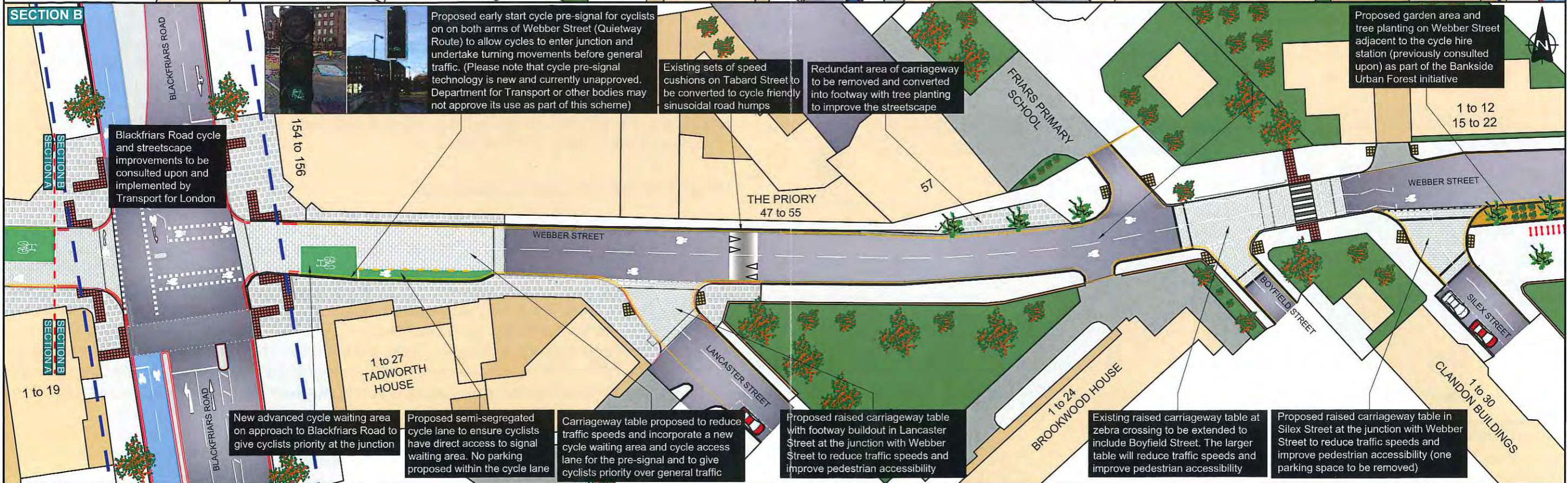
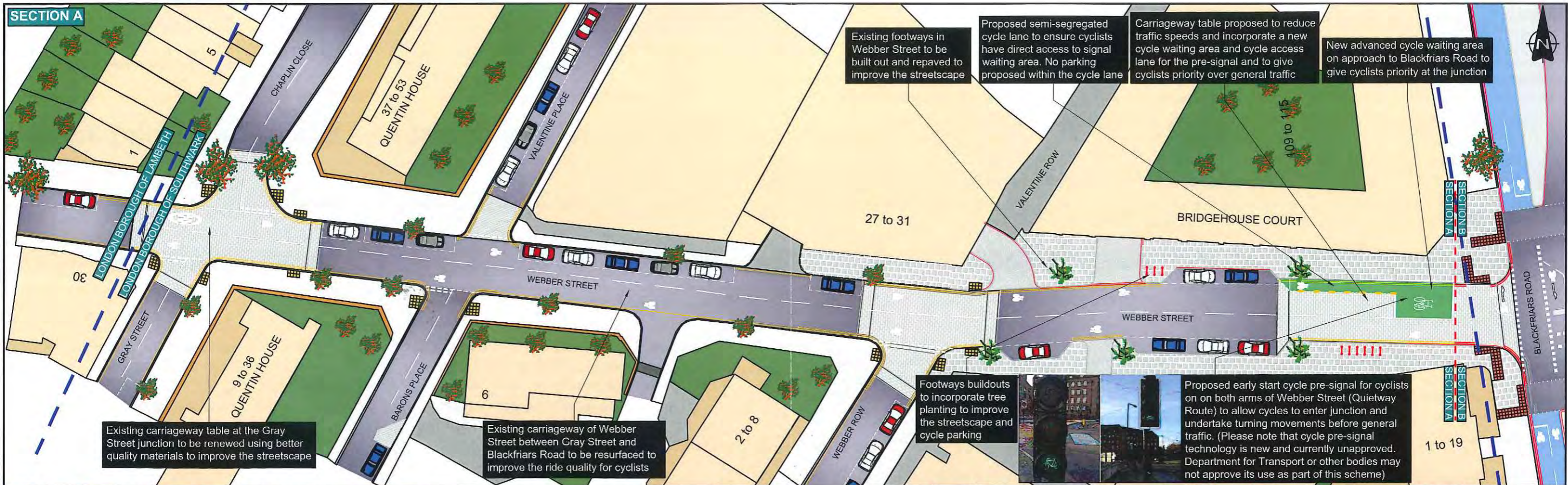
Southwark Council is holding a consultation to receive residents' and key stakeholders' comments regarding proposals relating to the borough's section of the Quietway (Greenwich to Waterloo) cycle route. This site forms part of a series of improvements along the route within Southwark that starts from South Bermondsey station in the east and traversing local roads to The Cut in the west. The proposed schemes will not only improve accessibility and safety for cyclists, but also significantly improve the streetscape.

Background

Quietways are a network of bike routes for less confident cyclists using low-traffic back streets. The routes are not just for current cyclists, but are for people who have always been put off cycling by the thought of sharing the road with high volumes of cars, vans, buses and lorries. Introducing Quietways forms an integral part of the Mayor of London's vision for cycling and the council's objective to significantly increase the number of residents using a cycling as their preferred mode of transport, particularly for local journeys.

What are the proposed changes?

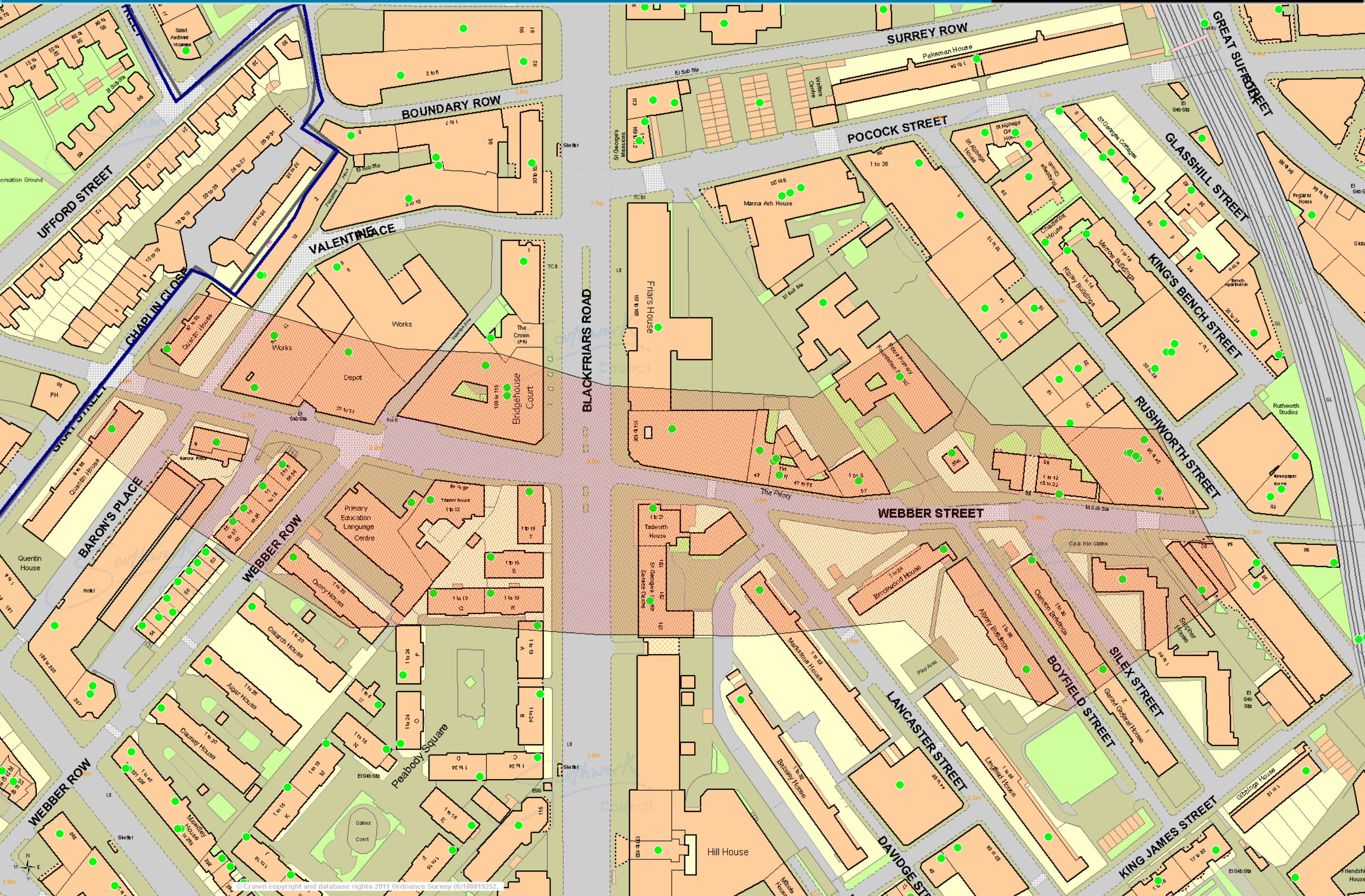
- Subject to approval from the DfT and other governing bodies, the Webber Street arms of the Blackfriars Road junction will have an early start cycle pre-signal so cyclists can traverse the junction and undertake turning movements before general traffic. These measures will be implemented in conjunction with Transport for London's Blackfriars Road Cycle Super Highway and streetscape improvements works in Blackfriars Road that will be consulted upon separately.
- Both approaches of Webber Street will have semi segregated cycle lanes so that cyclists can have unobstructed access to the waiting areas on approach to Blackfriars Road.
- Improved advance cycle waiting areas to be provided on both approaches of Webber Street for cyclists to take position ahead to general traffic at the junction.
- Cycle waiting areas and cycle lanes on both approaches of Webber Street to be incorporated onto new raised carriageway tables to reduce traffic speeds and improve safety.
- The carriageway of Webber Street is to be resurfaced and existing sets of speed cushions to be replaced with sinusoidal humps to improve the ride quality for cyclists.
- The streetscape of Webber Street to the west of Blackfriars Road and the borough boundary with Lambeth will have significant improvements with footway buildouts, additional parking bays and tree planting. Existing carriageway tables at Webber Row and Gray Street will also be improved with new materials to enhance the streetscape.



Appendix C: Location Plan and Extents of Consultation

Quietway Cycling Scheme - Site M - Consultation Area

Date 8/8/2014



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Appendix D: List of Addresses within Distribution Area

		6 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
		19 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
		9 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
		CLANDON BUILDINGS	BOYFIELD STREET	LONDON	SE1 0SD
JAMES POOL & SONS LTD	FLAT 9		207 WATERLOO ROAD	LONDON	SE1 8XD
JEF'S FOOD & DRINK LTD.	FOURTH FLOOR		43 WEBBER STREET	LONDON	SE1 0RF
MASTER SUPERFISH			191 WATERLOO ROAD	LONDON	SE1 8UX
DEALFIRST LTD & PATRICK GROUP LTD	UNIT 1		21 VALENTINE PLACE	LONDON	SE1 8QH
	UNIT 2		160 BLACKFRIARS ROAD	LONDON	SE1 8EZ
	UNIT 3		160 BLACKFRIARS ROAD	LONDON	SE1 8EZ
	LOWER GROUND FLOOR		WATERLOO ROAD	LONDON	SE1 8UX
	ROOM 402	BARONS PLACE, 195-203	WATERLOO ROAD	LONDON	SE1 8JU
	ROOM 209	NORTHCOTT HOUSE, 259	WATERLOO ROAD	LONDON	SE1 8JU
		NORTHCOTT HOUSE, 259	WEBBER STREET	LONDON	SE1 0RF
		1 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		2 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		3 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		4 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		5 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		6 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		7 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
		8 BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
	UNIT A	BELL HOUSE, 57	WEBBER STREET	LONDON	SE1 0RF
	ROOM 101	NORTHCOTT HOUSE, 259	WATERLOO ROAD	LONDON	SE1 8JU
LENTA PROPERTIES LTD			156 BLACKFRIARS ROAD	LONDON	SE1 8EN
FRIARS PRIMARY SCHOOL		RUSHWORTH AND FRIARS PRIMARY SCHOOL	WEBBER STREET	LONDON	SE1 0RF
	FLAT 10		59 WEBBER STREET	LONDON	SE1 0RD
	FLAT 8	MARKSTONE HOUSE	LANCASTER STREET	LONDON	SE1 0RL
	FLAT 5	ALBURY BUILDINGS	BOYFIELD STREET	LONDON	SE1 0SB
	BLOCK A FLAT 5	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HU
	BLOCK A FLAT 9	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HU
	BLOCK B FLAT 6	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HX
	ASSISTANT HALL MANAGERS FLAT	MCLAREN HOUSE, 1	ST GEORGES CIRCUS	LONDON	SE1 0AP
	FLAT 19	DAUNCY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QS
	FLAT 15	QUENTIN HOUSE	GRAY STREET	LONDON	SE1 8UY
	FLAT 31	QUENTIN HOUSE	GRAY STREET	LONDON	SE1 8UY
	BLOCK U FLAT 3	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JB
	BLOCK B FLAT 3	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HX
	BLOCK O FLAT 8	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8IA
	ROOM 311	NORTHCOTT HOUSE, 259	WATERLOO ROAD	LONDON	SE1 8JU
	BLOCK C FLAT 3	NORTHCOTT HOUSE, 259	WATERLOO ROAD	LONDON	SE1 8JU
	FLAT 9	17 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
	FLAT 12	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HY
	BLOCK G FLAT 2	OVERY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QX
		QUENTIN HOUSE	GRAY STREET	LONDON	SE1 8UY
THE COLLEGE HILL PRESS LTD		PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JH
	FLAT 12	DAUNCY HOUSE WEBBER ROW ESTATE	37 WEBBER STREET	LONDON	SE1 8QW
	FLAT 16	DAUNCY HOUSE WEBBER ROW ESTATE	189 WATERLOO ROAD	LONDON	SE1 8UX
	FLAT 7	DAUNCY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QS
	FLAT 11	DAUNCY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QS
	FLAT 16	ALGAR HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QS
	BLOCK G FLAT 1	ALGAR HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QT
	BLOCK M FLAT 1	PEABODY SQUARE	WEBBER ROW	LONDON	SE1 8QT
	BLOCK N FLAT 9	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JH
	BLOCK K FLAT 2	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JP
	BLOCK K FLAT 6	8 DELARCH HOUSE WEBBER ROW ESTATE	BLACKFRIARS ROAD	LONDON	SE1 8BQ
	FLAT 3	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JW
	FLAT 8	BRIDGEHOUSE COURT	BLACKFRIARS ROAD	LONDON	SE1 8JW
	BLOCK C FLAT 6	BRIDGEHOUSE COURT	BLACKFRIARS ROAD	LONDON	SE1 8HW
	FLAT 1	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HW
	FLAT 14	MAWDLEY HOUSE WEBBER ROW ESTATE	BLACKFRIARS ROAD	LONDON	SE1 8HY
	FLAT 18	MAWDLEY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8XQ
	FLAT 3	MAWDLEY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8XQ
	FLAT 8	MAWDLEY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8XQ
	BLOCK B FLAT 1	PEABODY SQUARE	WEBBER ROW	LONDON	SE1 8XQ
	BLOCK R FLAT 1	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8XQ
	BLOCK C FLAT 10	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HX
	BLOCK D FLAT 5	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JB
	FLAT 12	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8IF
	FLAT 18	STOPHER HOUSE, 90	BLACKFRIARS ROAD	LONDON	SE1 8HZ
	FLAT 14	STOPHER HOUSE, 90	WEBBER STREET	LONDON	SE1 0RE
	BLOCK C FLAT 7	BRIDGEHOUSE COURT	BLACKFRIARS ROAD	LONDON	SE1 8HW
	BLOCK D FLAT 8	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HY
	FLAT 25	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HZ
	FLAT 7	ALGAR HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QT
	FLAT 10	ALGAR HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QT
	FLAT 15	OVERY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QX
	FLAT 18	OVERY HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QX
	FLAT 21	QUENTIN HOUSE	GRAY STREET	LONDON	SE1 8UY
	FLAT 3	QUENTIN HOUSE	GRAY STREET	LONDON	SE1 8UY
	BLOCK B FLAT 8	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HX
	SUITE 113	154-156	BLACKFRIARS ROAD	LONDON	SE1 8EN
	FLAT 20	BRIDGEHOUSE COURT	BLACKFRIARS ROAD	LONDON	SE1 8HW
	FLAT 25	BRIDGEHOUSE COURT	BLACKFRIARS ROAD	LONDON	SE1 8HW
	BLOCK T FLAT 2	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HS
	BLOCK T FLAT 11	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HS
	BLOCK S FLAT 9	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HT
	BLOCK H FLAT 2	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8J
	BLOCK H FLAT 6	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8J
	BLOCK H FLAT 11	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8J
	SUITE 309	154-156	BLACKFRIARS ROAD	LONDON	SE1 8EN
	SUITE 105	154-156	BLACKFRIARS ROAD	LONDON	SE1 8EN
	FLAT 17	ALGAR HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QT
	FLAT 21	ALGAR HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QT
			18 WEBBER ROW	LONDON	SE1 8QP
			26 WEBBER ROW	LONDON	SE1 8QP
			36 WEBBER ROW	LONDON	SE1 8QP
			44 WEBBER ROW	LONDON	SE1 8QP
			54 WEBBER ROW	LONDON	SE1 8QP
			219 WATERLOO ROAD	LONDON	SE1 8XH
TURNING POINT LTD	BLOCK S GROUND FLOOR OFFICE	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HU
	FLAT 48	STOPHER HOUSE, 90	WEBBER STREET	LONDON	SE1 0SE
	FLAT 6	STOPHER HOUSE, 90	WEBBER STREET	LONDON	SE1 0RE
		7 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
	WORKSHOP	STOPHER HOUSE, 90	WEBBER STREET	LONDON	SE1 0RE
		20 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
ABA (INTERNATIONAL) LTD			19 VALENTINE PLACE	LONDON	SE1 8QH
	FLAT 43	16 DELARCH HOUSE WEBBER ROW ESTATE	WEBBER ROW	LONDON	SE1 8QU
		STOPHER HOUSE, 90	WEBBER STREET	LONDON	SE1 0SE
		44-50	LANCASTER STREET	LONDON	SE1 0SJ
COLORAMA PROCESSING LABS LTD	FLAT 16	BRIDGEHOUSE COURT	BLACKFRIARS ROAD	LONDON	SE1 8HW
	FLAT 26	STOPHER HOUSE, 90	WEBBER STREET	LONDON	SE1 0SE
	BLOCK J FLAT 1	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8HN
		THE CROWN, 108	BLACKFRIARS ROAD	LONDON	SE1 8HW
	FLAT 3	CLANDON BUILDINGS	BOYFIELD STREET	LONDON	SE1 0SD
	BLOCK E FLAT 6	PEABODY SQUARE	BLACKFRIARS ROAD	LONDON	SE1 8JG
	FLAT 26	QUENTIN HOUSE	GRAY STREET	LONDON	SE1 8UY

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